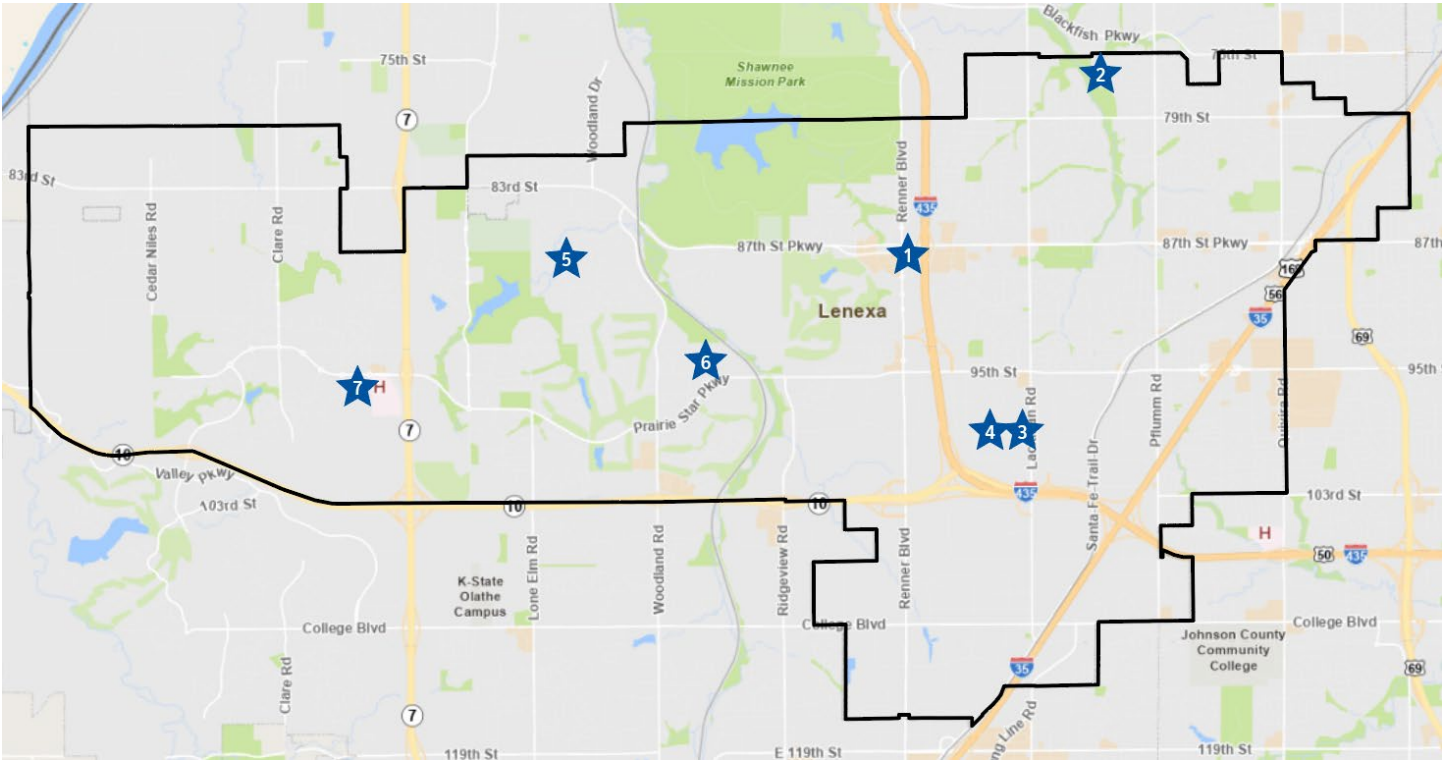


AGENDA MAP



CALL TO ORDER

ROLL CALL

APPROVE MINUTES FROM THE JUNE 1, 2026 MEETING

CONSENT AGENDA

All matters listed within the consent agenda have been distributed to each member of the Planning Commission for review, are considered to be routine, and will be enacted by one motion with no separate discussion. If a member of the Planning Commission desires separate discussion on an item, that item may be removed from the consent agenda and placed on the regular agenda.

1. **The Rise — Consideration of a final plan for a mixed-use development on property located at 8700 Ryckert Street within the CC, Planned City Center District. PL26-07F**
2. **Lenexa Municipal Services Storage Building — Consideration of a revised final plan for an accessory structure on property located at 7700 Cottonwood Street within the R-1, Single-Family Residential District. PL26-04FR**
3. **CMC Foods and CMC Lenexa — Consideration of a final plan and final plat for manufacturing, office, and warehousing uses with deferred parking on property located at 9806 Lackman Road within the BP-2, Planned Manufacturing District.**
 - a. **Consideration of a final plan for manufacturing, office, and warehousing uses with deferred parking within the BP-2, Planned Manufacturing District. PL26-08F**

- b. **Consideration of a final plat for a one-lot industrial subdivision within the BP-2, Planned Manufacturing District. PT26-19F**

- 4. **Kansas Commerce Center, Fifth Plat — Consideration of a revised final plat to adjust lot lines for warehousing and manufacturing uses on property located at 15300 W. 99th Street, 15600 W. 99th Street, and 9755 Commerce Parkway within the BP-2, Planned Manufacturing District. PT26-04FR**

- 5. **Wilson Accessory Structure — Consideration of a deviation from the required side yard setback to construct an accessory garage on property located at 8720 Old Trail Road within the RP-E, Planned Residential Estate District. DV26-03**

REGULAR AGENDA

- 6. **Marmon Estates and Woodland Lakes Lot 2 — Consideration of a rezoning, preliminary plat, and final plat for a two-lot single-family residential development located near the northeast corner of 95th Street & Cherry Lane. (Public Hearing)**
 - a. **Consideration of a rezoning from the RE, Residential Estate District to the R-1, Single-Family Residential District to allow a single-family residential lot. RZ26-04**
 - b. **Consideration of a preliminary plat for a two-lot single-family residential subdivision in the R-1, Single-Family Residential District. PT26-02P**
 - c. **Consideration of a final plat for a two-lot single-family residential subdivision in the R-1, Single-Family Residential District. PT26-16F**

- 7. **Prairie Star Parkway Center — Consideration of a rezoning and preliminary plan/plat for a nine-lot commercial development located south of Prairie Star Parkway between Gleason Road and Dunraven Street. (Public Hearing)**
 - a. **Consideration of a rezoning from the AG, Agricultural District to the CP-2, Planned Community Commercial District to allow a new commercial development. RZ26-03**
 - b. **Consideration of a preliminary plan/plat for a nine-lot commercial development in the CP-2, Planned Community Commercial District. PL26-06P**

STAFF REPORTS

ADJOURN

APPENDIX

8. **June 1, 2026 Draft Minutes**

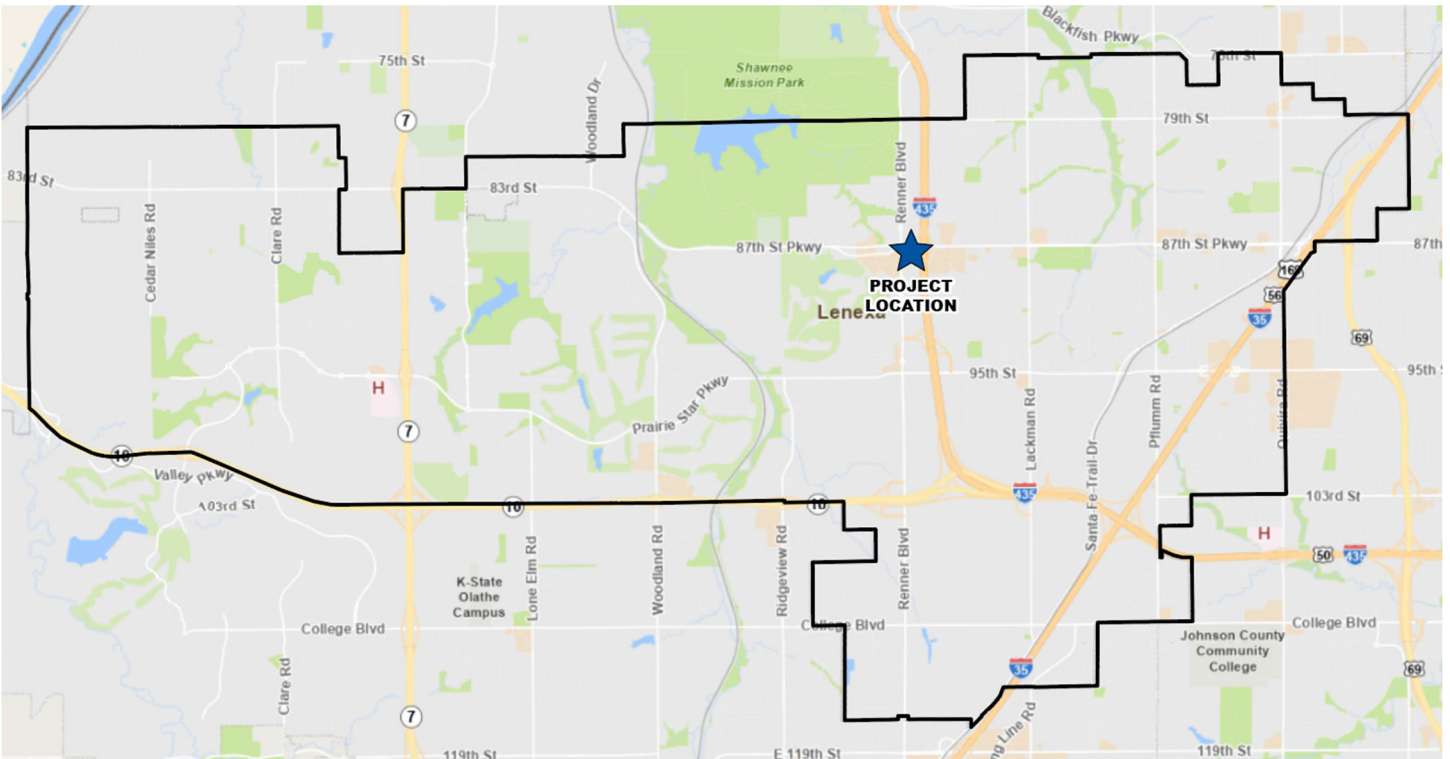
If you have any questions about this agenda, please contact Stephanie Sullivan, Planning Manager, at ssullivan@lenexa.com.

*If you need any accommodations for the meeting, please contact the City Clerk's Office at 913-477-7550 at least 48 hours prior to the meeting.
Kansas Relay Service: 800-766-3777*

Assistive Listening Devices are available for use in the Community Forum by request.

THE RISE

Project #:	PL26-07F	Location:	SEC of W. 87 th Street Parkway and Renner Boulevard
Applicant:	Cameron Gilyard, Finkle Williams Architects	Project Type:	Final Plan
Staff Planner:	Dave Dalecky	Proposed Use:	Mixed-Use



PROJECT SUMMARY

The applicant requests approval of a final plan for a mixed-use development in City Center at the southeast corner of W. 87th Street Parkway and Renner Boulevard. The mixed-use development contains a nine-story building with a ground floor parking structure (total of ten floors). The building will include 6,943 SF of office space, a 5,276 SF restaurant and bar, and 121 multifamily residential units. A final plan for this site was approved by the Planning Commission on April 6, 2026 (PL26-02F). The final plan was withdrawn by the applicant prior to consideration by the City Council. The proposed final includes modifications from the previous plan, including reducing the building height, minor changes to the square feet of the nonresidential uses, and one less apartment unit. The final plan does not require a public hearing by the Planning Commission.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The site is a 1.54-acre parcel of undeveloped land at the southeast corner of W. 87th Street Parkway and Renner Boulevard in City Center. The site is a lot in the City Center East Village bordered by W. 87th Terrace on the north, Ryckert Street on the east, Renner Avenue on the west, and a private drive on the south. There have been several iterations of plans for a mixed-use building on this property, with the most recent being “The Yard”, which was approved in June 2021 for a mixed-use development which included office, restaurant, and entertainment uses.

The site is in a subregion of City Center known as City Center East Village. City Center East Village consists of the Hyatt Place Hotel and Convention Center, The Lofts, Jack Stack, and the BE Smith building and parking structure.

TABLE 1: PREVIOUS APPLICATIONS

Project No.	Type	Project Name	Date Approved
RZ03-04	Rezoning	Lenexa City Center East Village	April 15, 2003
PL03-01CP	Concept Plan	Lenexa City Center East Village	April 15, 2003
PL16-02P	Preliminary Plan	Lenexa City Center East Village Mixed Use	July 19, 2016
PL18-15F	Final Plan	The Yard	August 21, 2018
PT18-12F	Final Plan	Lenexa City Center – East Village, Fifth Plat	August 21, 2018
PL19-03FR	Revised Final Plan	The Yard	June 3, 2019
PL21-07F	Final Plan	The Yard	June 28, 2021
PL25-14P	Preliminary Plan	The Rise	October 21, 2025
PL26-02F	Final Plan	The Rise	April 6, 2026 (PC only) <i>withdrawn prior to City Council consideration</i>

LAND USE REVIEW

The site is zoned CC, Planned City Center District. The site is at the southeast corner of W. 87th Street Parkway and Renner Boulevard, a major intersection in City Center. The intersection is the east gateway into the City Center District. The proposed final plan is for a nine-story mixed-use building that contains commercial and multifamily residential uses.

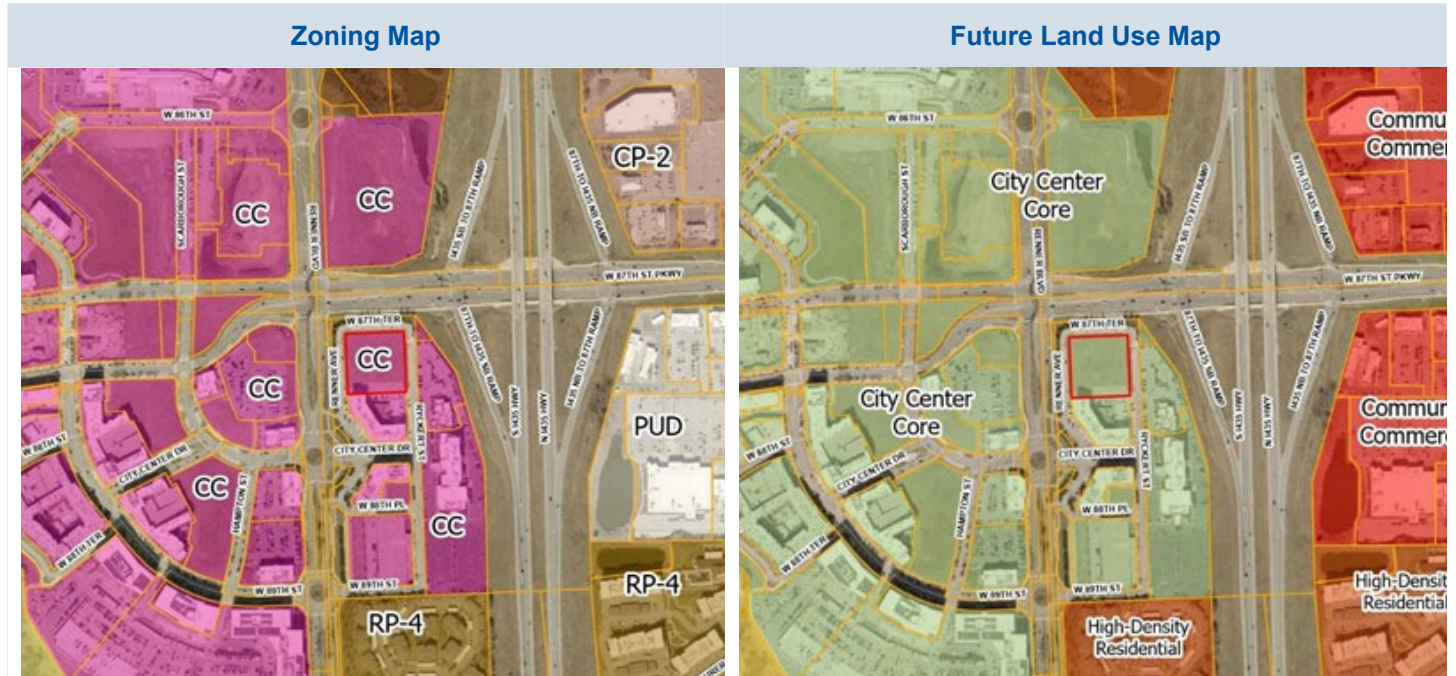


TABLE 2: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Classification	Zoning	Current Use
Subject Property	City Center Core	CC, Planned City Center District	Undeveloped land
North	City Center Core	CC, Planned City Center District	Undeveloped land
South	City Center Core	CC, Planned City Center District	Mixed-Use
East	Community Retail	PUD, Planned Unit Development District (across I-435)	Carwash, Restaurant, and Retail
West	City Center Core	CC, Planned City Center District	Restaurant

FINAL PLAN REVIEW

The site is located at the southeast corner of W. 87th Street Parkway and Renner Boulevard. The ground floor of the building will use the same “L” shaped floor plan of the first floor as proposed for the previous final plan but will have a larger floor plan for the upper eight floors of the building. The first floor will contain the office tenant space and resident amenities and leasing office. The upper eight floors are all residential. The top level (rooftop) will have a restaurant and bar accessible to the public.

The most recent application was for a mixed-use final plan for The Rise, which was withdrawn by the applicant prior to consideration by the City Council. This final plan included 6,400 SF of office uses, a 5,627 SF rooftop restaurant and bar, and 122 multifamily residential units. The proposed final plan for the project is slightly different than the previously submitted final plan. The current proposal contains 6,943 SF of office, 5,276 SF rooftop restaurant and bar, and 121 multifamily. The proposed height of the building is reduced from eleven stories to nine stories (ten total stories with the ground floor parking structure). The average height of the proposed building is reduced to 109’ tall. The previous final plan for an eleven-story building had an average height of 136’-6” tall.

A comparison of the changes to the uses in the building are listed in Table 2.

TABLE 2: SQUARE FEET COMPARISON				
	Office	Restaurant & Bar	Dwelling Units	Floors
PL26-02F (previous plan)	6,400 SF	5,627 SF	122	11 (12 with ground floor garage)
PL26-07F (proposed plan)	6,943 SF	5,276 SF	121	9 (10 with ground floor garage)



Exhibit 2: Building height comparison of previous (left image) and proposed (right image) final plan west elevations

The proposed changes from the previous final plan to the proposed final plan are still within the tolerance of consistency with the approved preliminary plan for the project based on the “general tests of consistency” per [Section 4-1-H-4-D](#) of the Unified Development Code.

The site is on a block that has a frontage street configuration. The lot is encircled by local streets on the north, east, and west sides of the site, and has a private drive along the south property line. The frontage street configuration further separates the building from the primary streets of W. 87th Street Parkway and Renner Boulevard. The configuration of this block in City Center East Village still achieves primary design expectations for city with the full vehicular circulation around the building with the pedestrian spaces and connections from the street network, including W. 87th Street Parkway and Renner Boulevard, to the building and to the surrounding development.

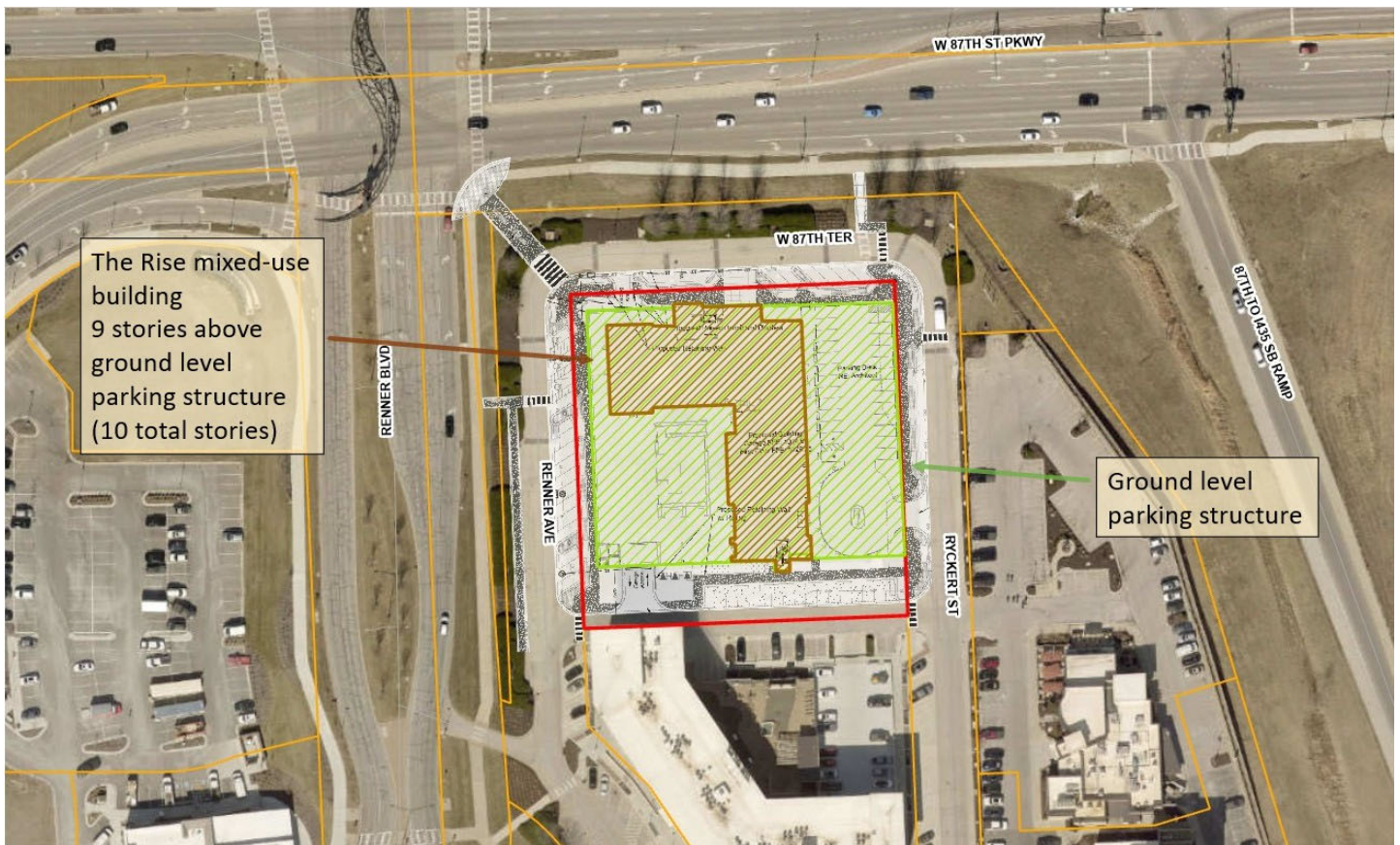


Exhibit 3: The Rise Site Plan.

DIMENSIONAL STANDARDS

The site is in City Center, which requires buildings to be brought up to the street to create an urban environment. The north and east facing sides of the building satisfies this requirement. The building and building entrance, along the east facing side, is setback away from Ryckert Street. This setback is an acceptable configuration. Ryckert Street is a secondary street of this development. The east side of the building has a small surface (top of parking garage level) parking area and a circular drop-off area. This side of the site has a similar character as the restaurant across Ryckert Street and as the site to the south. The parking area is well screened from the primary streets of W. 87th Street Parkway and Renner Boulevard.

The building is nine stories and is an average of 109' tall. The height is within the general height limit of 110' for buildings in City Center. [Section 4-1-B-28-F-4](#) of the UDC states the minimum and maximum building height for buildings in the CC Zoning District. Exceptions to these dimensions may be allowed if the building height is approved as part of a development plan and/or an associated development agreement.

The plan complies with the dimensional standards of vehicular circulation and pedestrian spaces for City Center. A parking garage structure in City Center will typically have 9' wide stalls and at least 24' wide drive aisles. The parking garage is consistent with these dimensions. The reduced dimension of the drive aisles is common for parking structures and is allowed as a design exception in the CC Zoning District. The region has several areas of structured parking where the 24' wide drive aisle is the standard design. The region is intended to be a more compact development where such an alternative design is acceptable provided certain conditions are met, such as compliance with all Fire Safety access standards. Staff does not object to the reduced width of these drive aisles as proposed.

PUBLIC IMPROVEMENTS

The public streets around the site will be partially or completely reconstructed and the public sidewalks and PAZ spaces will be constructed with this development. A new pedestrian space and sidewalk connections will be constructed between the arterial street intersection and local street intersection and new landscaping installed in the right-of-way. Decorative pavement in an arc pattern will be installed at the street intersection where pedestrians will wait to cross W. 87th Street Parkway and Renner Boulevard. These improvements will also require a public improvement plan submittal.

ACCESS, TRAFFIC, AND PARKING

Access into the East Village subregion is from the roundabout at the intersection of Renner Boulevard and City Center Drive. A second access drive into the East Village subregion is further south along Renner Boulevard. Local streets in the subregion interconnect throughout the area and directly abut the site. The site is bordered by W. 87th Terrace on the north, Ryckert Street on the east and Renner Avenue on the west. A private drive is along the south property boundary.

Pedestrian connections to this site are provided from the surrounding primary streets and along the “frontage” streets around this lot. The connection to W. 87th Street Parkway and Renner Boulevard is provided with a marked diagonal crosswalk leading from the northwest corner of the site down to the intersection. An additional connection is provided via the existing stairway along W. 87th Street Parkway, with a new pedestrian crosswalk at the northeast corner of the site. Other locations where a sidewalk will cross the public street, and private drive, will also have standard street markings for pedestrian crosswalks. The specific locations for each crossing will be reviewed in greater detail with a Public Improvement Plan submittal to ensure crossings are at appropriate locations and the required signage can be installed.

Entrances into the building are an important design element for development in City Center. Buildings are to have primary entrances along the streets to create an active streetscape. This site is unique that the grade changes from east to west approximately 9', nearly an entire floor level. This transition results in the ground level of the building being partially below the surrounding grade on the east side of the site and fully exposed to the west. The parking garage level of the building is partially below grade on the east side of the site and is the first-floor level of building along the west. The grade change continues from the north side of the building to W. 87th Street Parkway street level. The grade changes from the site to the surrounding street network directly impact the placement of the entrances into the building.

The primary access into the building is on the east façade. This entrance is to the first floor of the building. This floor will have shared amenity space for the multifamily units and access to the office space. The main access to the rooftop restaurant and bar is on the north façade. This entrance will provide access to an elevator and stairwell that will allow access to the rooftop for the public. This elevator will have controlled access to each floor for the residents of the building. Another means of access into the building will be from the southwest corner of the building. An exterior stair section will provide access to the first floor from the street level. This will also be a

controlled access for residents of the building. There is an additional entrance into the building on the south side of the site which is also a controlled access for residents.

The typical City Center Pedestrian Amenity Zone (PAZ) modular pavement pattern is provided around the three public street frontages of the site. The streetscape includes rows of angled parking intermixed with sections of parallel parking spaces. The PAZ includes planters for trees and landscaping in this space. The site will have a primary plaza feature at the northwest corner of the building with raised planters and seating. This space links to a new sidewalk connection to the street intersection of W. 87th Street Parkway and Renner Boulevard. A new pedestrian pad will be installed at this intersection to mimic the large circular pattern of the other intersections along W. 87th Street Parkway running the length of City Center. This circular pavement design is a defining statement that identifies these public spaces and major intersections which encourage pedestrian circulation through the district.

Parking is provided with a parking garage, surface parking, and on-street parking. The on-street parking is both angled and parallel spaces available along Renner Avenue, W. 87th Terrace, and Ryckert Street. The on-street angled parking is consistent with the on-street parking for public streets throughout City Center and the preferred configuration per the Lenexa City Center Neighborhood Design Standards & Guidelines (CCDG). A row of surface parking is provided from a private drive between this site and the site to the south, these parking spaces are ninety-degree spaces.

The ground level of the building will be a parking garage containing 120 parking spaces. A second level of parking is provided on the east side of the building and includes 26 parking spaces. Surface parking is provided on the south side of the building along the private drive and on-street parking is provided on the other three sides of the lot. A total of 199 public and private parking spaces are provided for the development. [Section 4-1-D-1](#) of the UDC requires 335 parking spaces per the ratio requirements for restaurant, office, and multifamily uses. The applicant presented a parking study with the preliminary plan that shows the actual needs for parking based on the mix of uses and the demand for parking during the peak times of the day for each use.

A reduced number of parking spaces from the minimum requirements of the UDC parking table is consistent for mixed-use developments throughout City Center. The nature of a compact development with both shared and complimentary uses results in a reduced overall demand for parking. City Center is designed to encourage walking between destinations instead of using a vehicle to move from place to place. Public parking areas are provided in locations such as the parking structure near City Hall and the parking structure near the Hyatt Place Hotel. Private parking garages are also intended to provide parking spaces for customers and visitors of other businesses than the building or tenants that the garage is associated with. This design results in the option to consider reducing the number of parking spaces that are required for various development throughout City Center. Other projects, both residential and nonresidential, have been approved with fewer parking spaces than what is required by the parking standard in [Section 4-1-D-1](#) of the UDC. Staff is supportive of the request for a 136-space reduction in parking as shown on the plan. Per [Section 4-1-B-28-F-5](#) of the UDC, a deviation for reducing the required parking is not necessary for developments in City Center.

TABLE 3: PARKING ANALYSIS

Use	Requirement	Required	Provided	Difference
Multifamily	1 space per efficiency unit, 1.5 spaces per 1-bedroom unit, 1.75 spaces per 2-bedroom unit, 2 spaces per 3+-bedroom units and 0.25 spaces per unit for visitor parking if parking spaces are located in common parking area	236		
Office	1 space per 250 square feet	28		
Restaurant/Bar	1 space per 75 square feet	71		
TOTAL		335	199	-136

The preliminary plan was approved with a reduction of 104 parking spaces. The revised plan has a larger rooftop restaurant and bar but has fewer multifamily residential units. A restaurant has a significantly higher parking requirement than multifamily residential uses. The parking study and narrative provided by the applicant shows that the site will accommodate the typical parking demand for the mix of uses and that additional parking is available in public parking areas around the site and through lease agreements with The Lofts, which is under the same ownership as The Rise.

Staff supports the request for a parking reduction, including the additional 32 parking spaces beyond what was approved within the preliminary plan, from the minimum number of spaces required. Per [Section 4-1-B-28-F-5](#) of the UDC, a deviation for reducing the required parking is not necessary for developments in City Center. The applicant provided a parking study reflecting the mix of uses of the site and the combination of public on-street and private parking, along with shared parking available on nearby development, the site will meet the typical demands for parking for the development. Times of peak demand may result in visitors parking farther away in public parking areas, which is an expected condition for development in City Center.

STORMWATER

Stormwater management for this site is integrated into the previously constructed stormwater system for City Center East and there will be no new features or facilities added.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

LIGHTING

The site will include parking lot lighting and pedestrian lighting with bollards and building mounted sconces. The lighting is consistent with lighting for similar development in City Center.

ARCHITECTURE

The building is a contemporary styled building using a limited palette of materials and colors but incorporating multiple plane changes on each of the wall planes. The building uses brick, block, metal, EIFS and glass as the primary exterior finish materials. The block materials are to be created by using a poured concrete form-liner to

emulate the finished character of the proposed materials. The form-liner is to be used for the exterior walls of the ground floor parking structure. The parking structure walls are part of the building that will be along the PAZ space. The form-liner construction technique is not common for City Center. The sections of the wall will use three different finishes, a dark gray smooth finish stacked stone, a beige sandblasted finish stacked stone, and a beige coarse finish random stone. The three unique color and surface finishes must be installed in a manner to reflect the high-quality exterior building materials that are expected for buildings in City Center. Staff will coordinate this through the construction approval process for the building. This may include preparing a mockup of the form-liner finishes for review prior to actual construction of the building.

The upper floors of the building will use brick, metal, EIFS, and glass for the exterior materials. The brick is a composite material that emulates brick coursing. This is common material which is used on many existing buildings in City Center. The brick is oriented in a running bond in most applications and as a soldier stack in some section of the wall to create variety in the wall plane. The metal panel material is used as accent materials on all facades to maintain continuity of all sides of the building.

The building facades use multiple wall plane changes to create depth and shadow lines. Individual patio spaces will use a recessed space in the wall and projecting balconies with a suspended railing. The upper floors will overhang the first floor of the building along parts of the façade to provide protected cover along the first-floor façade. The rooftop restaurant has an open-seating area on the west side of the building. This section of the rooftop space has a glass railing to contrast with the other sections of the building façade. The interior space of the rooftop restaurant will use floor to ceiling glass windows. The public access to the rooftop restaurant is by an elevator from the street level to the rooftop. The elevator will have a glass wall for visitors to view the street while in the elevator car.

The main entrance for residents, visitors and for the office tenant space is on the east side of the building. This entrance has a circular drop-off area and a 26-space parking area on the second level of the parking garage. The circular drive has a covered arcade providing weather protection when dropping off. This access faces directly south to the private drive between this site and The Lofts.



Exhibit 4: The Rise rendering of west elevation.



Exhibit 5: The Rise rendering of north & east elevation.

The ground floor of the building is the parking garage which fills the block and brings the wall of the structure to the street on all four sides. This garage level has a vehicular access at the southwest corner of the building. This access faces directly south to the private drive between this site and The Lofts. The vehicle entrance to the garage will have solid overhead doors. A stairway that provides pedestrian access to the second level (first floor of the building) is immediately around the corner, facing west. This stairway leads to a controlled access door to the residents' amenities and outdoor pool and deck. The ground level of the west façade will be the exterior wall

of the parking garage. This façade will not have a direct entrance into the building but does have the stairway at the south end of the façade. The applicant intends to make this a multipurpose part of the building and site, where the street could be closed for special events and community gatherings. Ornamental metal panels are installed on the west facing façade. The north façade will have two overhead doors. One of the doors is for a loading area for residents moving large items into and out of the building. A second overhead door is for trash service. Both doors will be glass doors with a clouded or spandrel coating, not clear glass.

SIGNS

The building elevations show signs for the development. The signs include typical wall signs and a blade sign on the south façade that identify the project as “The Rise.” A sign is shown on the north façade to identify the rooftop restaurant and bar. Wall signs will likely be installed for the office tenant on the first floor. The signs are consistent with other developments in City Center. The installation of monument signs are not encouraged for City Center because monument signs are oriented to vehicular traffic. Each sign will require a separate sign permit to be submitted to the City for review and approval prior to installation.

LANDSCAPING

The final plan shows landscaping throughout the site consistent with the UDC requirements for City Center. Most landscaping is installed in the PAZ in planters along the modular decorative paving space. This site has some additional spaces for landscaping at the ends of the row of parking spaces on the south side of the site.

There is a lawn and landscape space between the arterial streets and frontage roads around the site. This space has existing trees and shrubs planted several years ago and have grown to maturity. Some of these shrubs will be removed for the installation of sidewalk connections from the frontage streets to the arterial streets.

Additional ornamental and seasonal landscaping is installed along the sidewalk connection from the street intersection to create an inviting link to a plaza space at the northwest corner of the building. The plaza space is integrated with the PAZ space with planters and seating benches.



Exhibit 6: Landscape plan.

DEVIATIONS

The applicant is not requesting any deviations from the UDC.

REVIEW PROCESS

- The Planning Commission is the approval authority on this application. The City has a practice of submitting City Center final plans to the City Council for affirmation of the Planning Commission’s decision. Pending the Planning Commission’s decision, the project will be tentatively scheduled for consideration by the City Council on July 21, 2026.
- The applicant must receive permit(s) prior to commencing construction.
- The applicant must obtain a Certificate of Occupancy before opening for business.
- The applicant must obtain a Business License prior to opening for business.
- The applicant should inquire about additional City requirements, such as permits and development fees.

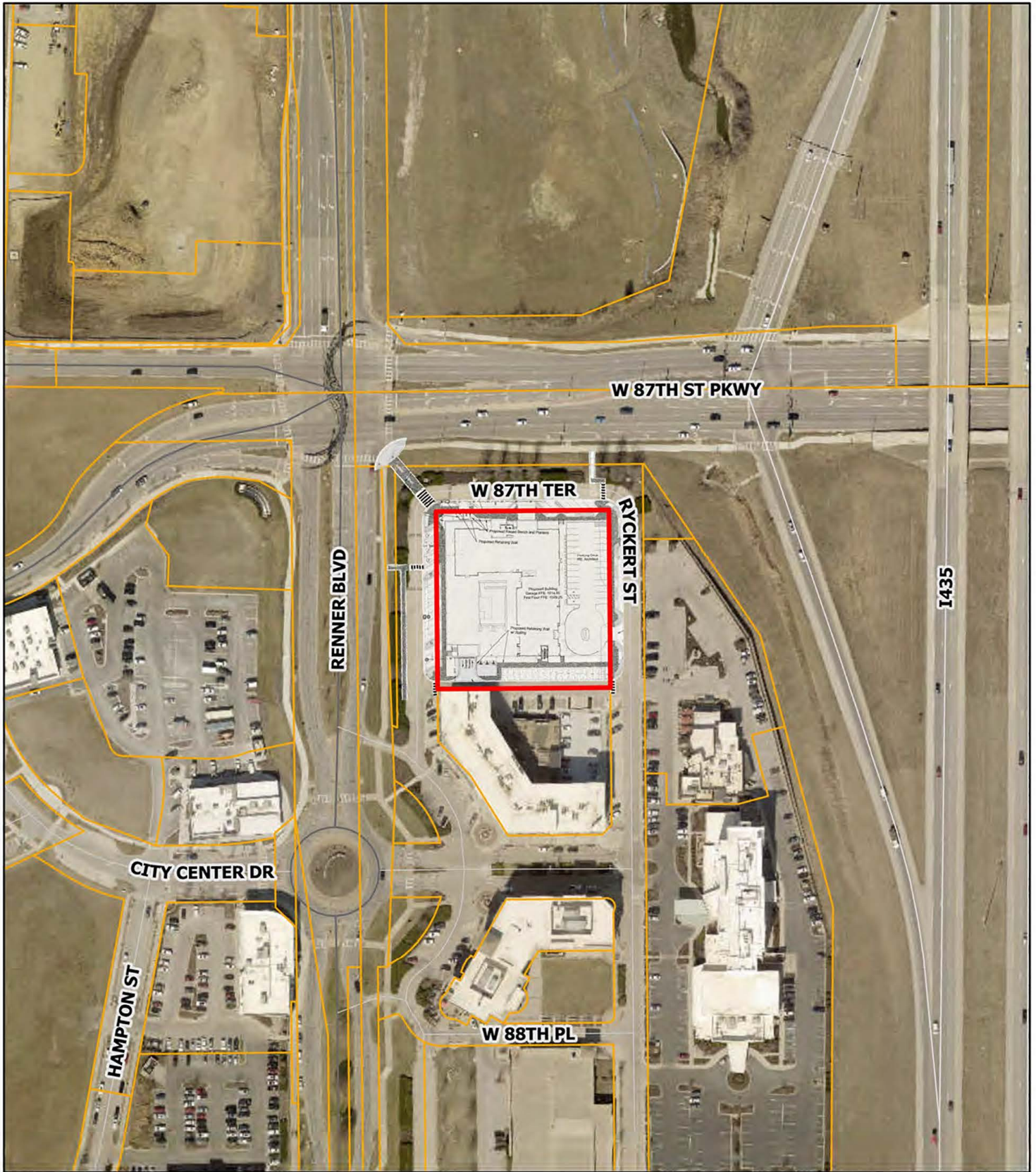
RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the proposed Final Plan for The Rise.**

- The final plan will facilitate the development of a mixed-use building consisting of ten levels, including a nine-story structure above a ground-floor parking garage. The development will include 6,943 SF of office space, 5,276 SF of restaurant/bar space, and 121 multifamily residential units.
- The project is consistent with Lenexa’s goals through ***Responsible Economic Development*** to create ***Inviting Places***.

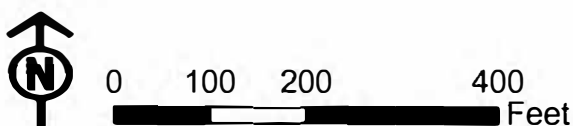
FINAL PLAN

Staff recommends **APPROVAL** of the final plan for PL26-07F – **The Rise** at the southeast corner of W. 87th Street Parkway and Renner Boulevard, for a mixed-use development.



Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

The Rise





the
RISE

the
RISE

 **FINKLE +
WILLIAMS**
ARCHITECTURE



 FINKLE +
WILLIAMS
ARCHITECTURE

the
RISE

THE RiSE Shared Parking Study - May 28, 2026

UNADJUSTED BASE PARKING RATIOS BY LAND USE *



Land Use:	Building SF or Units	Spaces Per Unit**				Base Unit Design***	Indicated Spaces
		Weekday		Weekend			
		Visitor	Resident/Employee	Visitor	Resident/Employee		
Existing Restaurant	0					0	0
Existing Office	0					0.00	0
TOTAL EXISTING	0						0
New Rooftop Bar	2,650	10.00	1.00	12.35	1.00	13.35	35
New Retail	0	2.90	0.70	3.20	0.80	4.00	0
New Office	6,943	0.50	3.50	0.03	0.35	4.00	28
New Residential	120	0.15	1.76	0.15	1.50	1.91	229

UNADJUSTED BASE PARKING INDICATED WITHOUT ANY SHARED PARKING 292

PEAK SHARED PARKING INDICATED BY THIS MODEL 187

* Source of Data - City of Lenexa

** Spaces are per 1,000sf for Commercial Use

*** Spaces are per unit for each Residential Use

**** Residential RESERVED Parking as a % of Total Parking: 135 78.9%

**** SHARED Public Parking as a % of Total Parking: 53 21.1%

188 100.0%

ADJUSTMENTS* TO BASE UNIT DESIGN RATIOS

May Weekday 7:00PM

Land Use:	Base Unit Design	Month Adjustment	Peak Hour Adjustment	Noncaptive Adjustment	Mode Adjustment	Total Adjustment	Base Design Indicated Spaces	Actual Parking Demand
Existing Restaurant	0	100.0%					0	
Existing Office	0.00	100.0%					0	
TOTAL EXISTING							0	
New Rooftop Bar	13.35	80.1%	82.4%	80.0%	75.0%	39.6%	35	14
New Retail	4.00	66.6%	0.0%	0.0%	0.0%	0.0%	0	0
New Office	4.00	100.0%	13.4%	90.0%	90.0%	10.8%	28	3
New Residential	1.91	100.0%	91.6%	90.0%	90.0%	74.2%	229	170
							292	187

December Weekend 6:00PM

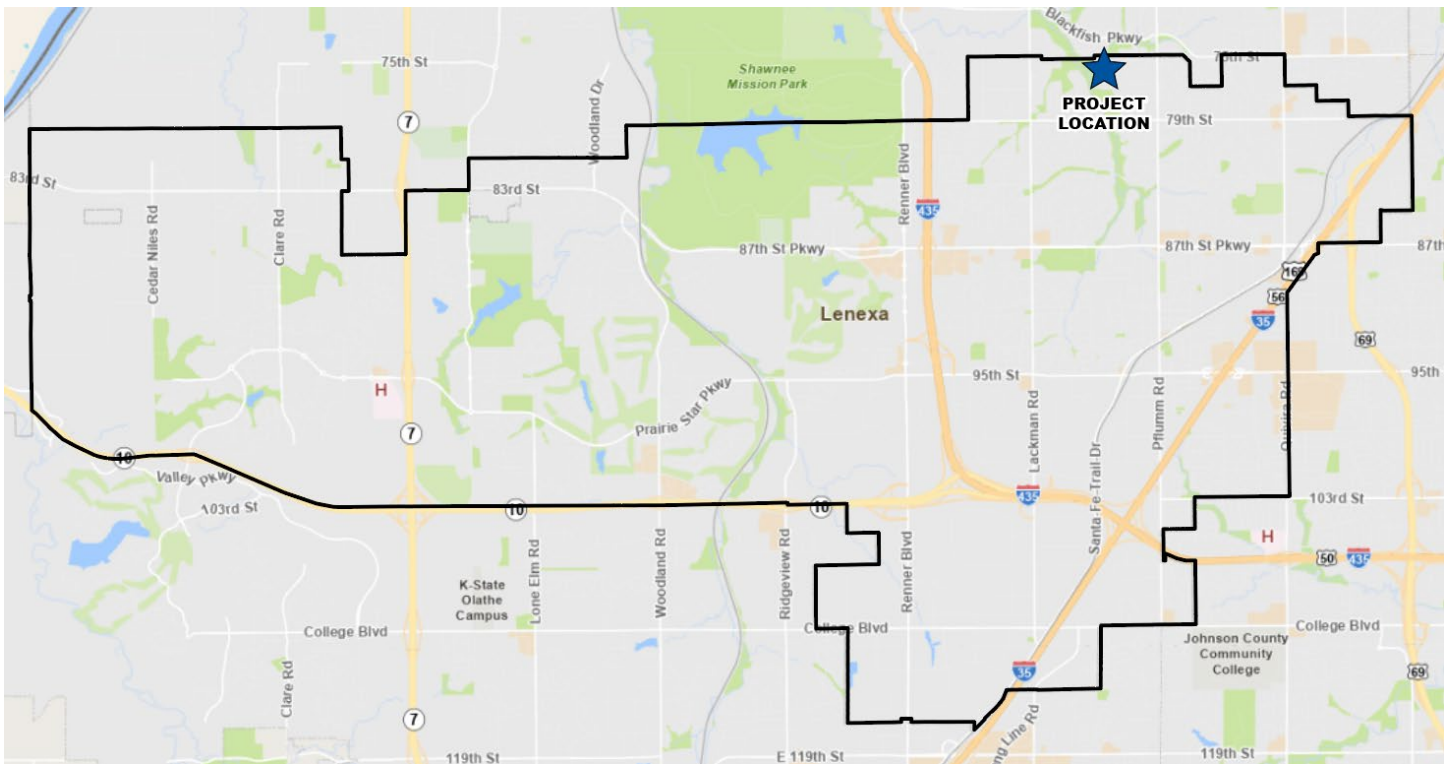
Land Use:	Base Unit Design	Month Adjustment	Peak Hour Adjustment	Noncaptive Adjustment	Mode Adjustment	Total Adjustment	Base Design Indicated Spaces	Actual Parking Demand
Existing Restaurant	0	100.0%					0	
Existing Office	0.00	100.0%					0	
TOTAL EXISTING							0	
New Rooftop Bar	13.35	100.0%	97.8%	90.0%	90.0%	79.2%	35	28
New Retail	4.00	100.0%	0.0%	0.0%	0.0%	0.0%	0	0
New Office	4.00	9.5%	0.5%	90.0%	90.0%	0.0%	28	0
New Residential	1.91	86.4%	78.1%	90.0%	95.0%	57.7%	229	132
							292	160

* MODE ADJUSTMENT adjusts the base parking ratios for local transportation characteristics. Two factors are considered in these adjustments: modal split for private autos and auto occupancy. Even in areas where there is limited transit, walking, drop-offs, and ridesharing occur. The base ratios then are appropriate for conditions of free parking and negligible use of public transit, while the mode adjustment then reflects local transit availability, parking fees, ride sharing programs characteristic of the specific project location. For example, if the mode adjustment equals 75%, this means 75% of people arrive in a private auto and require a parking space, while the remaining 25% arrive without a private auto (carpool, transit, walk, etc.)

* NON CAPTIVE adjustment is an estimate of the percentage of parkers at a land use in a mixed-use development or district who are not already counted as being parked at another of the land uses. For example, when employees of one land use visit a nearby food court or coffee store, there usually is not any additional parking demand generated. This describes a "captive" parked vehicle and does not require an additional parking space. Other vehicles coming to the site specific to a single use are considered "non-captive." As such, if the non-captive ratio equals 70%, this means that 70% of parkers will arrive and park for only a single use (such as an office visit), while the remaining 30% can be considered captive and may use both a restaurant and a retail store while using the same parking space.

LENEXA MUNICIPAL SERVICES STORAGE BUILDING

Project #:	PL26-04FR	Location:	7700 Cottonwood Street
Applicant:	Megan Garrett, City of Lenexa	Project Type:	Revised Final Plan
Staff Planner:	James Molloy	Proposed Use:	Legal Nonconforming Public Service Facility



PROJECT SUMMARY

The applicant seeks approval of a revised final plan and proposes to relocate a 1,200 SF accessory structure from the former Lenexa City Hall site at 12350 W. 87th Street Parkway to the Lenexa Municipal Services Campus at 7700 Cottonwood Street. The structure is currently used for storage of fire apparatus and other Fire Department equipment. Once relocated, this structure will be placed on the western side of the Municipal Services site and is expected to house two street sweepers or other municipal equipment. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

LAND AREA (AC) 74.74	BUILDING AREA (SF) 1,200 SF	CURRENT ZONING R-1	COMP. PLAN Institutional
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The subject property is located along the city limits of Lenexa in the northern part of the city, along West 79th Street. The property has been used as a municipal facility since its initial development. The site was originally a water treatment facility; however, it has since been improved for use by the City as a Municipal Services site. The site has had two significant additions since its use as a Municipal Services facility: the construction of a new Municipal Services office building in 2011 and a new maintenance building in 2019.

TABLE 1: PREVIOUS APPLICATIONS			
Project Number	Application Type	Project Name	Date Approved
PL10-02P	Preliminary Plan	Lenexa Municipal Services Facility	December 21, 2010
PL11-01F	Final Plan	Lenexa Municipal Services Facility	January 3, 2011
PL18-13P	Preliminary Plan	Fleet Maintenance Building	August 21, 2018
PL18-19F	Final Plan	Fleet Maintenance Building	October 1, 2018
**PL22-12F	Final Plan	Lenexa Public Safety Building	June 27, 2022

****PL22-12F was for approval of the subject accessory structure on the former City Hall site at 12350 W. 87th Street Parkway**

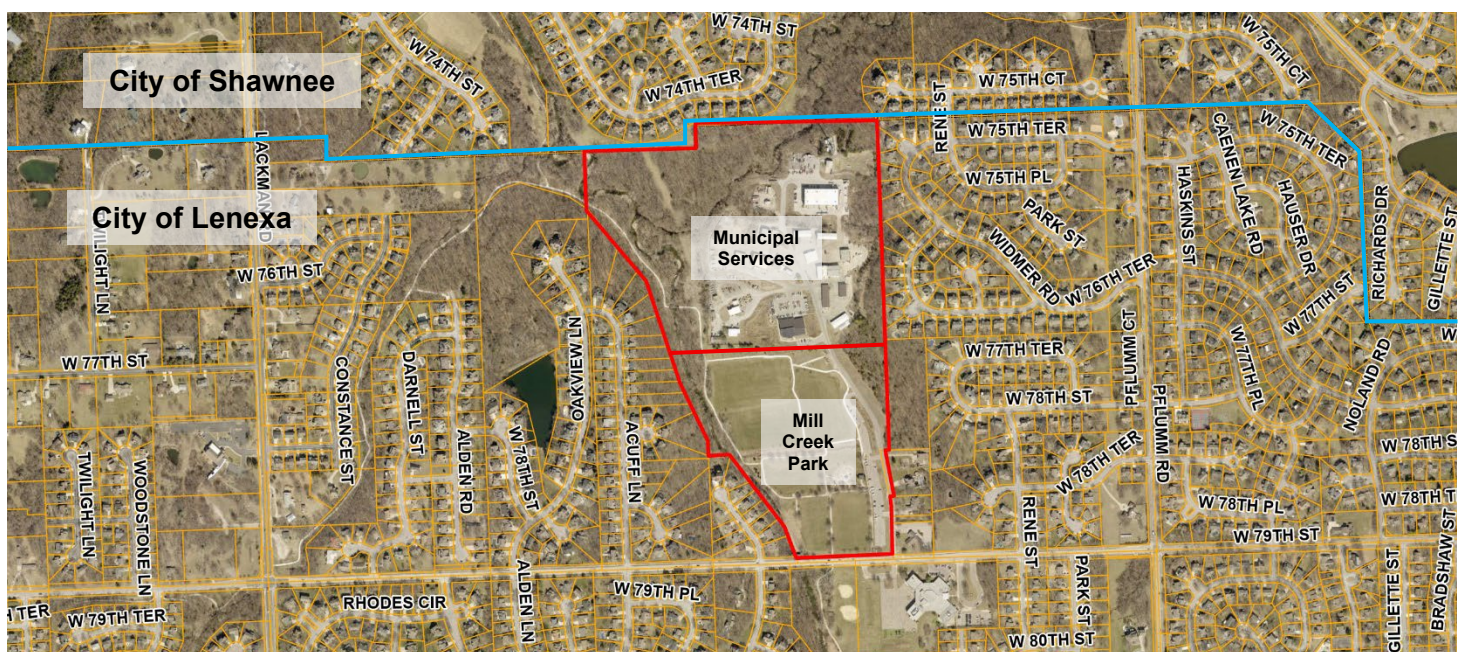


Exhibit 1: Aerial Image of Subject Site

LAND USE REVIEW

The applicant proposes to relocate a 1,200 SF accessory structure on the site to be used as a storage building for street sweepers or other municipal equipment. The property is zoned R-1, Residential Single-Family District, and is designated as “Institutional” on the Comprehensive Plan’s Future Land Use Map. The institutional classification allows for use of the property as a Municipal Services site, which includes buildings for storage such as what is proposed with this plan, provided that the building is screened from public view.

The current R-1 Zoning District does not allow for this type of use as a special or permitted use; however, previous iterations of the R-1 District did allow this facility as a “Publicly Owned and Operated Building” and the site was used for wastewater treatment and other municipal functions under the authority of those iterations of the code. As this site has been operating as a public fleet facility prior to the 1992 code update which precluded such uses in the R-1 District, Staff considers this use to be legally nonconforming and eligible for modifications so long as the use remains established at the property.

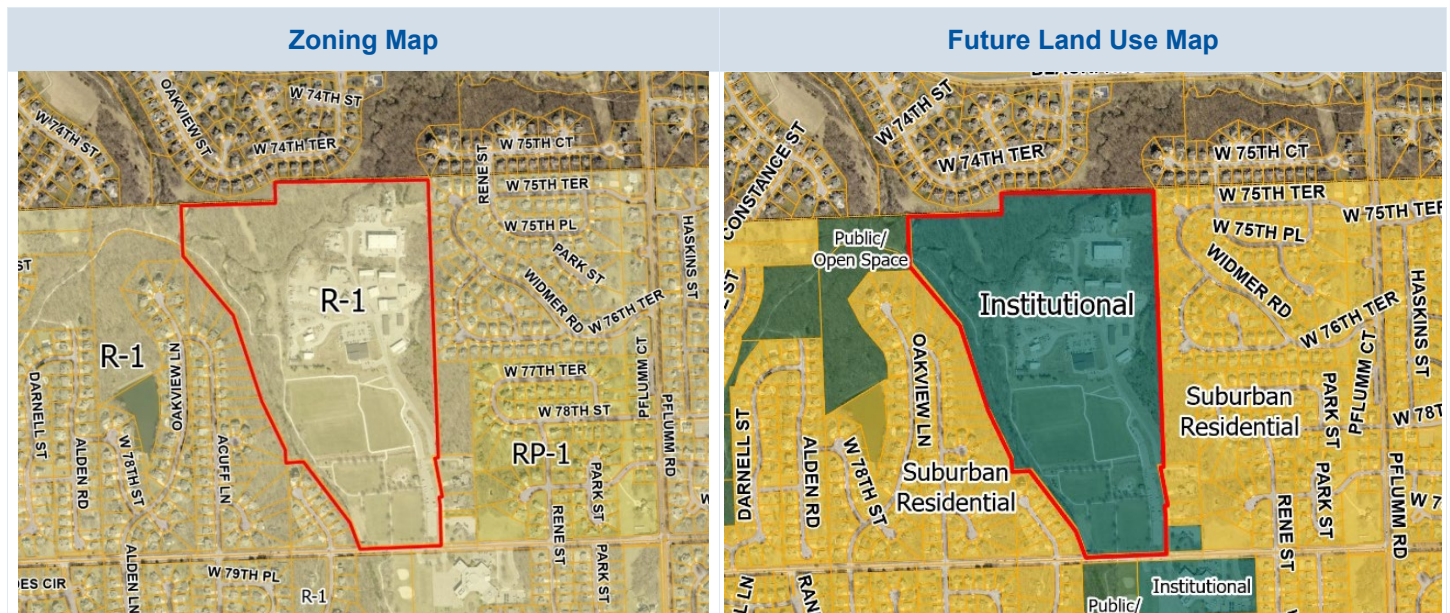


TABLE 2: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Classification	Zoning	Current Use
Subject Property	Institutional	R-1 Residential Single-Family District	Lenexa Municipal Services Campus
North	Suburban-Density Residential	R-1 Residential Single-Family District	Single-Family Residential
South	Institutional	R-1 Residential Single-Family District	Mill Creek Elementary
East	Suburban-Density Residential	R-1 Residential Single-Family District	Single-Family Residential
West	Suburban-Density Residential	R-1 Residential Single-Family District	Single-Family Residential

FINAL PLAN REVIEW

This is a revised final plan application for the relocation of a 1,200 SF accessory structure within the Lenexa Municipal Services Center, which includes fueling facilities, storage, and repair facilities for the City vehicle fleet, as well as storage for items, such as stop lights, traffic control devices, salt and sand, and other items involved with the operation of the City. The storage building is constructed of metal materials and features garage doors on two ends of the structure, suitable to drive vehicles and equipment within the structure.

DIMENSIONAL STANDARDS

The proposed building will be located on the interior of the site, with a setback of roughly 550 feet from the west property line. The storage building is roughly 15' tall, which is similar to many of the other storage buildings, as it was designed to accommodate larger vehicles, such as the fire apparatus currently utilizing the structure. The proposed plan meets both setbacks and height standards of the R-1 Zoning District. At approximately 1,200 SF, this will be one of the smallest buildings on the Municipal Services site, aside from the fueling station canopy (Building J) and a storage building (Building D). The proposed storage building is shown on Exhibit 2 as Building E, "Proposed Morton Building".



Exhibit 2: Site Plan and Information

PUBLIC IMPROVEMENTS

There are no public improvements expected with this project. This project is a relocation of a an accessory structure that will not require the construction or relocation of any roads, trails, or sidewalks.

ACCESS, TRAFFIC, AND PARKING

Access onto the site is from Cottonwood Street, which extends southward to W. 79th Street. There is an interior road network that connects the various office buildings and storage buildings on the site and connects out to Cottonwood Street. There is a trail network which runs through the site and down through Mill Creek Park along the west side of the property. This proposed building will not require additional on-site parking to be provided, as it will not be used as an office, but rather as storage.

STORMWATER

Staff anticipates that there will be little to no impact on stormwater with the addition of the building. The building is being placed in an area that already contains impervious surface. It is expected that existing stormwater measures will be able to accommodate this building.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed code review will be conducted based on the adopted codes at the time of the building permit submittal.

LIGHTING

The storage building includes lighting above the garage doors on either side, as well as above the side entry on the south side of the building. These fixtures include 90-degree cutoffs, which will mitigate the impact of these additional lights. This lighting was reviewed and approved by Staff with the initial approval of the building on the former City Hall site.

LANDSCAPING

There will be no landscaping improvements as part of this project. Existing landscaping flanks the site on three sides. Staff believes that the existing landscaping will provide adequate buffering and screening from this storage building, as well as the site overall.



Exhibit 3: Municipal Services Site Landscaping

ARCHITECTURE

The building is a prefabricated metal building with ribbed steel, brown wainscot, and awning and brown accents. Windows are featured around the building to add additional detail and articulation. The east and west facing doors also feature windows with the intent of addition character to the facade.



Exhibit 4: (Left) Photo of the Storage Structure at Former City Hall.

Metal buildings are generally discouraged within Lenexa; however, Staff is supportive of the proposed design due to the mature landscaping that surrounds the subject site, the location of the storage building on the site, and the additional architectural features on the building. The building is of a higher quality design and aesthetic than some of the older storage buildings found on the Municipal Services campus.

Exhibit 5: (Right) Photo of the Storage Structure at Former City Hall.



Exhibit 6 (Below, Left): Photo of south side of the storage structure.

Exhibit 7 (Below, Right): Elevation of the structure showing a garage door.



NEXT STEPS

- The Planning Commission is the final authority for approval of this project.
- The applicant must receive permit(s) prior to relocating the structure.

RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the revised final plan for City of Lenexa Municipal Services Storage Building.**

- This project is a relocation of the existing accessory structure on the former City Hall site at 12350 W. 87th Street Parkway to the current Municipal Services site at 7700 Cottonwood Street.
- This building is accessory to the site operations that will be utilized to store street sweepers or other municipal equipment.
- The project conforms with the Comprehensive Plan, complies with zoning and subdivision regulations, and provides appropriate utility, stormwater, and site design.
- The project is consistent with Lenexa's goals through *Prudent Financial Management* and *Responsible Economic Development* to create *Vibrant Neighborhoods*.

REVISED FINAL PLAN

Staff recommends **APPROVAL** of the revised final plan for PL26-04FR – **Lenexa Municipal Services Storage Building** at 7700 Cottonwood Street, for an accessory municipal services storage building.



Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Lenexa Municipal Services Storage Building



0 250 500 1,000
Feet



City of Lenexa Municipal Services Preliminary Plan | Site Campus



7700 Cottonwood Dr.
Lenexa, KS 66216
MS Campus Sq. Footage: 1,175,058



Label	Building Name	Sq. Feet
A	Traffic Office	1,931.68
B	Traffic Maintenance	3,122.94
C	Traffic Sign Shop	2,760.28
D	Green Storage	875.87
E	Proposed Morton Building	1,172.96
F	Admin Building	16,635.48
G	Greenhouse	2,028.22
H	East T-Shed	14,083.91
I	Old Vehicle Maintenance Shop	9,613.15

Label	Building Name	Sq. Feet
J	Fuel Station	1,100.29
K	Fuel and Car Wash Facility	2,508.62
L	Salt Storage	10,185.72
M	Prewash Bay	2,655.97
N	East Truck Storage	8,629.67
O	North Barn	4,753.15
P	Facilities Shop	3,348.95
Q	New Vehicle Fleet Building	19,538.66
R	Blood Borne	636.15





To: City of Lenexa Planning Commission
From: Megan Garrett, Municipal Services Project Manager
Subject: Preliminary Plan – Municipal Services Campus
Date: May 27, 2026

Overview

The City of Lenexa Municipal Services Department is submitting a Preliminary Plan for review to document existing conditions at the Municipal Services Campus located at 7700 Cottonwood Street and to present the proposed placement of the Morton Building, currently located at 12350 W. 87th Street Parkway. The proposed relocation will maximize the useful life of a recently constructed public asset while supporting long-term operational needs at the Municipal Services Campus. No significant changes to surrounding land uses or overall campus operations are anticipated as part of this proposal.

Background

The Morton Building serving Fire Station 6 was constructed in late 2022/early 2023 and remains in active use. With the planned redevelopment of the current site, the City determined that relocation represents the most cost-effective and sustainable solution. Demolition of a nearly new building would result in the unnecessary loss of a functional public facility and would not represent efficient stewardship of public resources.

Upon relocation, Municipal Services intends to utilize the building to house two street sweepers. Providing enclosed storage for these vehicles will protect the assets from weather exposure, improve operational readiness, reduce long-term maintenance impacts, and help extend the service life of the equipment.

Proposed Placement

The Preliminary Plan identifies a conceptual location within the Municipal Services Campus that:

- Maintains compatibility with existing operational uses
- Supports efficient internal circulation and site access
- Allows for appropriate utility connections
- Preserves flexibility for future campus improvements

The proposed placement is conceptual and will be further refined during subsequent engineering and design phases.



Next Steps

Following Planning Staff and Planning Commission review, the City will proceed with additional project development and detailed analysis, including:

- Grading and drainage design
- Stormwater management
- Utility coordination
- Final site and architectural design

Recommendation

Staff recommends approval of the Preliminary Plan as submitted, allowing the project to advance into detailed design and permitting. Relocation of the Morton Building will enhance operational efficiency, support responsible stewardship of public infrastructure, preserve the value of an existing City asset, and provide long-term operational benefits to the Municipal Services Campus.

Thank you for your time and consideration.

A handwritten signature in blue ink, appearing to read "M. Garrett".

Megan Garrett
Municipal Services Project Manager

Preliminary Plan Narrative

City of Lenexa Municipal Services Campus

**7700 Cottonwood Street
Lenexa, KS 66216**

This Preliminary Plan is submitted for Planning Commission review to document existing conditions at the City of Lenexa Municipal Services Campus, located at 7700 Cottonwood Street, and to present the proposed placement of the Morton Building planned for relocation from its current site at 12350 W. 87th Street Parkway. The purpose of this narrative is to summarize the project scope, outline the operational need, and demonstrate how the proposed relocation aligns with the City’s long-term facilities strategy and land use objectives.

The Municipal Services Campus currently supports essential City operations, including the Traffic, Stormwater, Facilities, and Streets Divisions, as well as Fleet services, materials storage, and administrative functions. The accompanying Preliminary Plan identifies existing building footprints to provide a clear understanding of current site utilization and operational layout.

It is understood that the Morton Building proposed for relocation previously received Planning Commission approval in 2022 at its current location at 12350 W. 87th Street Parkway. The structure remains an active fire station and currently houses the fire engine serving Fire Station 6. Because the Morton Building was only recently constructed in late 2022/early 2023, relocating and repurposing the facility represents a fiscally responsible approach that preserves a nearly new public asset. The alternative—demolishing the existing structure—would result in the unnecessary loss of a functional, modern building and would not represent an efficient use of public resources.

Upon relocation, Municipal Services anticipates utilizing the Morton Building to house two street sweepers. Providing enclosed storage for these vehicles will protect the assets from weather exposure, improve operational readiness, reduce long-term maintenance impacts, and help extend the service life of the equipment. Relocating the building to the Municipal Services Campus will therefore not only extend the useful life of the structure itself but also support the preservation and efficient operation of critical public works equipment.

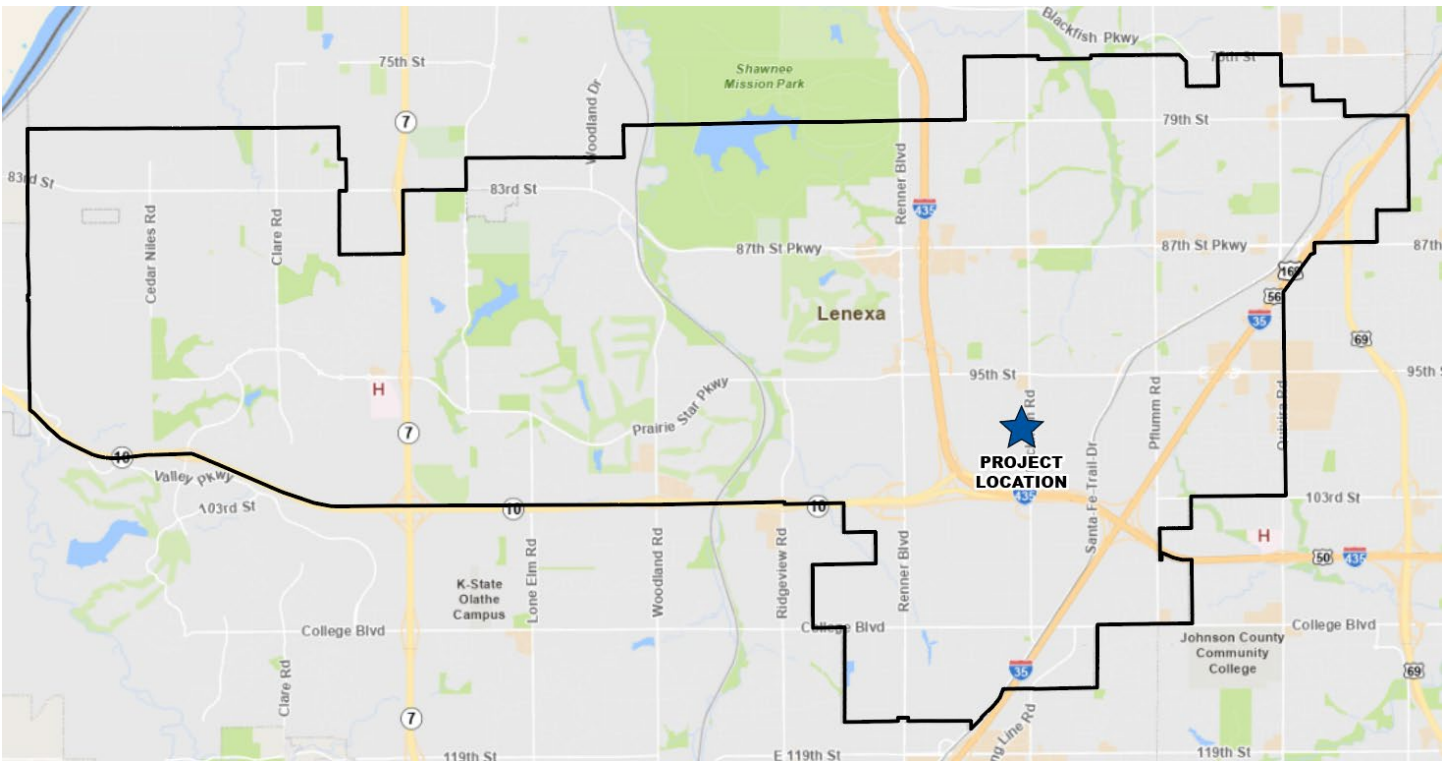
The Preliminary Plan identifies a proposed building location that maintains compatibility with surrounding uses, supports efficient internal circulation, and allows for appropriate utility connections and site access, while preserving flexibility for future campus improvements.

The proposed location is conceptual and does not represent final engineering or architectural design. Detailed analysis, including grading, stormwater management, utility coordination, traffic circulation, and site design, will be completed during subsequent phases of project development. The intent of this Preliminary Plan is to confirm that the relocated structure can be accommodated within the existing campus framework and to support continued coordination with the Planning Department and other reviewing agencies.

The City anticipates that relocating the Morton Building to the Municipal Services Campus will enhance service delivery, streamline operations, and maximize the value of an existing public facility through thoughtful reuse and long-term operational planning.

CMC FOODS & CMC LENEXA

Project #:	PL26-08F, PT26-19F	Location:	9806 Lackman Road
Applicant:	Johnathon Phillips, Davidson A+E, LLC	Project Type:	Final Plan and Final Plat
Staff Planner:	Noah Vaughan	Proposed Use:	Manufacturing, Office, and Warehousing



PROJECT SUMMARY

The applicant requests approval of a final plan and final plat for use by CMC Foods. The site is located at 9806 Lackman Road and will contain manufacturing, office, and warehousing uses. CMC Foods is a commercial food business that specializes in the processing and distribution of food grade oils. The plan includes various changes such as the addition of a southern access point off 99th Street, renovations to the southern portion of the site for oil delivery, installation of a fence, and related improvements to paving and landscaping. The final plan includes a request to defer 148 parking spaces, which requires approval by the Planning Commission. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL



SITE INFORMATION

The subject property is a 12.56-acre site with an existing building and parking area located at northwest corner of 99th Street and Lackman Road, addressed as 9806 Lackman Road. The existing building was constructed in 1978 as a warehouse and office building and has been used historically as such. A railroad spur exists to the west of the building. The site is zoned BP-2, Planned Manufacturing District and is currently unplatted. The proposed manufacturing, office, and warehousing uses are allowed in the BP-2 Zoning District.

At Left, Exhibit 1: Entrance to the existing building on the subject site.

LAND AREA (AC) 12.56	BUILDING AREA (SF) 194,924	CURRENT ZONING BP-2	COMP. PLAN Business Park
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Exhibit 2: Aerial Image of Subject Site.

LAND USE REVIEW

CMC Foods proposes to occupy the site with industrial uses. CMC Foods is involved in the processing and distribution of food grade oils. The building would also house their administrative offices. CMC Foods plans to employ 32 people and would operate from 5:00 AM – 7:00 PM, Monday – Friday. The vicinity of the nearby railway to the building is imperative to business operations of CMC Foods, as they plan to utilize the railway to receive oil deliveries to the location. Oil will also be delivered via tanker trucks; it is then loaded into stalls on the southern side of the site.

There are no conflicts with the proposed uses in the BP-2 Zoning District. The Comprehensive Plan’s Future Land Use Map classifies this site as Business Park. The FLU classification and existing zoning are consistent with the proposed use.

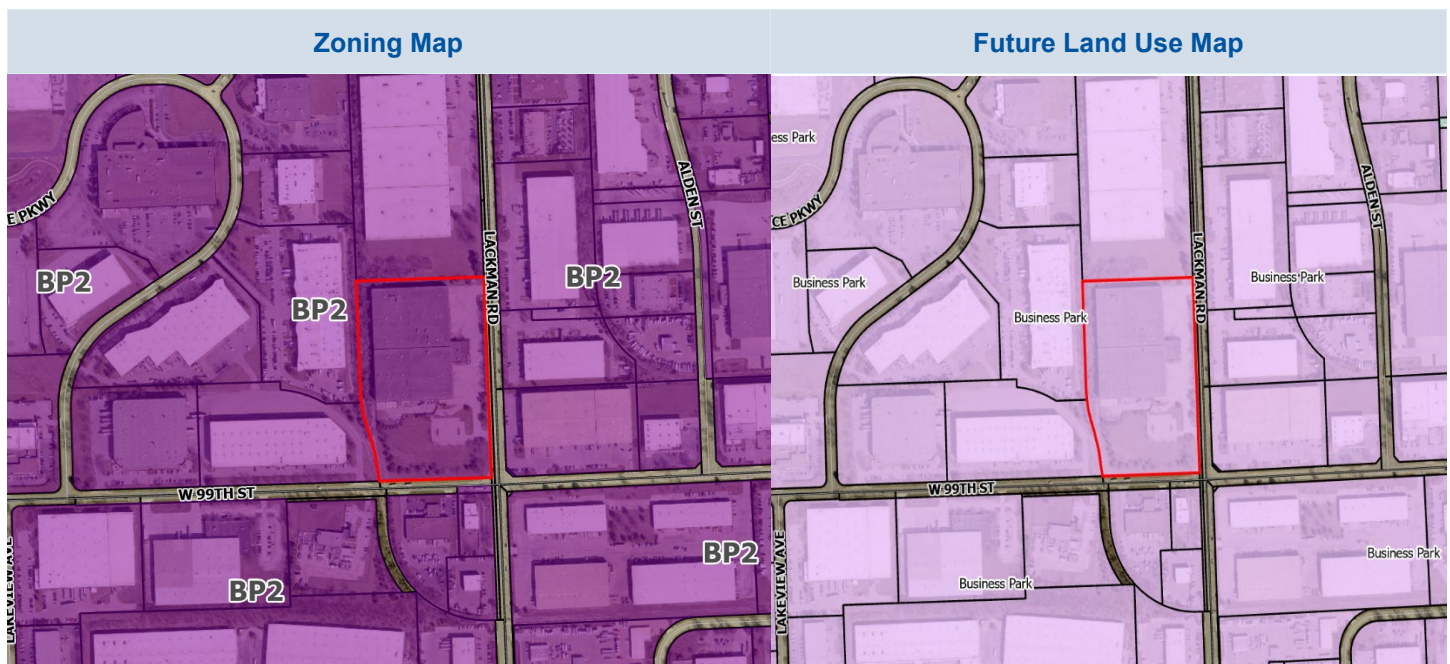


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Classification	Zoning	Current Use
Subject Property	Business Park	BP-2, Planned Manufacturing District	Vacant
North	Business Park	BP-2, Planned Manufacturing District	Office, Manufacturing, Wholesale/Warehousing, Retail
South	Business Park	BP-2, Planned Manufacturing District	Office and Maintenance Services
East	Business Park	BP-2, Planned Manufacturing District	Office, Wholesale/Warehousing, Manufacturing, Maintenance Services
West	Business Park	BP-2, Planned Manufacturing District	Office, Construction Sales and Service, Wholesale/Warehousing, Manufacturing

FINAL PLAN REVIEW

The proposed final plan is for the redevelopment of the site as well as the existing office and warehouse building. The redevelopment includes installation of a new access point and drive off 99th Street into the site, which will be fenced and gated in with a combination of existing and proposed 8' vinyl chain-link fencing. The south façade will serve as the new transfer area for the oil product. Products will be pumped into storage stalls via a hose and pump system; additionally, there will be a new guard shack and chiller unit added to this portion of the site. The existing parking area lies to the east of this new drive and will contain a new sidewalk between the parking and building areas. The new drive will also connect to the east side of the property and the additional access points from Lackman Road. The loading docks on the east façade will remain for other non-oil deliveries and will also be used for shipping out product. Existing parking on the east side will remain along with the existing parking area, both of which will be subject to restriping and spot improvements/repair where necessary. Additionally, CMC Foods will be reactivating the adjacent railroad property, which has not been in use for many years, for transporting product.

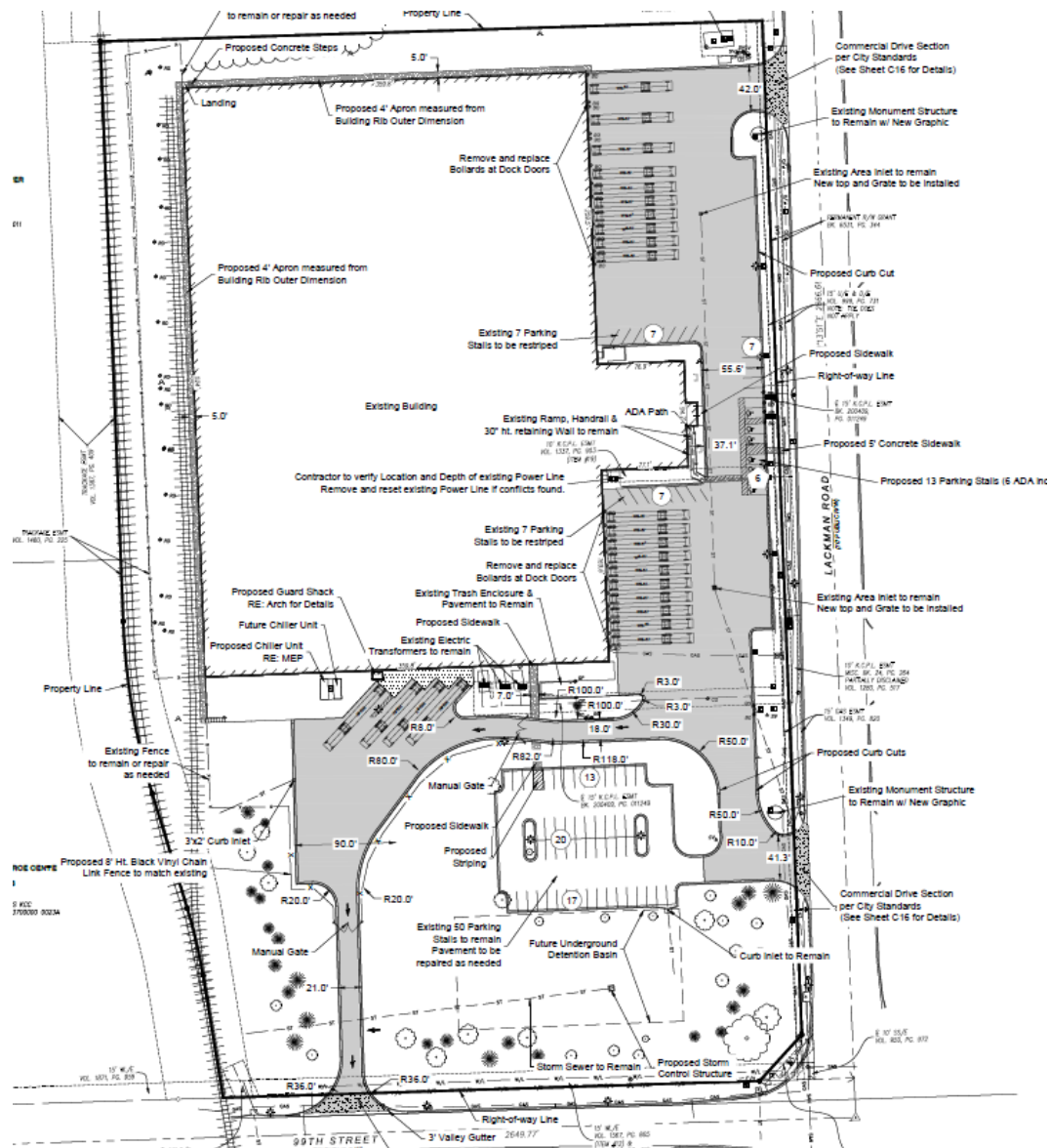


Exhibit 3: Site Plan.

Other changes to the site include a new sidewalk near the eastern parking area to the public sidewalk, a 4' concrete apron that wraps around the west and north sides of the building, and alterations to the existing monument sign on the site to accommodate new graphics. All signs will be subject to separate permit review following decision of the plan and plat. New bollards near the dock doors are also shown on the plan, as well as various curb cuts and site infrastructure improvements are proposed across the property.

The parking area is currently an open concrete lot with various lighting and landscape islands on the south side of the site. The applicant proposes to keep this area as-is with the exception of restriping and repairs to the pavement, as necessary. A new sidewalk will be installed from the existing parking area to the building.

DIMENSIONAL STANDARDS

The existing building meets all setback requirements for the BP-2 Zoning District (30' from the building to property lines, 50' from the building to streets). The minimum open space requirement in the BP-2 Zoning District is 25% and the site will maintain 36.4% of pervious, undeveloped area prior to installation of any deferred parking. Following the potential future installation of the deferred parking, the pervious amount at the site will be 28.5%, which is still in compliance with open space requirements. All other dimensional requirements for the building and parking areas are compliant with the UDC dimensional requirements.

PUBLIC IMPROVEMENTS

There will be no new public improvements required or provided with this proposed project.

ACCESS, TRAFFIC, AND PARKING

As a currently developed site, the site's existing access consists of two drives on the eastern property line off of Lackman Road, a north drive and a south drive. Both of these drives will remain in their current location and widths (as they currently exist), with slight modifications to the inside radius of the south drive to better facilitate truck circulation.

A new access point to 99th Street is proposed along the south property line. This new drive functions as an exit-only drive and will be 20' wide, connecting north into a newly modified truck dock area on the south side of the building. The new access will be to the west of the existing parking area and will require removal of a few landscape buffer trees while the other surrounding landscaping will be expanded.

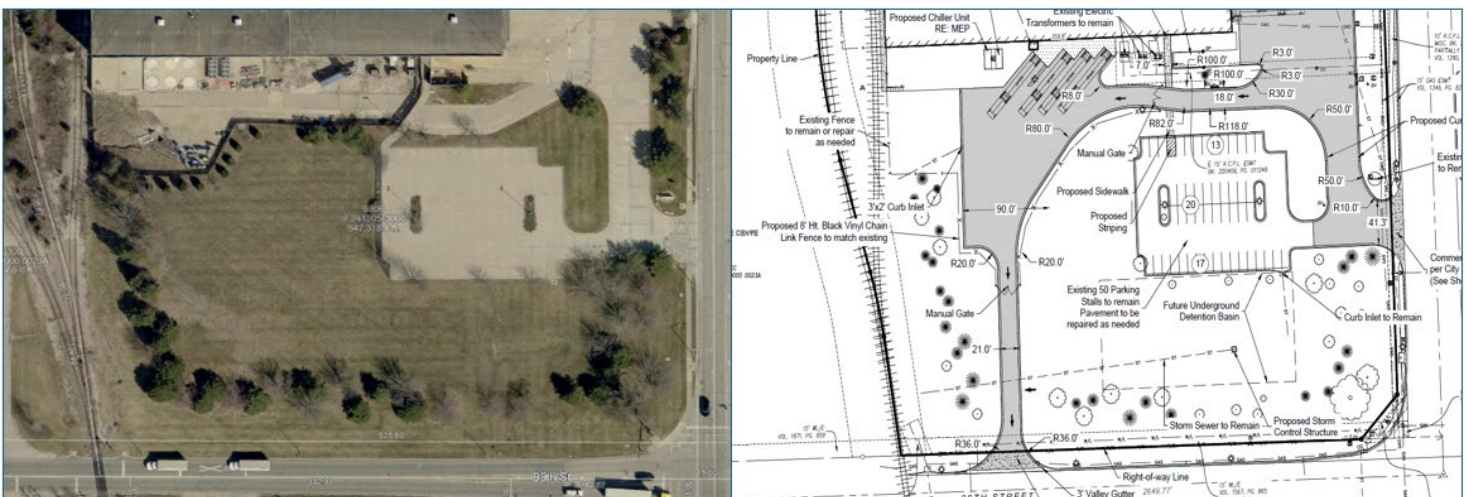


Exhibit 4: Current conditions and proposed plan (showing new 99th Street access point).

As it exists today, the site has two parking areas: an isolated paved lot at the south side of the building with 50 spaces and 27 spaces on the sides of and across from the primary entrance along the east façade of the building. The proposed plan is for the existing parking areas to remain, with any future deferred parking to be installed near the existing southern parking area. Existing parking areas require general maintenance, including pavement repairs and new striping. If the deferred parking area is completed in the future, 148 additional spaces will be provided at the site; however, the changes to the parking area would also reduce the existing parking by 4 spaces, resulting in meeting the exact code requirement. The Planning Commission must approve requests for deferred parking. Staff supports the applicant’s request for 148 deferred parking spaces as CMC Foods will only employ 32 people, and because the business is not open to the general public, the existing spaces will be adequate to serve the parking needs of the business at this site.

TABLE 2: PARKING ANALYSIS				
Use	Requirement	Required	Existing	Proposed
Office	1 space per 200 square feet	83		
Wholesale/Warehousing	1 space per 1,500 square feet	89	77	73 (to remain) 148 (proposed as deferred parking)
Manufacturing	1 space per 1,000 square feet	49		
TOTAL		221	77	221

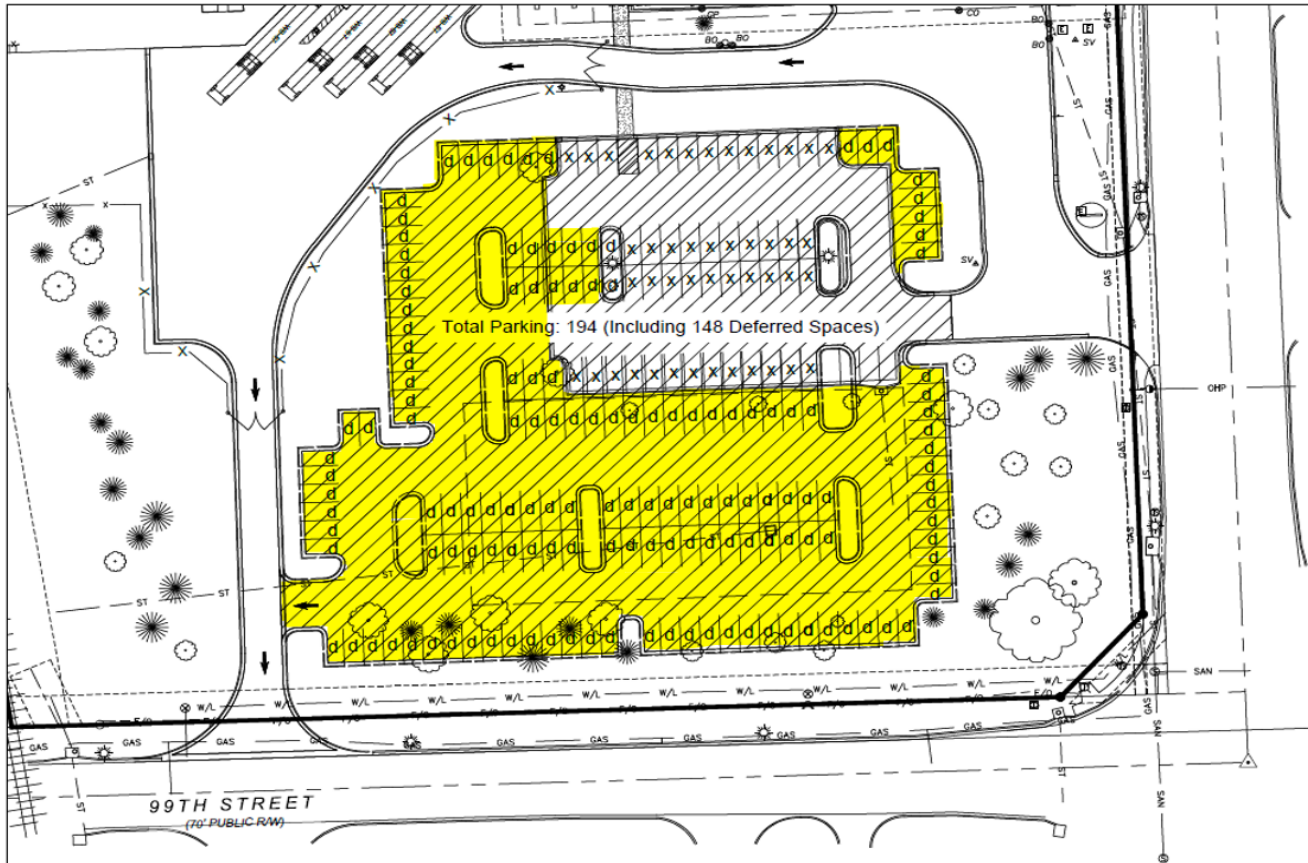


Exhibit 5: Deferred Parking Plan Detail, with proposed deferred parking shown in yellow.

LANDSCAPING

The landscape plan shows a combination of existing and proposed landscaping installed around the perimeter of the site and around the parking areas per the requirements of Section 4-1-D-2-L and M of the UDC. Some existing trees, primarily near the entrance to the building, will be removed; however, the majority of landscaping elements around the perimeter of the site will remain. A total of 9 trees, 84 shrubs, and 52 grasses will be added to the site. The applicant stated that any dead or dying plant material will be replaced by the landscape contractor.

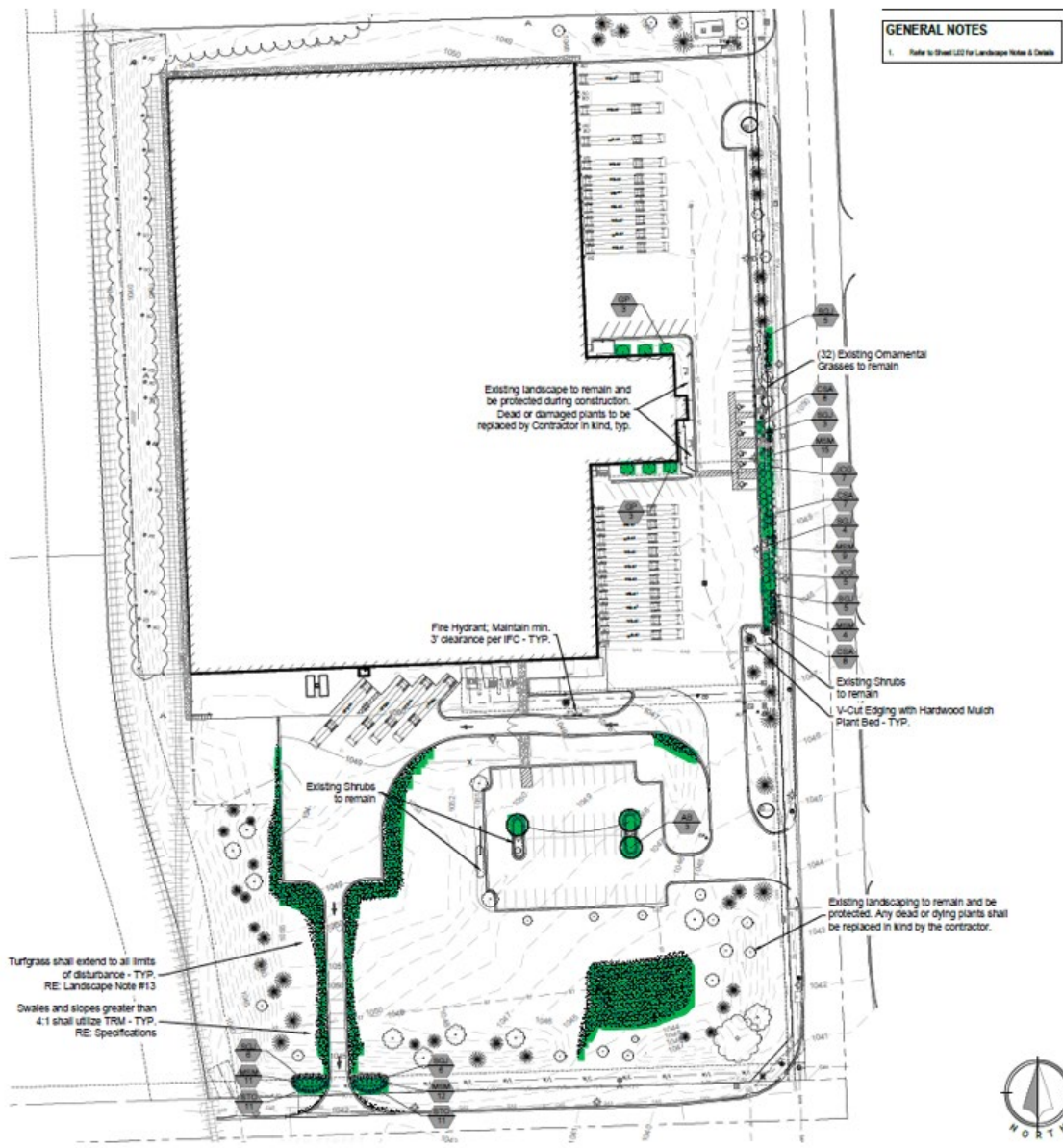


Exhibit 6: Landscape Plan with new landscaping shown in green.

LIGHTING

The site currently contains light poles along the street frontages of 99th Street and Lackman Road, as well as poles within the southern parking area on the existing landscape islands. New lighting will be added along the proposed drive from 99th Street. Any new lighting installed on the site will need to meet the code requirements of 4-1-C-4-I of the UDC. Aside from this proposed lighting, the rest of the lighting on-site will remain as-is.

ARCHITECTURE

The existing one-story building consists of aluminum composite material, pre-cast concrete, and glass; the majority of the building consists of the pre-cast concrete material. The aluminum and glass materials make up the primary entrance to the building along the eastern façade. The building is approximately 32' tall with dock doors on the east and west sides of the building and meets the allowed maximum building height for the BP-2 Zoning District of 45'. The contractors plan to paint the pre-cast concrete light and dark grey, alternating the color scheme on the east and south sides where the building is most visible. The west and north sides of the building will be primarily light grey, except for the western dock doors, which will be the dark grey color.



Exhibit 7: Rendering of the east façade of the building, parallel to Lackman Road.



Exhibit 8: Rendering of the south façade of the building, demonstrating the new entrance from 99th Street.

STORMWATER

The applicant submitted a stormwater study identifying the site's stormwater needs. The site currently has a detention basin within the open area just south of the isolated parking area. With the slight increase in impervious area, the applicant will re-grade and slightly enlarge the existing basin, along with completing modifications to the outflow structure in order to maintain the appropriate release rates as required by City Code.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

SIGNS

The applicant proposes changes to the existing monument signs near the east entrances along Lackman Road. Alterations of these signs to install new graphics will require approval of a separate sign permit prior to the installation of the sign. Signage is not reviewed in detail as part of the final plan or final plat process.

FINAL PLAT REVIEW

The subject site is located at the northwest corner of the intersection of 99th Street and Lackman Road. Lots 17 and 23 of Kansas Commerce Center are directly adjacent to the west of the subject property. The property is currently unplatted. Staff requests platting at times when new projects occur at existing sites and the applicant submitted a final plat in order to bring the site into compliance with platting standards.

Currently, access into the site is only available at two points off of Lackman Road, but an additional access drive is proposed off 99th Street. The final plat will consist of one 12.3-acre lot per the legal description.

The plat does not contain any new dedications for easements or right-of-way. There are multiple existing easements on the property, including utility, gas, and drainage easements along Lackman Road, a waterline easement along 99th Street, and multiple utility easements for Evergy at various points along the site. Additionally, there is a 30'x40' equipment easement at the northeast corner of the site, as well as a trackage easement for the railroad operated by BNSF on the west side of the property. The proposed final plat is in compliance with applicable requirements.

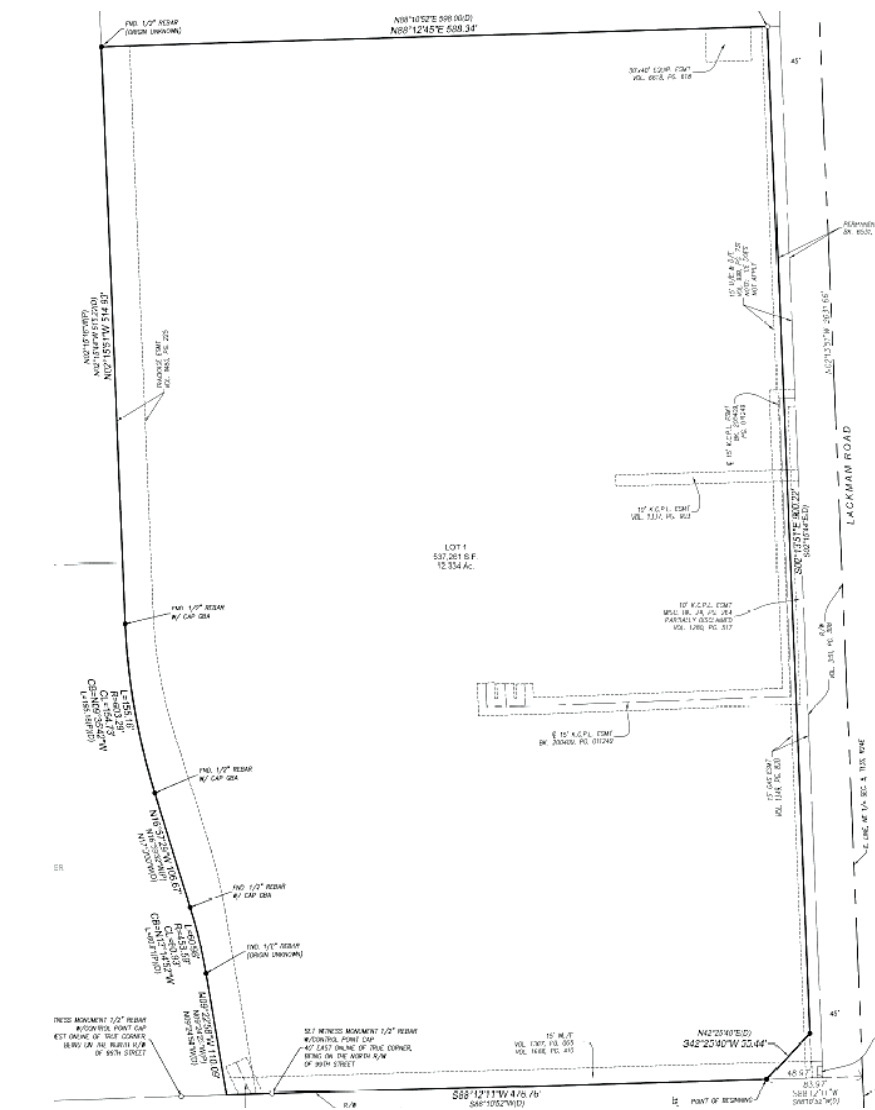


Exhibit 7: Final Plat.

DEVIATIONS

The applicant is not requesting any deviations from the Unified Development Code (UDC).

NEXT STEPS

- The Planning Commission is the final authority for approval of this project.
- The applicant must receive permit(s) prior to commencing construction.
- The applicant must obtain a Certificate of Occupancy before opening for business.
- The applicant must obtain a Business License prior to opening for business.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the final plan and final plat for CMC Foods.**

- This project includes site improvements for an industrial site for office, wholesale/warehousing, and manufacturing uses. Improvements include: addition of a southern access point, renovations to the southern portion of the site for delivery purposes, installation of a fence, and related improvements to paving and landscaping.
- The proposed plans include 148 deferred parking spaces.
- The project is consistent with Lenexa's goals through *Responsible Economic Development* to create a *Thriving Economy*.

FINAL PLAN

Staff recommends **APPROVAL** of the final plan for PL26-08F – **CMC Foods** located at 9806 Lackman Road for office, wholesale/warehousing, and manufacturing uses with deferred parking.

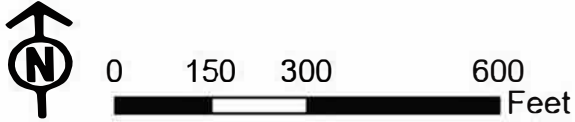
FINAL PLAT

Staff recommends **APPROVAL** of the final plat for PT26-19F – **CMC Lenexa** located at 9806 Lackman Road for an industrial site.



Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

CMC Foods & CMC Lenexa



May 31, 2026

City of Lenexa
Planning & Zoning Division
17101 W. 87t St. Pkwy.
Lenexa, KS 66219

Re: CMC Foods – Project Narrative
9806 Lackman Rd
Lenexa, KS 66219

Planning & Zoning Division:

The project involves the renovation and partial redevelopment of an existing building to accommodate food grade oil processing, distribution, and administrative operations. The work will include interior reconfiguration, structural and exterior architectural modifications, MEP system upgrades and site access/civil improvements to support operational flow and facility compliance with applicable codes and food industry standards.

CMC will operate a maximum of two shifts per day Monday through Friday from 5:00AM to 7:00PM. The current employee count is 32±. CMC is projecting growth at 5-8% annually.

CMC will receive oil deliveries by the existing railway west of the building and tanker trucks. The tanker trucks will enter the property from Lackman Rd and depart onto W 99th St. by way of the new driveway entrance. The tankers will unload at the four stalls and canopy on the south elevation of the building. 5 to 6 tanker deliveries are anticipated each day between the hours 7:00am and noon.

CMC will receive other material deliveries and ship full truckload products from the existing dock doors facing Lackman Rd. CMC anticipates 6 to 14 tractor trailers per day. These trucks will depart onto Lackman. The truck arrivals/departures will be dispersed throughout operating hours.

Sincerely,

Karsten Erdman

Karsten Erdman
Associate Designer
Davidson A + E, LLC

June 22, 2026

Attn: City of Lenexa

RE: CMC Foods Lenexa Campus Parking Deferral

CMC Foods, Inc. specializes in the procurement, packaging, and distribution of edible oils, vinegars, and cooking wines. The company recently acquired the existing approximately 190,000-square-foot facility located at 9806 Lackman Road in Lenexa, Kansas, which has operated under various ownerships and industrial users over the past several decades. CMC Foods took ownership of the property in 2025 and anticipates staffing levels of approximately 30 employees initially, with growth to approximately 60 employees over the next ten years.

The site was originally developed to support a manufacturing, warehouse, and office operation, with approximately 43 parking spaces located along the Lackman Road frontage. Over time, additional parking spaces were incorporated within existing paved areas to accommodate operational needs. In 2006, a separate employee parking lot was constructed south of the building, adding approximately 51 spaces and increasing the total parking supply to 94 spaces.

The existing building will remain approximately 190,000 square feet in size and is proposed to contain the following uses:

- Manufacturing: 89,052 ± SF
- Warehousing: 72,744 ± SF
- Office/Business: 20,750 ± SF

Based on the current requirements of the Lenexa Unified Development Code, a total of 221 parking spaces are required for the proposed uses. However, CMC Foods is requesting approval of 77 parking spaces with a deferral of the remaining 144 required spaces (153 total deferred shown). The requested parking supply is supported by the facility's operational characteristics, anticipated staffing levels, and historical parking demand associated with the property. The proposed 77 spaces are expected to adequately accommodate employees, visitors, and day-to-day business operations for the foreseeable future. Employee parking will continue to be concentrated within the southern parking lot, while visitor, ADA and office staff parking will be located near the primary building entrance.

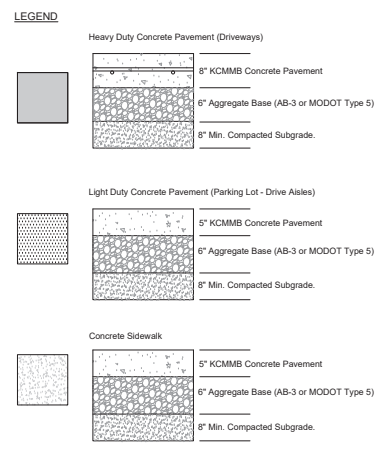
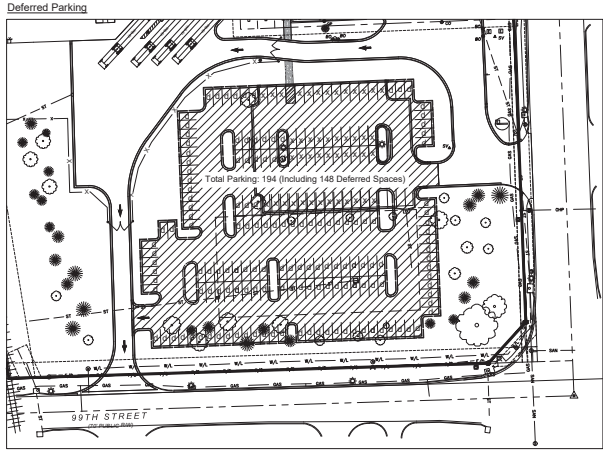
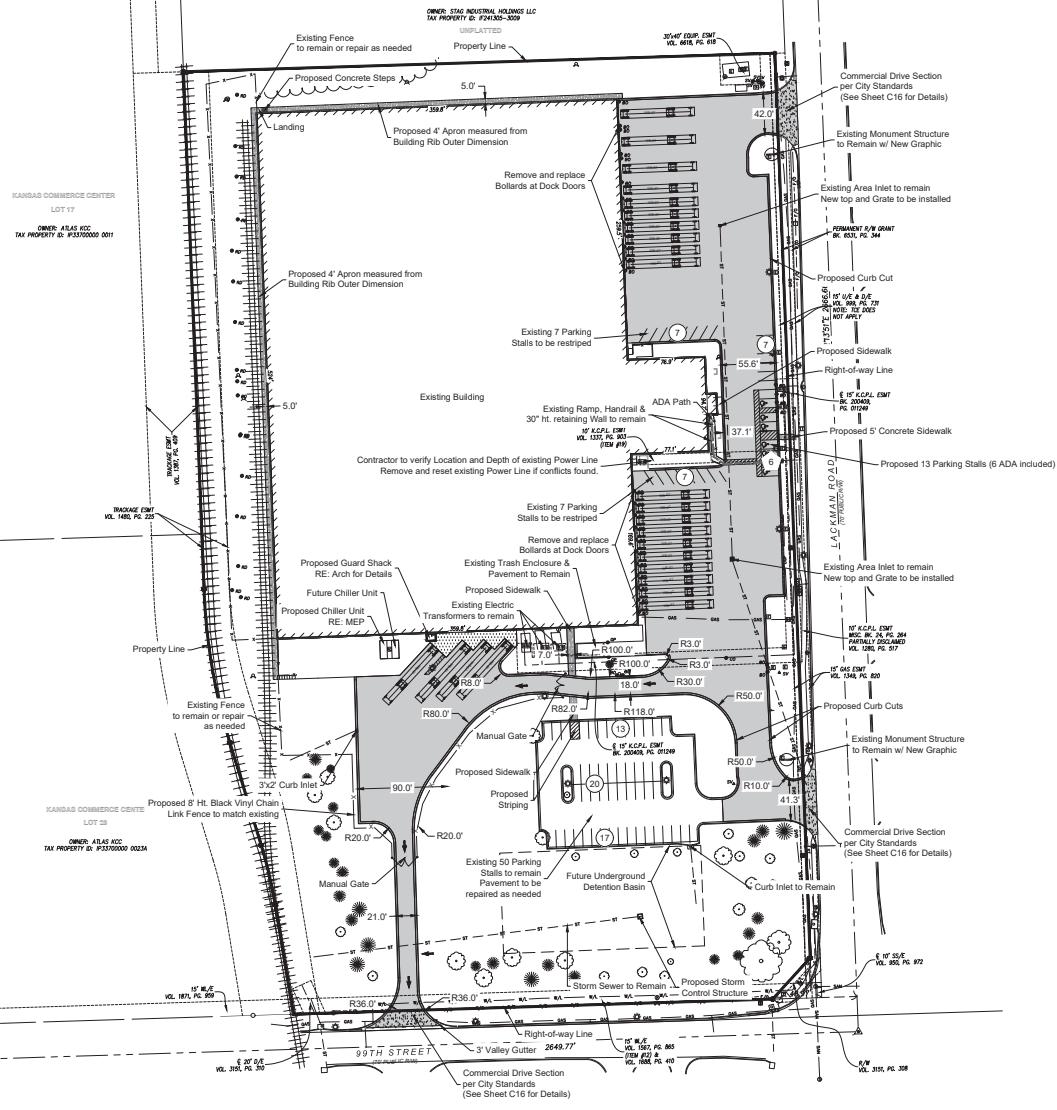
Constructing the full code-required parking supply at this time would result in a substantial number of underutilized parking spaces and an unnecessary increase in impervious surface area. The requested deferral allows the site to develop in a manner that is more consistent with actual parking demand while preserving the ability to accommodate future growth. The Deferred parking is shown on the proposed site plan for reference.

Should the Lenexa Transportation Manager determine that observed parking demand warrants additional parking to mitigate adverse land use impacts resulting from a deficiency of off-street parking, CMC Foods acknowledges and agrees that the deferred parking areas will be constructed by the property owner in accordance with City requirements.

Sincerely,



Andy Gabbert, PLA
Vice President
Renaissance Infrastructure Consulting



PAVEMENT NOTES:
 Certara Geotechnical Report (#14-252536-0) includes Standard Penetration Test results on existing gravel subgrade indicate loose soils (in values 4 to 6), which is not recommended for new pavement. Report recommends stripping to existing clay subgrade, scarifying, moisture conditioning, and compaction at prescribed moisture content and to recommended compaction levels.
 Report includes recommended pavement thicknesses for both asphaltic concrete and portland cement concrete pavement. Report recommends KCMMB concrete for durability reasons.

STORM SEWER NOTES:
 Existing storm sewer system to remain in place. Area inlets in the east lot will receive grade adjustments and likely need to be replaced if their condition is deteriorated.
 The existing Nyoplast basin in the green space south of the building will need to be removed and replaced on the West side of the Drive for a Curb Inlet.
 The existing curb inlet on the south side of the 50-space parking lot will remain in place.

ADDRESS
 9806 Lackman Rd, Lenexa, KS 66219

ZONING INFORMATION
 District: BP2 - Planned Manufacturing District

SETBACKS
 Street: 50'
 Other: 30'

BUILDING INFORMATION
 Office Area: 20,750 S.F.
 Manufacturing Area: 89,052 S.F.
 Warehousing Area: 72,144 S.F.
 Building Footprint Area: +/- 190,000 S.F.

PARKING
 Required Office Parking: 1 per 250 SF = 83
 Required Manufacturing Parking: 1 per 1,000 SF = 89
 Required Warehousing Parking: 1 per 1,500 SF = 49
 Total Required Parking: 221
 Future Impervious Area: 382,884 S.F. - 71.41%
 77 Parking Stalls (6 ADA included)

AREA CALCULATIONS
 Total Building Area: 190,000 S.F. - 43.06%
 Impervious Area(2026): 348,825 S.F. - 63.60%
 Open Space: 212,816 S.F.
 Future Pervious Area: 153,632 S.F. - 28.59%
 Future Impervious Area: 382,884 S.F. - 71.41%
 Net Site Area: 537,261 S.F. or 12.334 Ac.

DEFERRED PARKING
 148 Proposed Parking Stalls
 73 Existing Stalls to remain
 Total Future Parking:
 221 Parking Stalls (6 ADA included)

Parking Count Legend

ADA Stall Count	X
Standard Stall Count	⊗
Existing Stalls	X
Deferred Stalls	d



KANSAS COMMERCE CENTER
 LOT 17
 OWNER: ATLAS KCC
 TAX PROPERTY ID: #13700000 0017

KANSAS COMMERCE CENTER
 LOT 23
 OWNER: ATLAS KCC
 TAX PROPERTY ID: #13700000 0023A

Deferred Parking

Site Development

26-0112
 CMC Foods
 Lenexa, Johnson County, Kansas

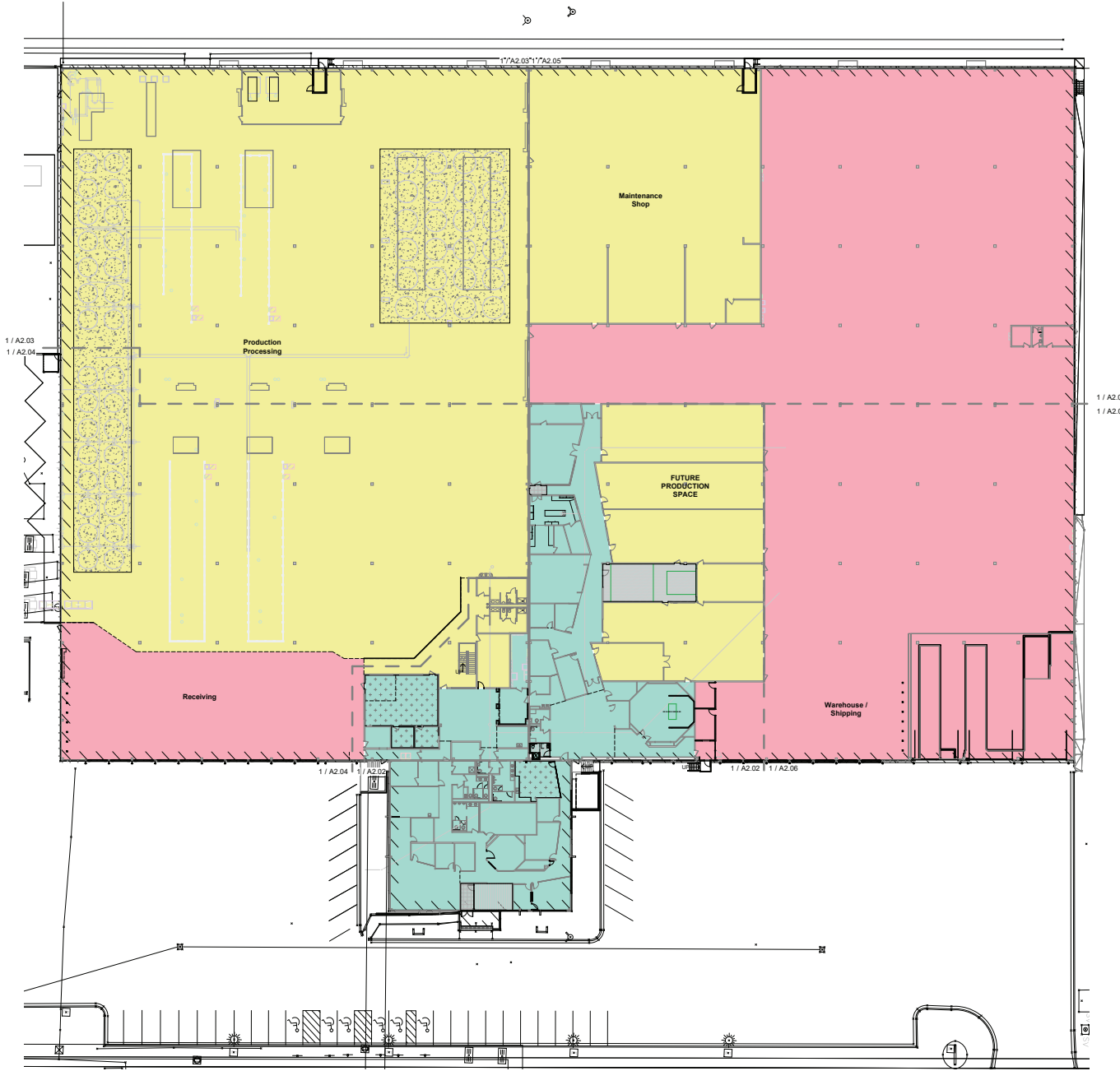
General Layout

NO.	DATE	REVISION

DESIGNED BY: [Signature] CHECKED BY: [Signature]
 DATE: [Date] REVISION: [Date]

Renaissance Infrastructure Consulting
 102 Albee Avenue, Suite 100
 Lenexa, KS 66219
 P: 781.337.0000
 W: www.riconsult.com

Sheet
 C04



project synopsis

Governing Municipality: Lenexa, KS
 Governing Codes: 2018 International Building Code, 2018 International Plumbing Code, 2018 International Mechanical Code, 2018 International Fire Code, 2018 International Residential Code, 2018 International Fuel Gas Code, 2018 International Existing Building Code, 2017 National Electrical Code, 2012 International Energy Conservation Code
 Zoning: BP2 - Planned Manufacturing
 Construction: IB
 Fire Suppression: existing fire alarm / fire sprinkler system shall be modified as required and maintained per NFPA13
 Stories: 1-story
 Building Height: 34'-0"
 Building Area: 194,924 sq. ft. total
 Areas by Use: Manufacturing: 89,052 SF (yellow), Warehousing: 72,744 SF (red), Office/Business: 20,750 SF (green)

© 6/22/2026 10:29:40 AM Autodesk Docs (CMC Foods 25225 CMC Foods Central_A2.01)

1 Overall Floor Plan
scale: 3/64" = 1'-0"



A Tenant Improvement for
CMC Foods
 9806 Lackman Road
 Lenexa, KS 66219

date: 06.18.2026
 drawn by: KWIE
 checked by: JRP
 revisions:

NOT FOR CONSTRUCTION

sheet number
A2.01
 drawing type: Final Plan
 project number: 25225

exterior materials and finishes

- ACM: color per elevations
- precast concrete: color per elevations
- glass: (existing to remain)
- staircase: dark bronze (existing to remain)
- tollow metal doors: color per elevations
- overhead doors: color per elevations
- fashing:
 - prefinished metal cap fashing: color per elevations
- dock seals:
 - dark gray
- bollards:
 - painted - yellow
- fabric awning:
 - color TBD
- railings:
 - painted: color TBD
- caulk:
 - to match adjacent walls



Exposed Aggregate
(existing precast)



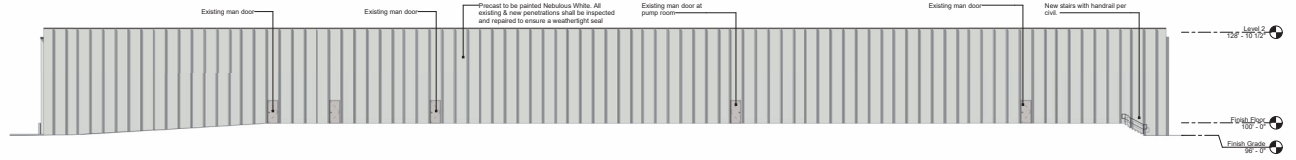
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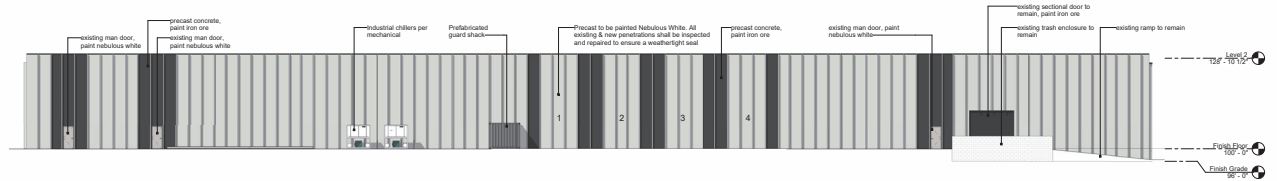
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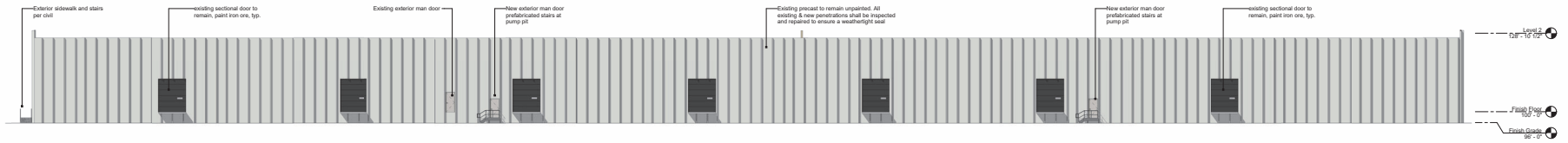
ACM - Slate



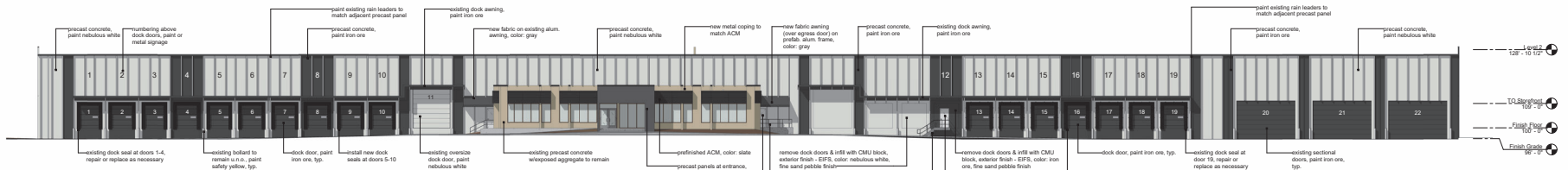
4 | Elevation - North
scale: 1/16" = 1'-0"



3 | Elevation - South (W 99th)
scale: 1/16" = 1'-0"



2 | Elevation - West (Rail Spur)
scale: 1/16" = 1'-0"



1 | Elevation - East (Lackman Rd)
scale: 1/16" = 1'-0"

A Tenant Improvement for
CMC Foods
 9806 Lackman Road
 Lenexa, KS 66219

date: 06.18.2026
 drawn by: DAE
 checked by: DAE
 revisions:

NOT FOR CONSTRUCTION

sheet number
A3.10
 drawing type
 Final Plan
 project number
 25225



A Tenant Improvement for
CMC Foods
9806 Lackman Road
Lenexa, KS 66219

date
06.18.2026
drawn by
DAE
checked by
DAE
revisions

NOT FOR CONSTRUCTION

sheet number
A3.0x
drawing type
Final Plan
project number
25225



© 6/22/2025 10:11:25 AM Autodesk® Revit® 2025: CMC Foods Central_A3.1x

A Tenant Improvement for
CMC Foods
9806 Lackman Road
Lenexa, KS 66219

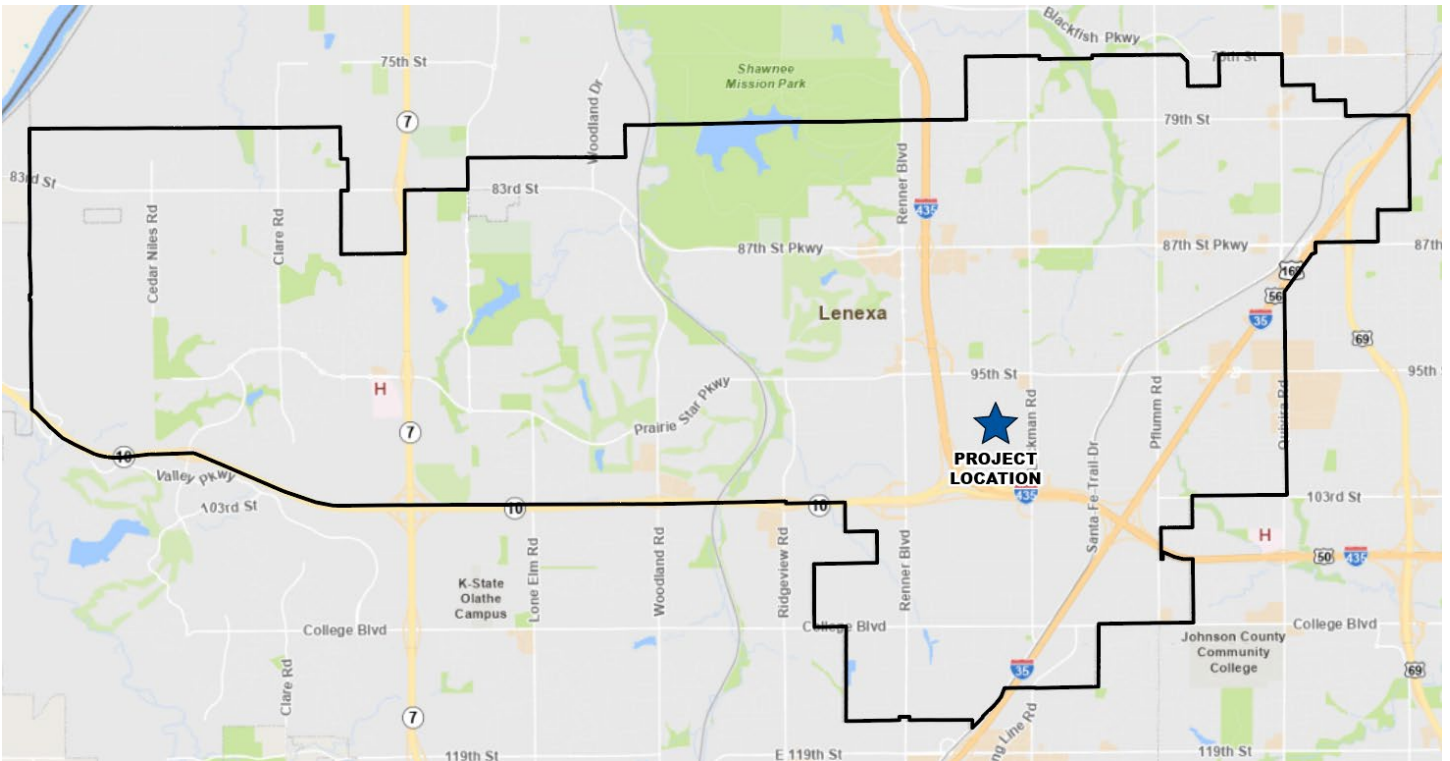
date
06.18.2026
drawn by
DAE
checked by
DAE
revisions

NOT FOR CONSTRUCTION

sheet number
A3.1x
drawing type
Final Plan
project number
25225

KANSAS COMMERCE CENTER, FIFTH PLAT

Project #:	PT26-04FR	Location:	15300 & 15600 W. 99 th Street and 9755 Commerce Parkway
Applicant:	Matt Mitchell, Westfield Co.	Project Type:	Revised Final Plat
Staff Planner:	Jessica Lemanski	Proposed Use:	Warehousing & Manufacturing, General



PROJECT SUMMARY

The applicant is requesting approval of a revised final plat for Lots 17, 22, 23, and part of Lot 11 in the Kansas Commerce Center Plat in order to adjust several lot lines to resolve building encroachments that have occurred over the years. No physical changes to the site are proposed with this application. All physical improvements at the property will become compliant with applicable zoning regulations via this replat. This project does not require a Public Hearing. There are no easements or dedications proposed with this plat.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
24.1	364,958	BP-2	Business Park

These properties are within the Kansas Commerce Center Business Park. Kansas Commerce Center is approximately 135 acres and is bordered by 95th Street on the north, Lackman Road on the east, 99th Street on the south and Loiret Boulevard on the west. The development was platted in 1986 and has had continual construction activity over the course of 40 years. The subject properties are currently developed with three separate warehousing, office and manufacturing buildings and their associated parking, loading docks, and landscaping.

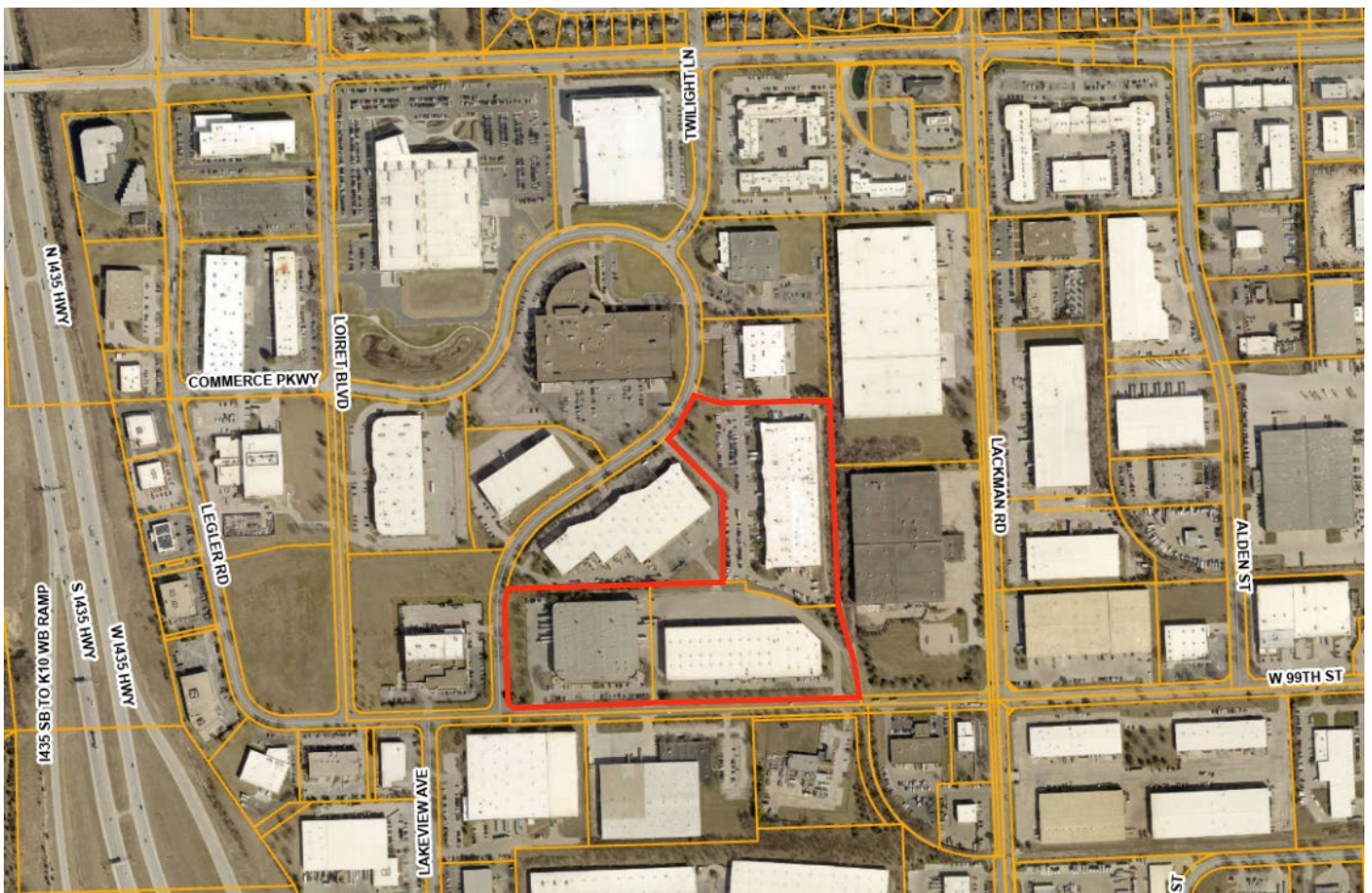


Exhibit 1: Aerial Image of Subject Site

LAND USE REVIEW

The site is zoned BP-2, Planned Manufacturing District, and is designated as a Business Park in the Future Land Use Map. The surrounding properties are zoned BP-2, Planned Manufacturing District and are also designated as Business Park within the Future Land Use Map. Surrounding uses include warehousing, manufacturing, and office spaces. The proposed use for the subject property is not intended to change.

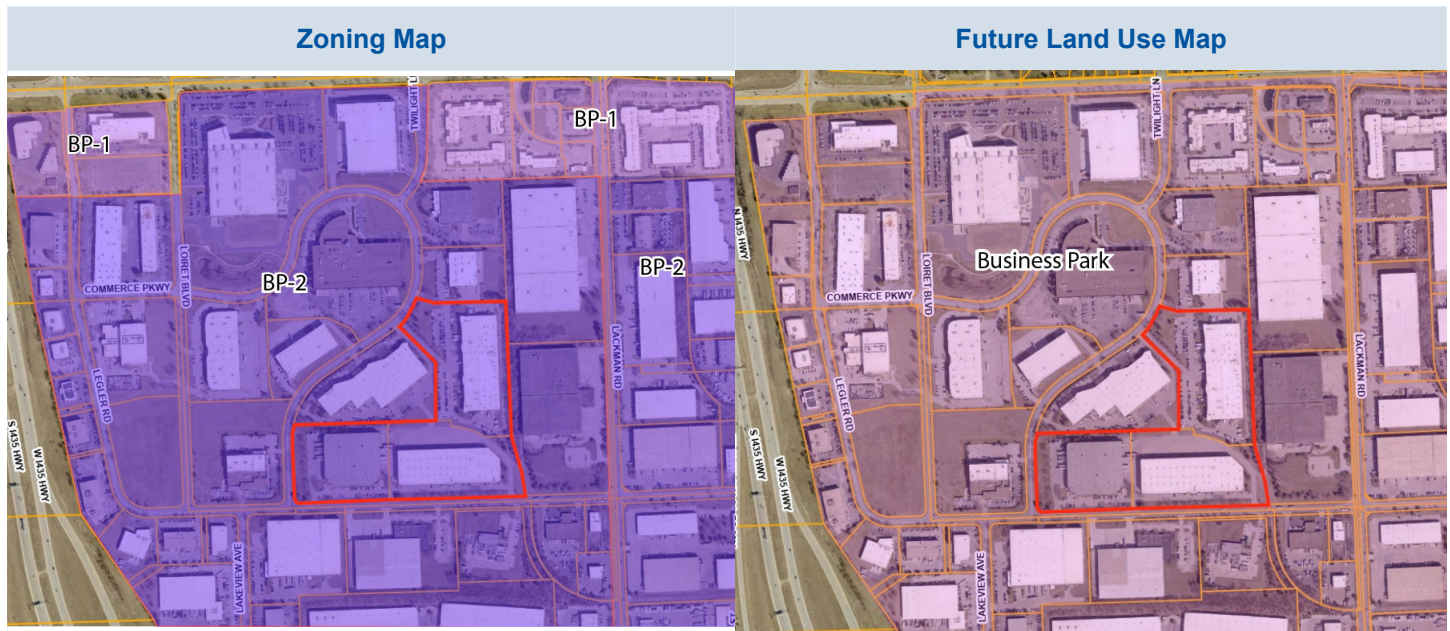


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Classification	Zoning	Current Use
Subject Property	Business Park	BP-2 Planned Manufacturing District	Warehousing and Office
North	Business Park	BP-2 Planned Manufacturing District	Warehousing and Office
South	Business Park	BP-2 Planned Manufacturing District	Warehousing and Office
East	Business Park	BP-2 Planned Manufacturing District	Warehousing and Office
West	Business Park	BP-2 Planned Manufacturing District	Manufacturing and Indoor Storage

REVISED FINAL PLAT REVIEW

Approval of a revised final plat is requested to move the lot line that currently runs through the building on Lots 22 and 23, and to adjust the lot line between Lots 17 and 23. The property was platted in October 1986. The original Kansas Commerce Center Plat is shown in Exhibit 2. The lot line between Lots 22 and 23 currently runs through an existing building.

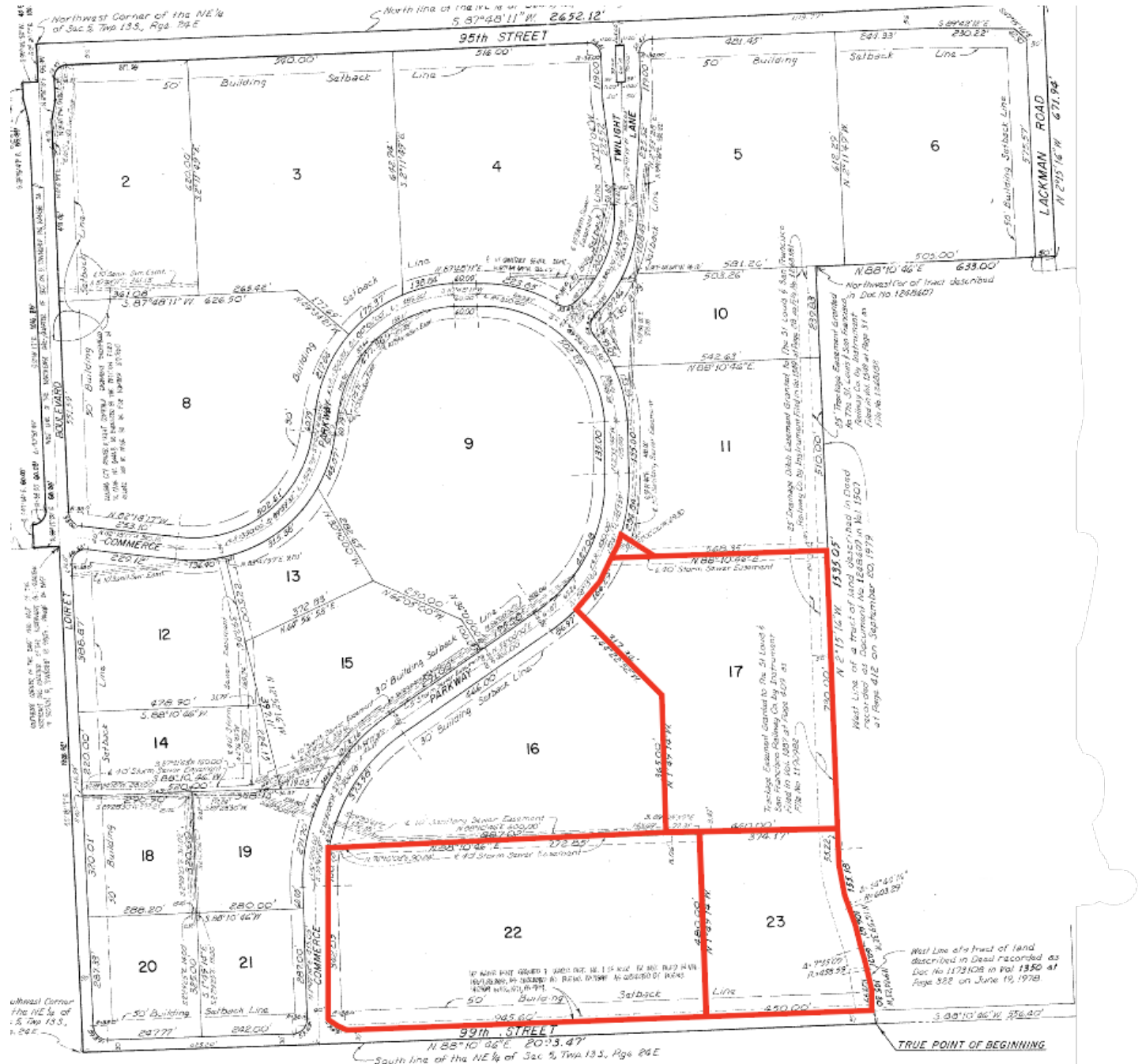


Exhibit 2: Excerpt from Kansas Commerce Center plat (1986)

Proposed Lot 1 (north) will consist of 414,799 SF (9.5 acres). This lot shares access to Commerce Parkway with Lot 11 of the Kansas Commerce Parkway plat and includes a 124,724 SF building with its associated parking and loading dock areas. Proposed Lot 2, which is directly south of Lot 1, will consist of 349,790 SF (8 acres). A two-story 131,113 SF masonry building already exists on the property and is currently bisected by the original lot line between Lots 22 & 23 of the 1986 Kansas Commerce Center plat. Proposed Lot 3, which is situated to the west of Lot 2, will consist of 283,901 SF (6.5 acres). The existing two-story 109,121 SF masonry building on Lot 3 shares parking and drive aisles with Lot 2. Both Lot 2 and 3 take access off W. 99th Street via separate driveways.

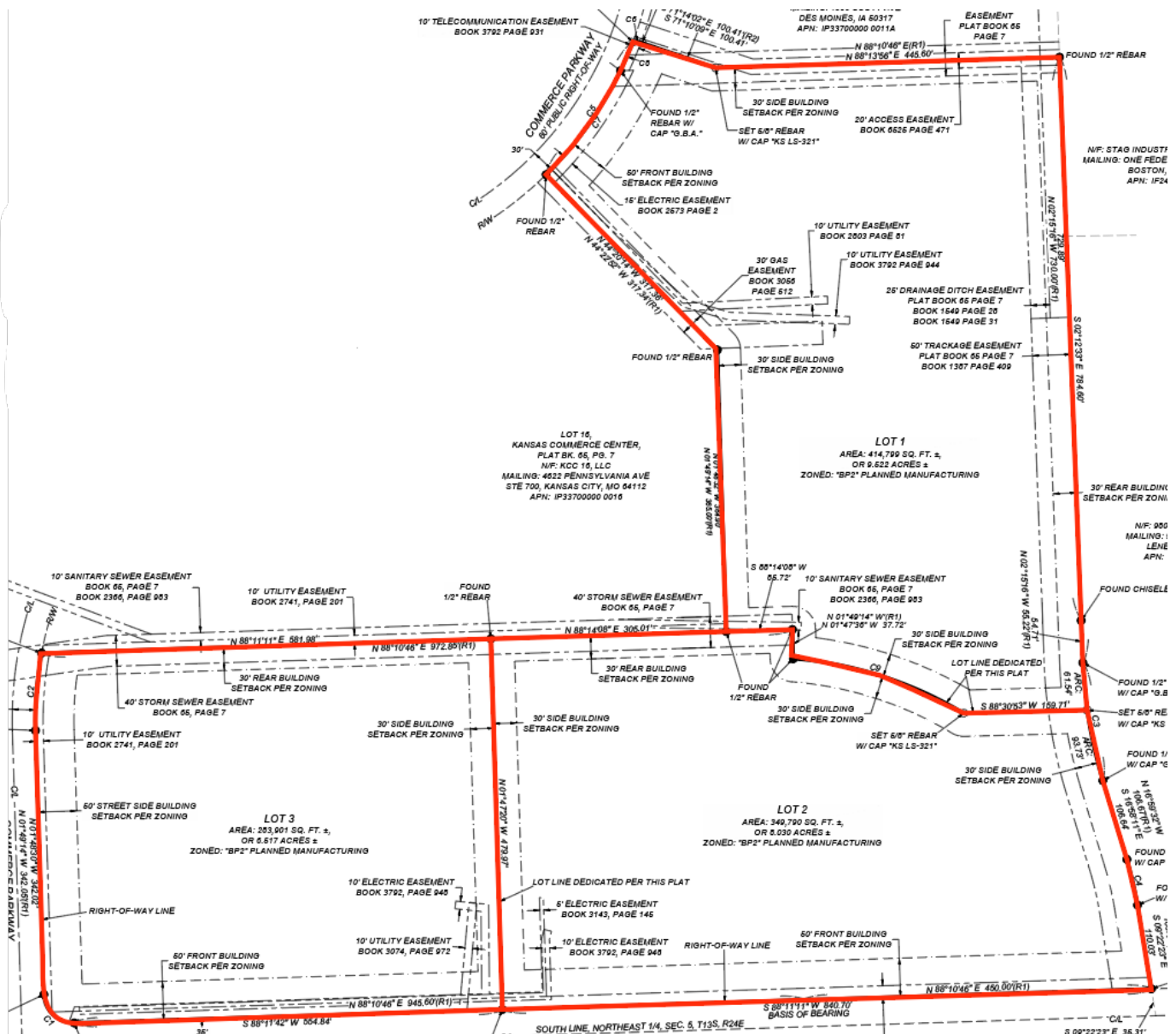


Exhibit 3: Excerpt from Kansas Commerce Center, Fifth Plat (current)

There are no easements or right-of-way proposed to be dedicated to the City with this replat since they were established and accepted with the original Kansas Commerce plat in 1986. There are also no tracts associated with this replat. All existing buildings and parking lot areas are compliant with the dimensional standards for the BP-2, Planned Manufacturing District, and no new nonconformities will be created with this replat.

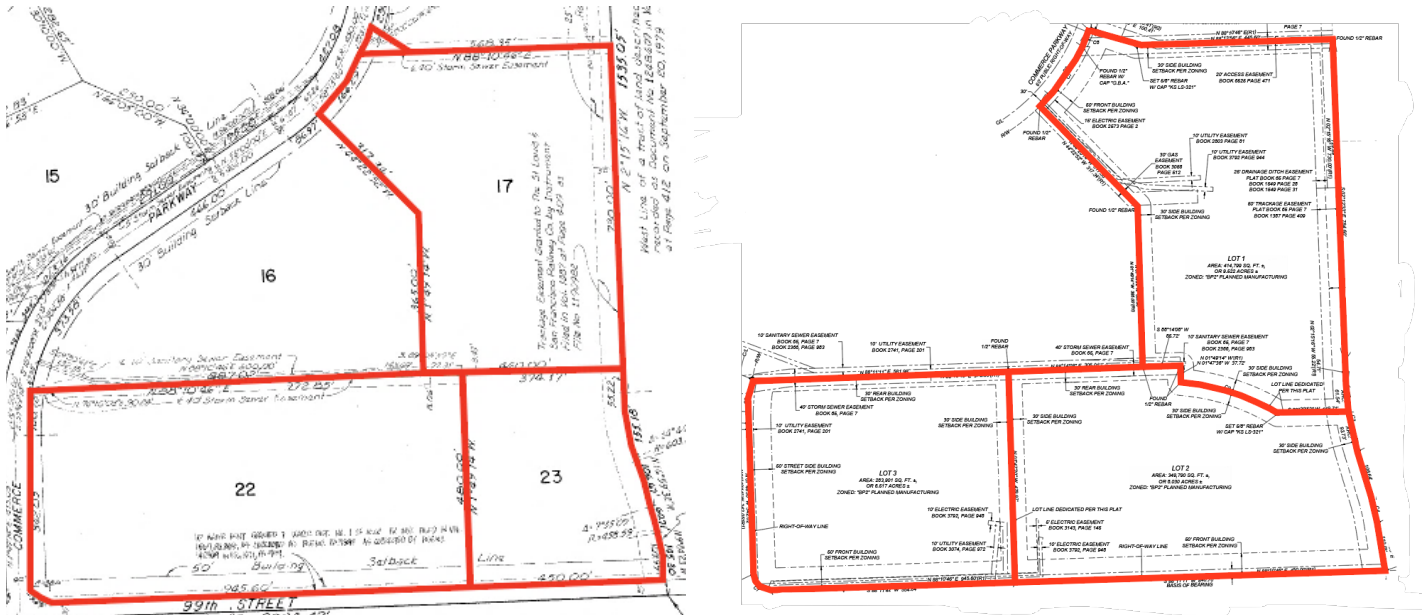


Exhibit 4: Side by side of Kansas Commerce Center Plat (1986) and Kansas Commerce Center, Fifth Plat (2026) showing the lot line adjustments.

DEVIATIONS

The applicant is not requesting any deviations from the Unified Development Code (UDC).

NEXT STEPS

- This project requires approval from the Planning Commission.
- The final plat must be recorded with Johnson County prior to permit(s) being released.
- The applicant should inquire about additional City requirements and development fees.

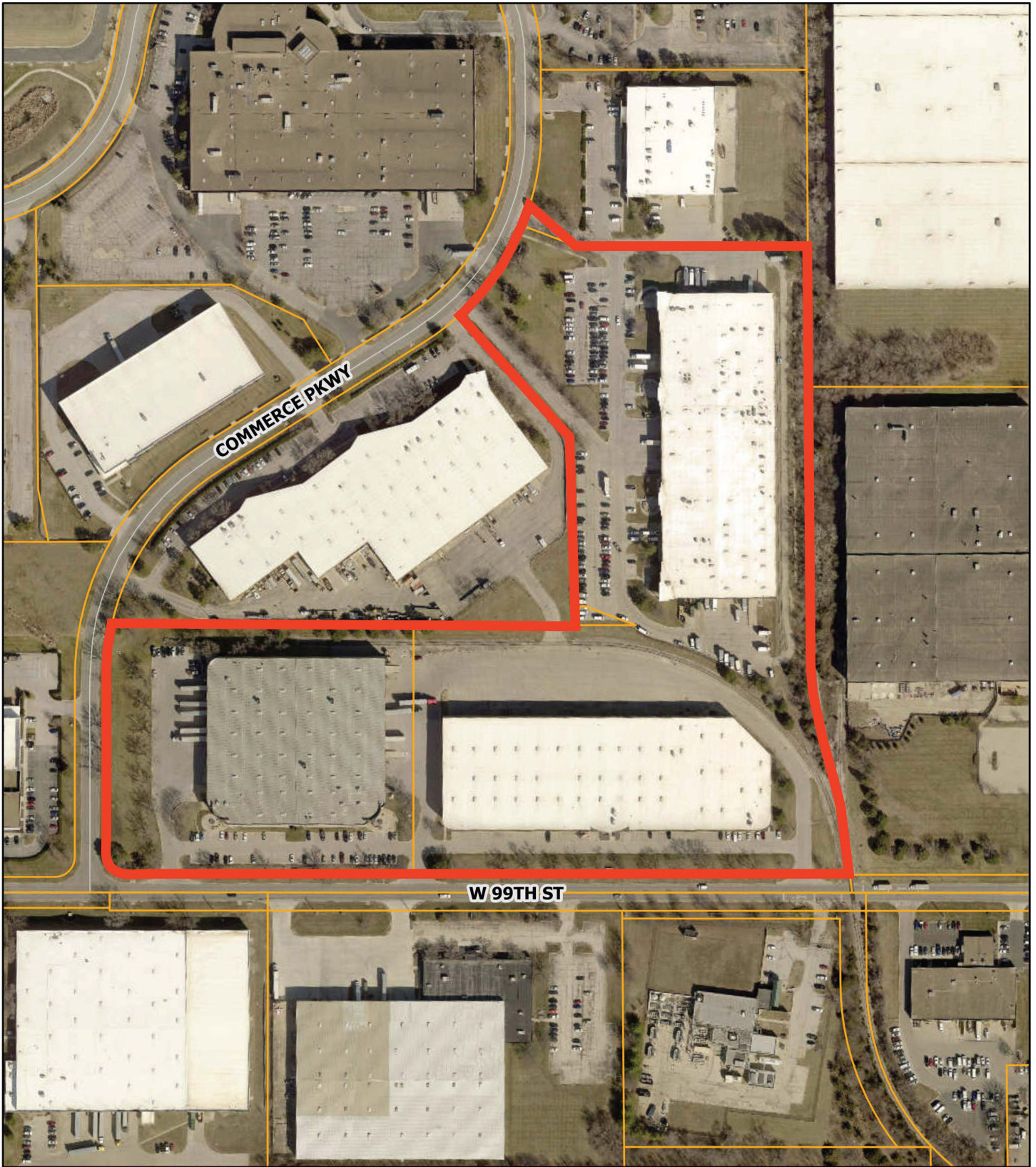
RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the proposed Revised Final Plat for Kansas Commerce Center, Fifth Plat.**

- The project conforms with the Comprehensive Plan and complies with zoning and subdivision regulations.
- No physical changes are requested with this application for the site.
- No new dedications of easements or rights-of-way are proposed with this plat.
- The project is consistent with Lenexa’s goals through **Responsible Economic Development** to create **Thriving Economy**.

REVISED FINAL PLAT

Staff recommends **APPROVAL** of the revised final plat for PT26-04FR – **Kansas Commerce Center, Fifth Plat** at 15300, & 15600 W. 99th Street and 9755 Commerce Parkway, for an existing warehousing and manufacturing development.



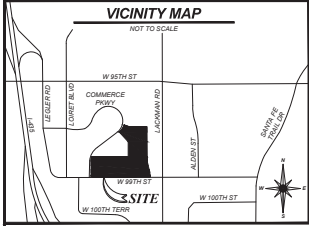
Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Kansas Commerce Center, Fifth Plat



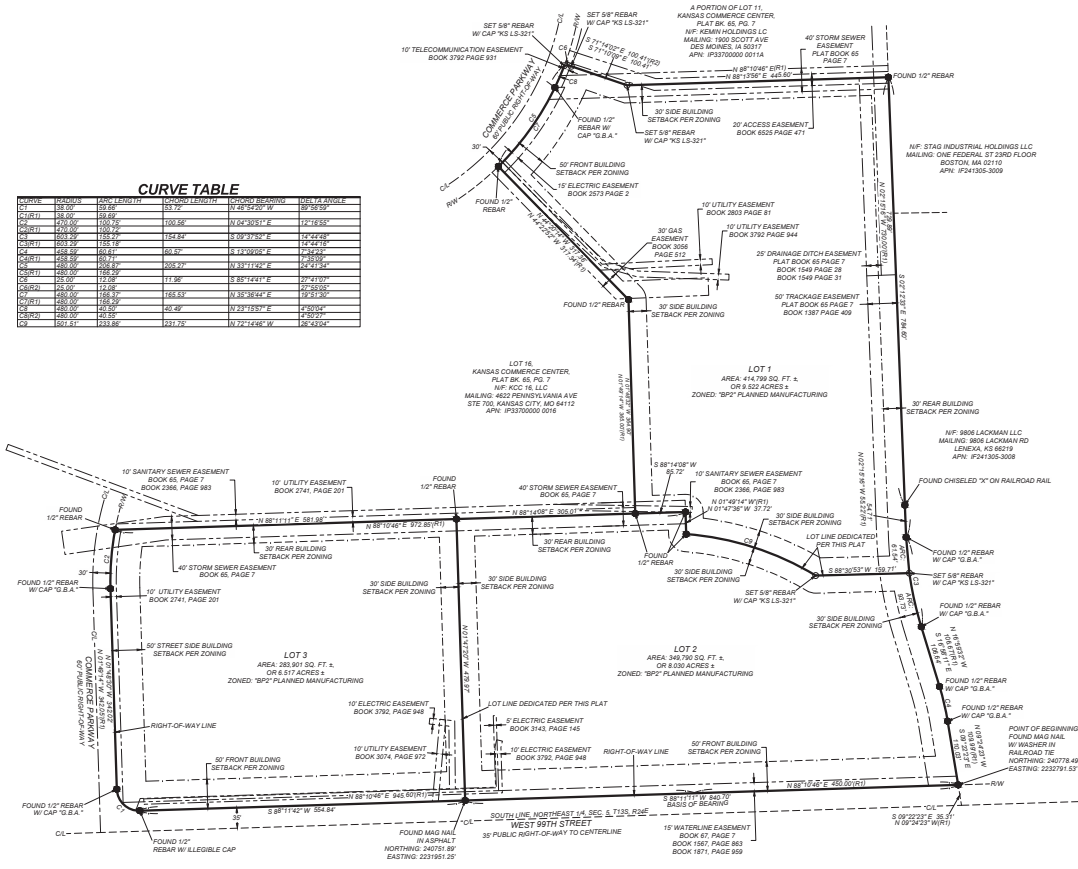
0 175 350 700 Feet





KANSAS COMMERCE CENTER, FIFTH PLAT

A PORTION OF LOTS 17, 22, 23, & A PORTION OF LOT 11 OF KANSAS COMMERCE CENTER
A SUBDIVISION IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS PER PLAT BOOK 65, PAGE 7
LOCATED IN SECTION 5, TOWNSHIP 13 SOUTH, RANGE 24 EAST, 6TH PRINCIPAL MERIDIAN



CURVE TABLE

MARK	ORDINATE	CHORD LENGTH	CHORD BEARING	CHORD BEARING	CHORD BEARING
1	18.00	18.00	0.00°00'00"	0.00°00'00"	0.00°00'00"
2	18.00	18.00	135.00°00'00"	135.00°00'00"	135.00°00'00"
3	18.00	18.00	270.00°00'00"	270.00°00'00"	270.00°00'00"
4	18.00	18.00	0.00°00'00"	0.00°00'00"	0.00°00'00"
5	18.00	18.00	135.00°00'00"	135.00°00'00"	135.00°00'00"
6	18.00	18.00	270.00°00'00"	270.00°00'00"	270.00°00'00"
7	18.00	18.00	0.00°00'00"	0.00°00'00"	0.00°00'00"
8	18.00	18.00	135.00°00'00"	135.00°00'00"	135.00°00'00"
9	18.00	18.00	270.00°00'00"	270.00°00'00"	270.00°00'00"
10	18.00	18.00	0.00°00'00"	0.00°00'00"	0.00°00'00"

LEGEND & SYMBOLS

- FOUND MONUMENT AS NOTED
- SET MONUMENT AS NOTED
- (RT) RECORD DIMENSION PER PLAT BOOK 65, PAGE 7
- (RD) RECORD DIMENSION PER BOOK 202103, PAGE 2383
- NOW OR FOREVER
- BOUNDARY LINE
- EASEMENT LINE
- BUILDING SETBACK PER ZONING
- RIGHT-OF-WAY LINE
- CENTERLINE OF RIGHT-OF-WAY

LEGAL DESCRIPTION

All that tract or parcel of land, being Lots 17, 22, 23, and a portion of Lot 11 of Kansas Commerce Center, a subdivision in the City of Lenexa, Johnson County, Kansas per Plat Book 65, Page 7 of said County Records, being located in the Northeast 1/4 of Section 5, Township 13 South, Range 24 East, 6th Principal Meridian, and being more particularly described as follows:

Beginning at a nail near the corner in a railroad be found for the southeast corner of said Lot 23, said corner being on the north right-of-way line of West 99th Street (35 foot public right-of-way to centerline);

Thence, along said right-of-way line, the following three courses:

- 1) South 88°11'17" East a distance of 462.70 feet to a found nail in asphalt;
- 2) South 88°14'58" East a distance of 306.84 feet to a found 1/2 inch rebar with negligible cap;
- 3) Along a 24.00 foot radius curve to the right for a distance of 138.66 feet to a found 1/2 inch rebar with cap "O.B.A.", said curve having a delta angle of 99°58'59" and a chord bearing and distance of North 47°54'24" West 53.72 feet, and terminating to the east right-of-way line of Commerce Parkway (60 foot public right-of-way);

Thence, along said east right-of-way line of Commerce Parkway the following two (2) courses:

- 1) North 0°49'59" West a distance of 342.02 feet to a found 1/2 inch rebar with cap "O.B.A.";
- 2) Along a 47.00 foot radius curve to the right for a distance of 100.75 feet to a found 1/2 inch rebar, said curve having a delta angle of 12°16'55" and a chord bearing and distance of North 37°51'31" East 100.58 feet;

Thence, beginning said right-of-way line, along the Lot lines of Lot 16, Kansas Commerce Center the following four (4) courses:

- 1) South 88°11'17" East a distance of 85.90 feet to a found 1/2 inch rebar;
- 2) North 88°14'58" East a distance of 306.84 feet to a found 1/2 inch rebar;
- 3) North 0°49'59" West a distance of 342.02 feet to a found 1/2 inch rebar;
- 4) North 42°20'24" West a distance of 317.36 feet to a 1/2 inch rebar on the southeast right-of-way line of said Commerce Parkway;

Thence, along said right-of-way line, also a 480.00 foot radius curve to the left for a distance of 306.87 feet, said curve having a delta angle of 24°47'34" and a chord bearing and distance of North 33°11'42" East a distance of 205.27 feet to a 1/2 inch rebar with cap "O.B.A.";

Thence, beginning said right-of-way line, along the north line of the property described as "TRACT 7" per Book 202103, Page 2383 of said County Records the following two (2) courses:

- 1) Along a 28.00 foot radius curve to the right for a distance of 12.08 feet, said curve having a delta angle of 37°14'07" and a chord bearing and distance of South 80°14'41" East 11.98 feet to a set 5/8 inch rebar with cap "O.B.A.";
- 2) North 0°19'59" East a distance of 105.45 feet to a found 1/2 inch rebar;

3) South 71°02'09" East a distance of 105.45 feet to a set 5/8 inch rebar with cap "O.B.A.";

Thence, along said south line of Lot 11, North 88°13'58" East a distance of 445.60 feet to a found 1/2 inch rebar;

Thence, along the east line of said Lot 11 and continuing along the east line of said Lot 23 the following five (5) courses:

- 1) South 02°22'29" East a distance of 734.69 feet to a found 1/2 inch rebar with cap "O.B.A.";
- 2) Along a 653.29 foot radius curve to the left for a distance of 152.27 feet to a found 1/2 inch rebar with cap "O.B.A.", said curve having a delta angle of 14°44'48" and a chord bearing and distance of South 13°00'00" East 154.84 feet to a found 1/2 inch rebar with cap "O.B.A.";
- 3) South 16°51'11" East a distance of 106.64 feet to a found 1/2 inch rebar with cap "O.B.A.";
- 4) Along a 483.59 foot radius curve to the right for a distance of 60.61 feet to a found 1/2 inch rebar with cap "O.B.A.", said curve having a delta angle of 74°53'01" and a chord bearing and distance of South 13°00'00" East 60.27 feet;
- 5) South 02°22'29" East a distance of 110.03 feet to the Point of Beginning, and containing an area of 1,046.490 square feet or 24.073 Acres, more or less.

BASIS OF BEARING

THE BASIS OF BEARING OF THIS SURVEY IS GRID NORTH BASED ON THE SOUTH LINE OF THE SUBJECT PROPERTY, ALSO BEING THE NORTH RIGHT-OF-WAY LINE OF WEST 99TH STREET; THE BEARING IS DENOTED AS 88°11'17" W PER GPS COORDINATE OBSERVATIONS KANSAS STATE PLANE, ZONE 14N8.

LATITUDE = 38°56'33.194"
 LONGITUDE = -94°49'58.945"
 CONVERGENCE ANGLE = 42°02'38.079"

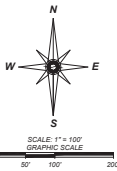
GENERAL NOTES

1. SOME FEATURES SHOWN ON THIS PLAT MAY BE NOTED OUT OF SCALE FOR CLARITY.
2. DIMENSIONS ON THIS PLAT ARE EXPRESSED IN FEET AND DECIMAL PARTS THEREOF UNLESS OTHERWISE NOTED. MONUMENTS WERE FOUND AT POINTS INDICATED.
3. COMPLETED FIELD WORK WAS JUNE 17, 2026.
4. THE DISTANCES SHOWN HEREON ARE GRID, COMBINED SCALE FACTOR (GRID TO GROUND) = 0.99999201.
5. NAMES AND ADDRESSES OF ADJOINING PROPERTY OWNERS WERE TAKEN FROM JOHNSON COUNTY GIS.
6. THE SUBJECT PROPERTY SHOWN HEREON FORMS A MATHEMATICALLY CLOSED POLYGON AND IS CONTIGUOUS WITH THE ADJOINING PUBLIC RIGHT-OF-WAY AND/OR ADJOINING PARCELS WITH NO GAPS OR OVERLAPS.

FLOOD ZONE INFORMATION

BY GRAPHIC FLOODING ONLY, THE PROPERTY IS IN ZONE "X" OF THE FLOOD INSURANCE RATE MAP, COMBINATION PAGE, NO. 200910000, WHICH BEARS AN EFFECTIVE DATE OF 08/03/2009 AND IS NOT IN A SPECIAL FLOOD HAZARD AREA.

ZONE "X" AREA OF MINIMAL FLOOD HAZARD, USUALLY DICTATED ON FIGURE 43 ABOVE THE 500-YEAR FLOOD LEVEL, ZONE "X" IS THE AREA DETERMINED TO BE OUTSIDE THE 500-YEAR FLOOD AND PROTECTED BY LEVEE FROM 100-YEAR FLOOD.



OWNER INFORMATION

LOTS 17, 22, 23, & A PORTION OF LOT 11, KANSAS COMMERCE CENTER
 OWNER: K.C. BUILDING OWNER 22, LLC
 MAILING: 4221 BRIGHTON BOULEVARD, DENVER, COLORADO 80216
 TOTAL AREA: 1,046.490 SQ. FT. OR 24.073 ACRES ±

ON THIS _____ DAY OF _____ 20____, BEFORE ME, A NOTARY PUBLIC IN _____ COUNTY, STATE OF _____, I CAUSE

(K.C. BUILDING OWNER 22, LLC REPRESENTATIVE) TO ME PERSONALLY KNOWN, AND WHO EXECUTED THE FOREGOING INSTRUMENT OF WRITING AND WHO DULY ACKNOWLEDGED THE EXECUTION OF THE SAME TO BE THE FREE ACT AND DEED OF SAID PARTNERSHIP.

NOTARY PUBLIC: MY COMMISSION EXPIRES _____

STATE OF KANSAS
 COUNTY OF JOHNSON

APPROVED BY THE PLANNING COMMISSION OF LENEXA, KANSAS ON THIS _____ DAY OF _____ 20____.

CHRIS POSS
 CHAIRMAN

SURVEYOR'S CERTIFICATE

I, BUCKLEY BLEW, PROFESSIONAL LAND SURVEYOR IN THE STATE OF KANSAS, DO HEREBY CERTIFY THAT A SURVEY WAS MADE UNDER THE SUPERVISION OF THE SURVEYOR DESCRIBED THAT A SURVEY WAS MADE ON THE GROUND IN ACCORDANCE WITH THE MINIMUM STANDARDS FOR BOUNDARY SURVEYS AND MINIMUM STANDARDS FOR MORTGAGE TITLE INSPECTIONS STANDARDS OF PRACTICE AS ADOPTED BY THE STATE BOARD OF TECHNICAL PROFESSIONS AND FURTHER CERTIFY THAT THIS PLAT MEETS SAID STANDARDS AND ACCURATELY REPRESENTS SAID SURVEY.

DATE OF PLAT OR MAP: 06/18/2026

PRELIMINARY

BUCKLEY BLEW
 LICENSED LAND SURVEYOR NO. 1498
 STATE OF KANSAS
 KANSAS C.O.A. LS-321

BLEW

Surveying | Engineering | Environmental

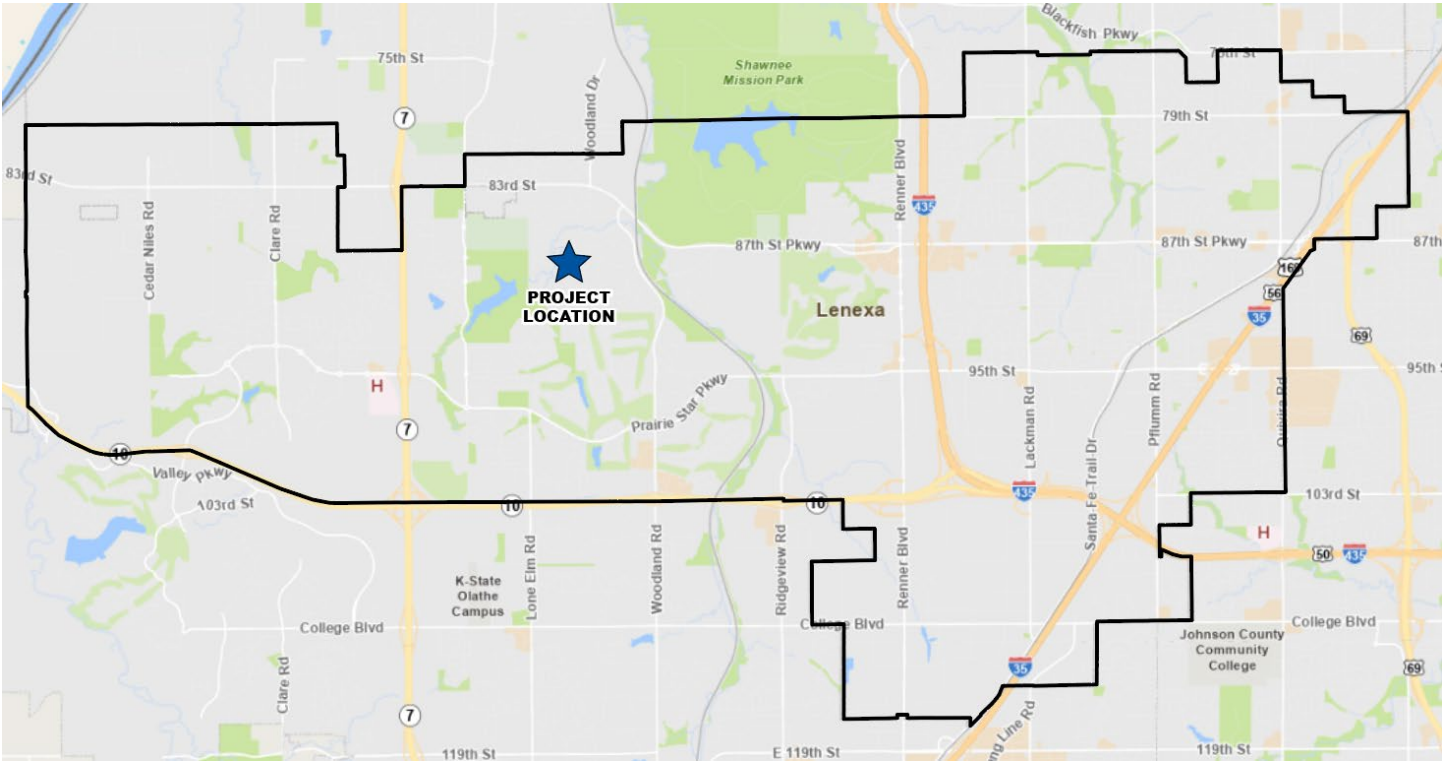
3825 N. SHILOH DRIVE - FAYETTEVILLE, AR 72703
 EMAIL: SURVEY@BLEWINC.COM
 OFFICE: 479.443.4506 FAX: 479.582.1883
 WWW.BLEWINC.COM

DATE	REVISION HISTORY	BY

SURVEYOR JOB NUMBER: 26-5041
 SURVEY DRAWN BY: AUK - 06/18/2026
 SURVEY REVIEWED BY: AUK
 SHEET: 1 OF 1
 SURVEY PREPARED FOR: WESTFIELD COMPANY, INC.

WILSON ACCESSORY STRUCTURE

Project #:	DV26-03	Location:	8720 Old Trail Road
Applicant:	Curtis Befort, Befort Construction	Project Type:	Deviation
Staff Planner:	Oliver Zigmund	Proposed Use:	Single-Family Residential



PROJECT SUMMARY

The applicant proposes to construct a 775 SF accessory structure at an existing single-family residence at an 8' setback from the east (side) property line where a 25' side yard setback is required. The property is located at 8720 Old Trail Road within the Whispering Hills Estates subdivision. The applicant is requesting a deviation from Section 4-1-B-5-F of the Unified Development Code (UDC) to allow a 17' encroachment into the required 25' minimum setback from the property line. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
1.08	Existing: 2,367 Proposed: 775 Total: 3,142	RP-E	Low-Density Residential

The subject property is within the Whispering Hills Estates, Section 2 subdivision and contains a 2,367 SF single-family residence. The property is zoned RP-E, Planned Residential Estate District.

Whispering Hills Estates was platted in 1986, while the existing single-family residence was constructed in 1994. Upon creation of the new zoning code in 1992, the property was designated as RP-E, a zoning district which has a larger setback requirement than a traditional single-family subdivision, such as the RP-1 Zoning District.

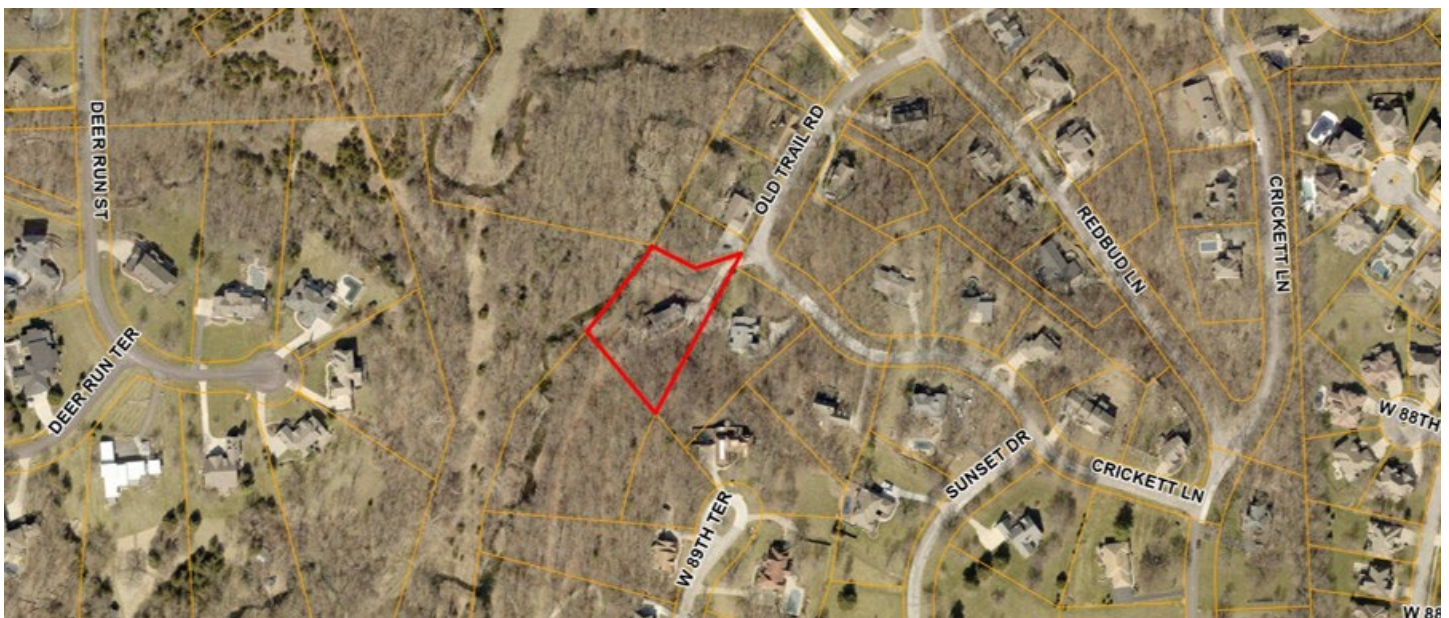


Exhibit 1: Aerial Image of Subject Site.



Exhibits 2 and 3: Site Photos, Dated June 5, 2026

LAND USE REVIEW

The subject property is zoned RP-E, Planned Residential Estate District, and designated as low-density residential on the Future Land Use Map within the Comprehensive Plan. The applicant proposes to construct a 775 SF accessory structure adjacent to the existing 2,367 SF single-family residence on the site. The proposed use is compatible with the surrounding zoning designations, land uses, and the future land use classification.



TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Classification	Zoning	Current Use
Subject Property	Low-Density Residential	RP-E Residential Planned Estate District	Single-Family Residential
North	Low-Density Residential	RP-E Residential Planned Estate District	Single-Family Residential
South	Low-Density Residential	RP-E Residential Planned Estate District	Single-Family Residential
East	Low-Density Residential	RP-E Residential Planned Estate District	Single-Family Residential
West	Public / Open Space	RP-E Residential Planned Estate District	Undeveloped

DEVIATION REQUEST

The applicant is seeking approval of a 17' deviation from Section [4-1-B-5-F](#) of the Unified Development Code (UDC) to construct a 775 SF accessory structure with a covered walkway adjacent to the existing single-family residence at an 8' setback, where a 25' side yard setback is required. The proposed accessory structure is located at the east side of the house, adjacent to the existing garage. The new structure will allow the homeowner more space for vehicle storage and maneuvering. Staff notes that the proposed garage is considered an accessory structure rather than an addition because the proposed garage will not be connected to the home via an enclosed building. Instead, the proposed garage includes a covered walkway connecting to the principal structure.

TABLE 2: DEVIATION REQUEST

Deviation Type	Zoning District	Code Requirement	Proposed	Deviation Request
Side Yard Setback	RP-E	25 ft	8 ft	17 ft

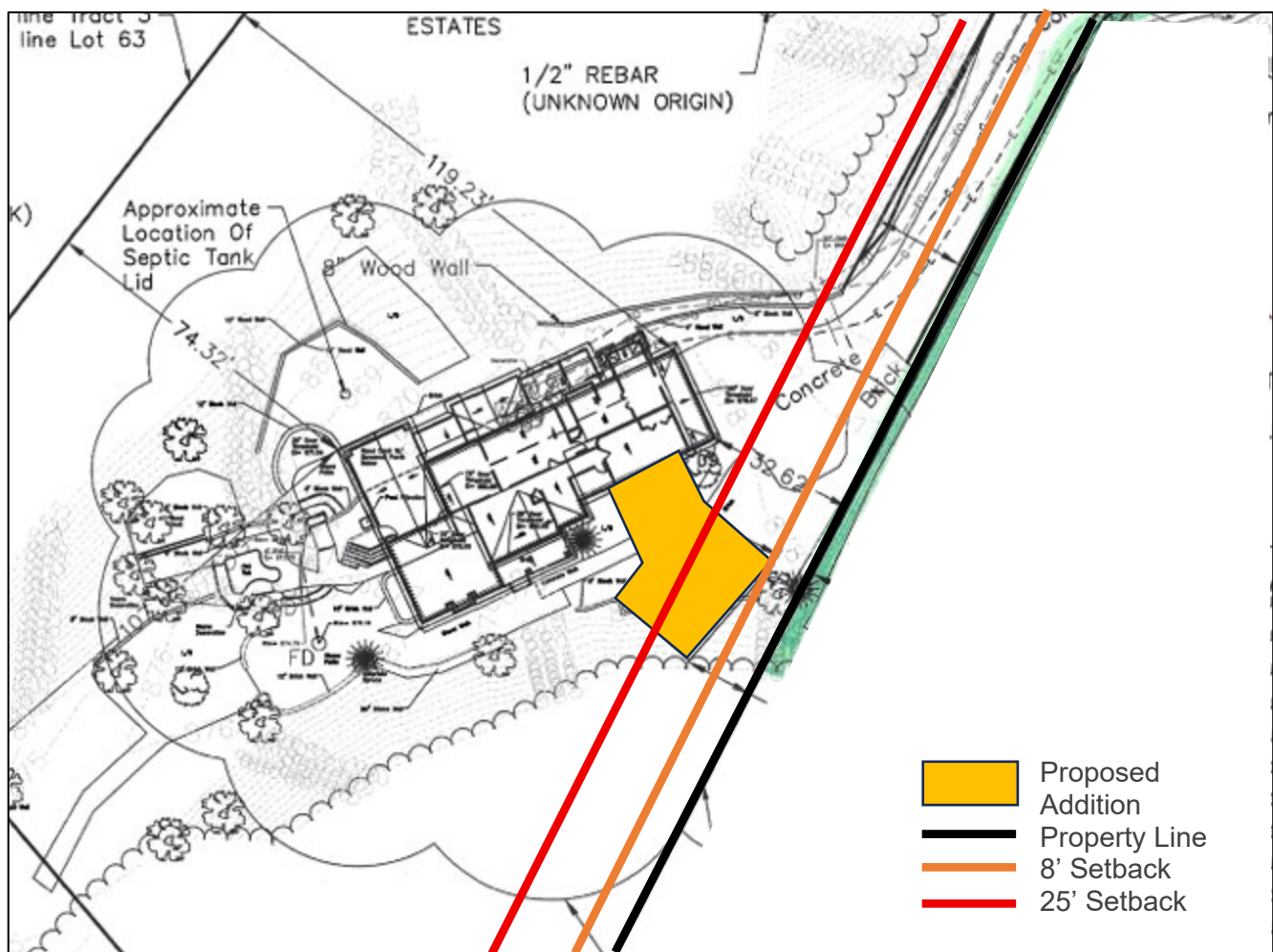


Exhibit 4: Setback Analysis.

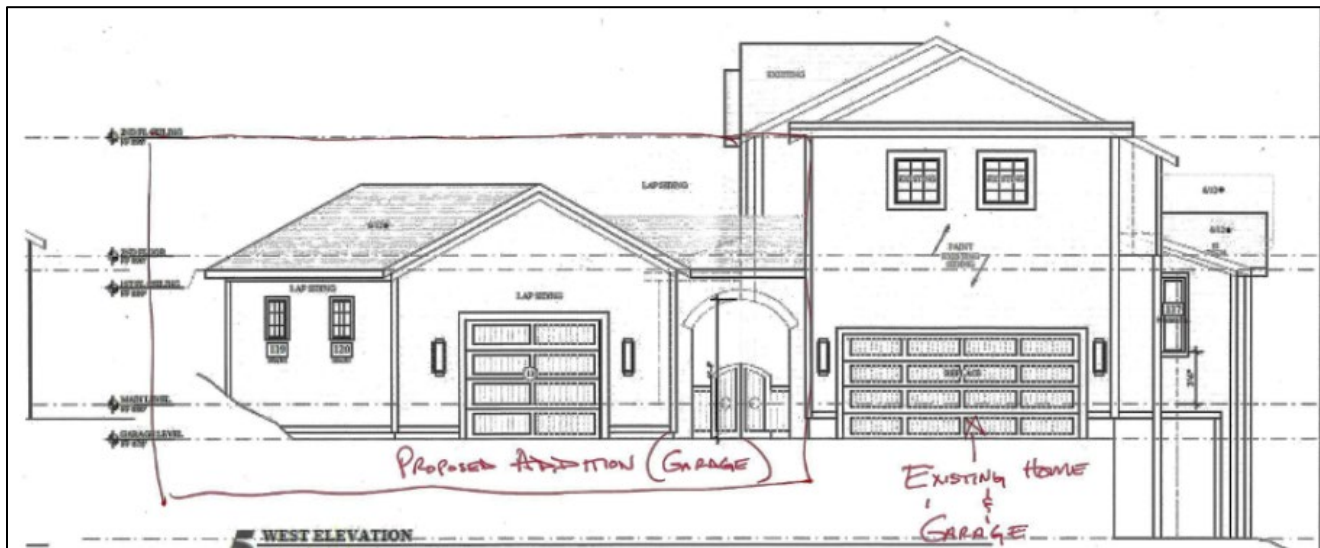


Exhibit 5: Proposed Architectural Drawings

Many surrounding properties were developed prior to annexation by Lenexa in 1986. Following annexation in 1986, the subdivision was given the R-1 zoning designation. In 1992, when the City’s Zoning Ordinance was overhauled and the RE/RP-E Zoning District was created, the neighborhood was then zoned RP-E, Planned Residential Estate District.

The RP-E Zoning District is intended for low-density large lot development with spacious setbacks on properties that may or may not have access to facilities and services seen in suburban residential subdivisions.

While the Whispering Hills Estates subdivision features several larger, estate style lots, many of the lots within the neighborhood are sized more similarly to a standard suburban style subdivision. As shown in Exhibit 6, several houses within this section of Whispering Hills feature a side yard setback of less than the RP-E District’s 25’ minimum, but the existing setbacks instead meet the typical R-1/RP-1 side yard setback requirement of 7’.



Exhibit 6: Nearby nonconforming lots (marked with yellow stars) in relation to subject site (red border).

DEVIATION ANALYSIS

Staff’s analysis of the deviation request required for granting of a setback deviation is outlined within this section. [Section 4-1-B-27-G](#) of the UDC states the Planning Commission has the authority to approve setback deviations from the minimum standards where there is ample evidence that such deviations will not adversely affect neighboring properties and surrounding areas and where such deviations do not constitute the granting of a privilege that would not be universally appropriate for other similarly designed and situated developments.

1. That such deviation will not adversely affect neighboring properties and surrounding areas.

While the proposed accessory structure encroaches into the existing 25’ setback, the site maintains significant screening through a combination of existing landscaping and topographic separation. The applicant indicated that the existing landscape material will be preserved, maintaining the established vegetative screen between the structure and adjacent properties. Together, these elements provide an effective buffer that will mitigate the impact from this expansion.

The applicant also provided a letter of support from the owner of the adjacent property to the east, 21001 Crickett Lane, which is nearest to the proposed accessory structure. While not determinative, this support demonstrates that the property owner most directly impacted by the reduced setback does not object to the proposal and believes the existing and proposed buffering measures adequately mitigate potential impacts. The graphic below depicts the proposed site plan with green highlighting to distinguish the existing landscape buffer to be maintained.

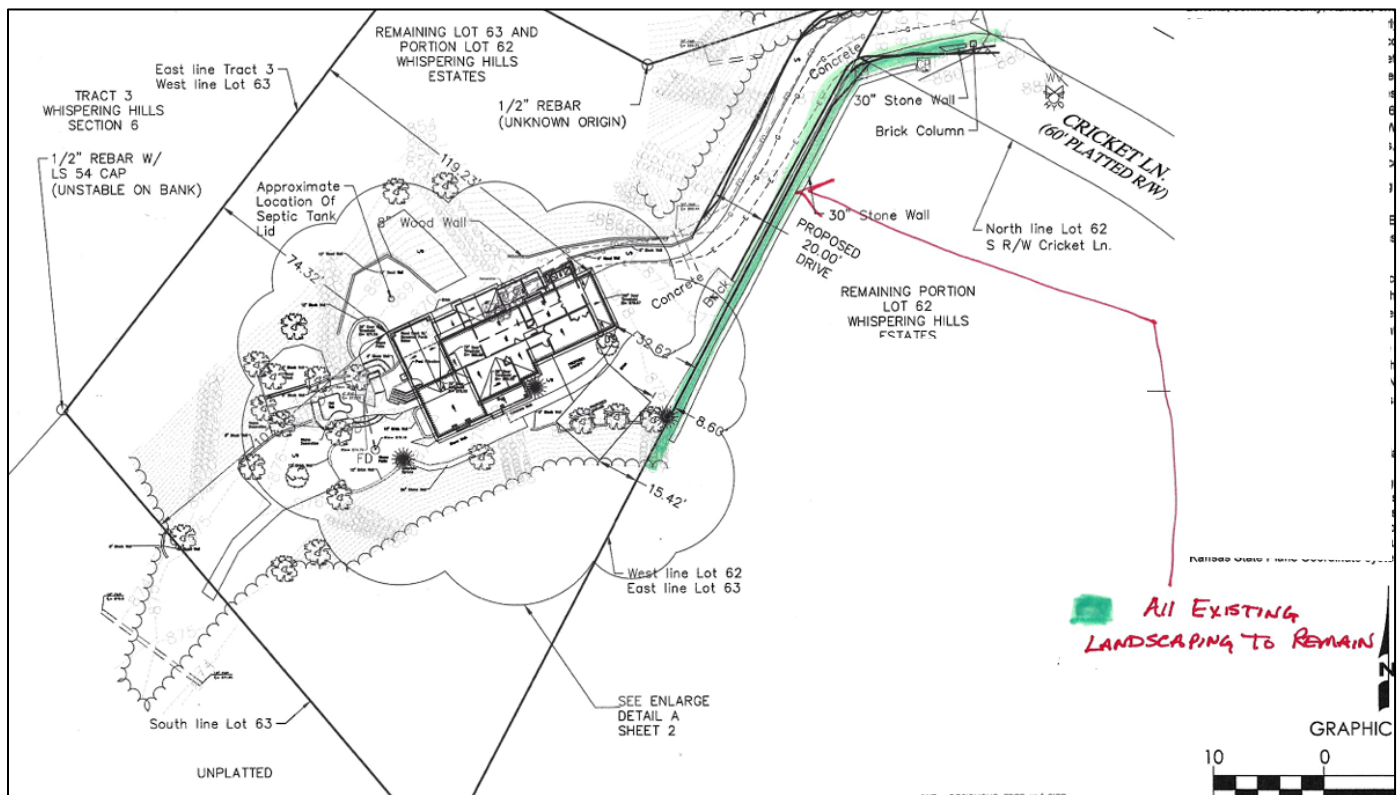


Exhibit 7: Overall Site Plan

2. That such deviations do not constitute the granting of a privilege that would not be universally appropriate for other similarly designed and situated developments.

The requested deviation does not constitute a special privilege, as it reflects a condition commonly found on similarly designed and situated developments on lots within this subdivision. While the Whispering Hills subdivision is zoned RP-E, numerous lots are noncompliant with the requirements of the RP-E Zoning District, either due to lot size, lot width, or setbacks. Many lots within this section of Whispering Hills Estates feature existing side yard setbacks of less than the required 25', with many ranging between 7' and 15'. As a result, the proposed 8' side yard setback is consistent with the existing development pattern of the surrounding neighborhood. Staff identified 11 lots in the vicinity with a setback encroachment on at least one of their yards. Staff finds the proposed 8' setback to be reasonable and not a special privilege, as it is consistent with rights afforded to other property owners in the subdivision.

NEXT STEPS

- The Planning Commission is the final authority for approval of this deviation request.
- The applicant must receive permit(s) prior to commencing construction.
- The applicant should inquire about additional City requirements and development fees.

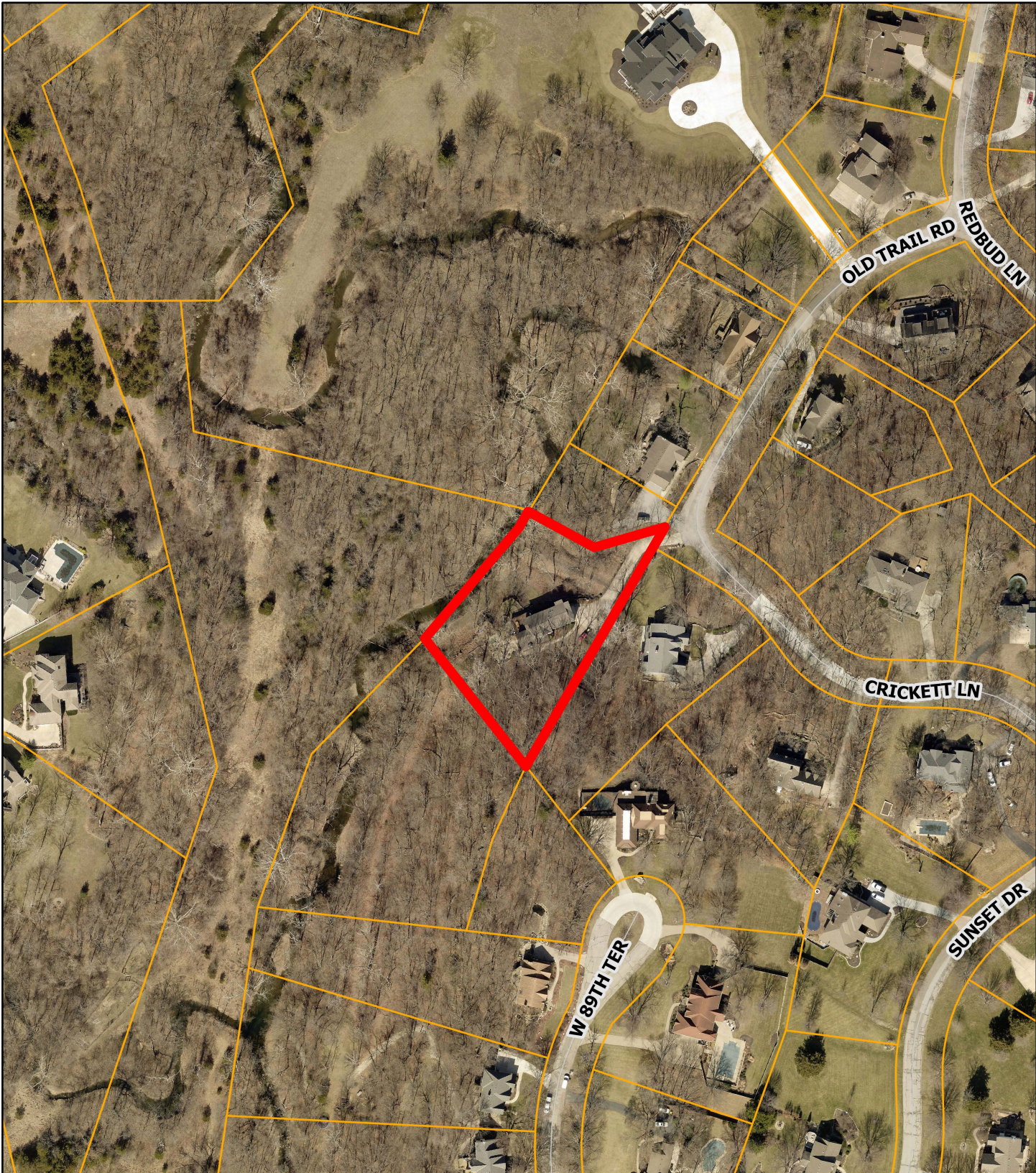
RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the requested deviation (DV26-03) at 8720 Old Trail Road.**

- The applicant is seeking approval for a deviation to facilitate construction of a 775 SF accessory structure with a covered walkway to attach to the existing single-family residence to expand the garage area. The proposed accessory structure encroaches within the required 25' side yard setback.
- Staff recommends approval of a deviation to allow a 17' encroachment into the required 25' side yard setback, resulting in the accessory structure having an 8' side yard setback, within the RP-E Zoning District.
- The project is consistent with Lenexa's goals through *Responsible Economic Development* to create *Vibrant Neighborhoods*.

DEVIATION

Staff recommends **APPROVAL** of DV26-03 – a 17' side yard setback deviation for **Wilson Accessory Structure** at 8720 Old Trail Road.



Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Wilson Accessory Structure

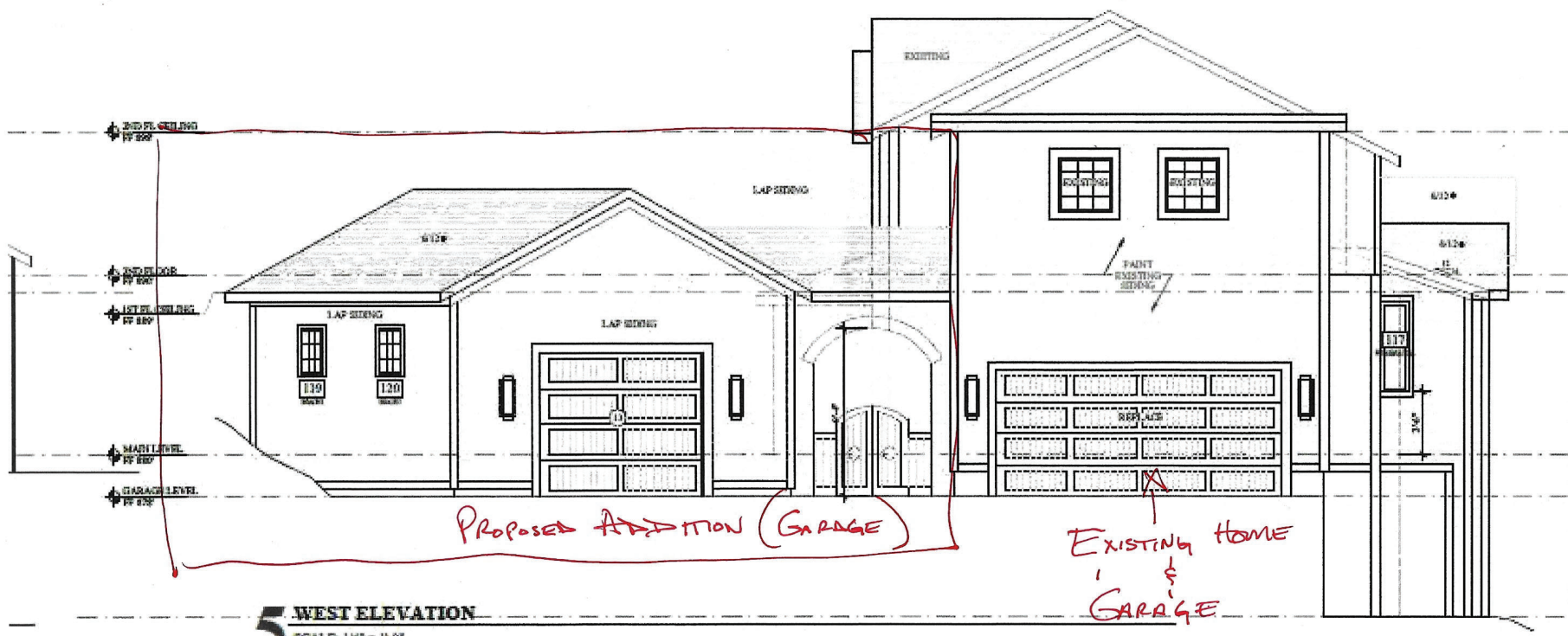




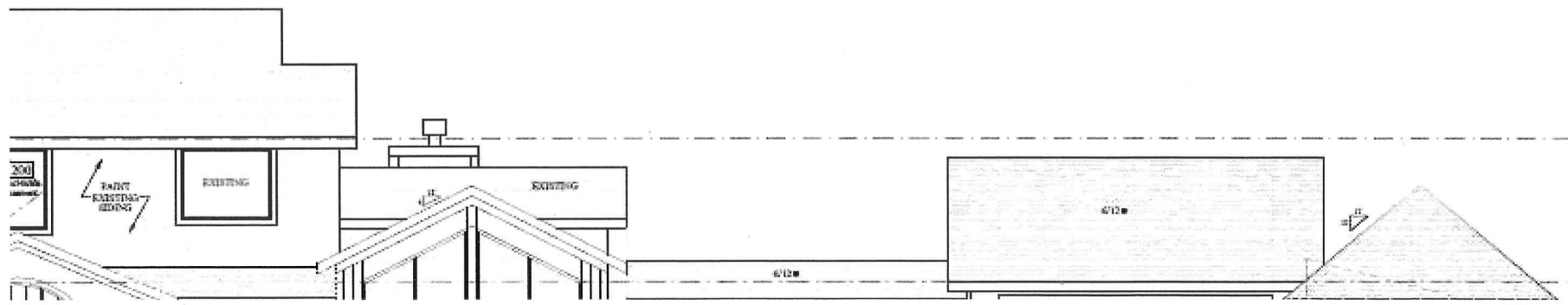
THREE DESIGN BUILD
121 W. State Lane Road, Lenexa, KS 66209

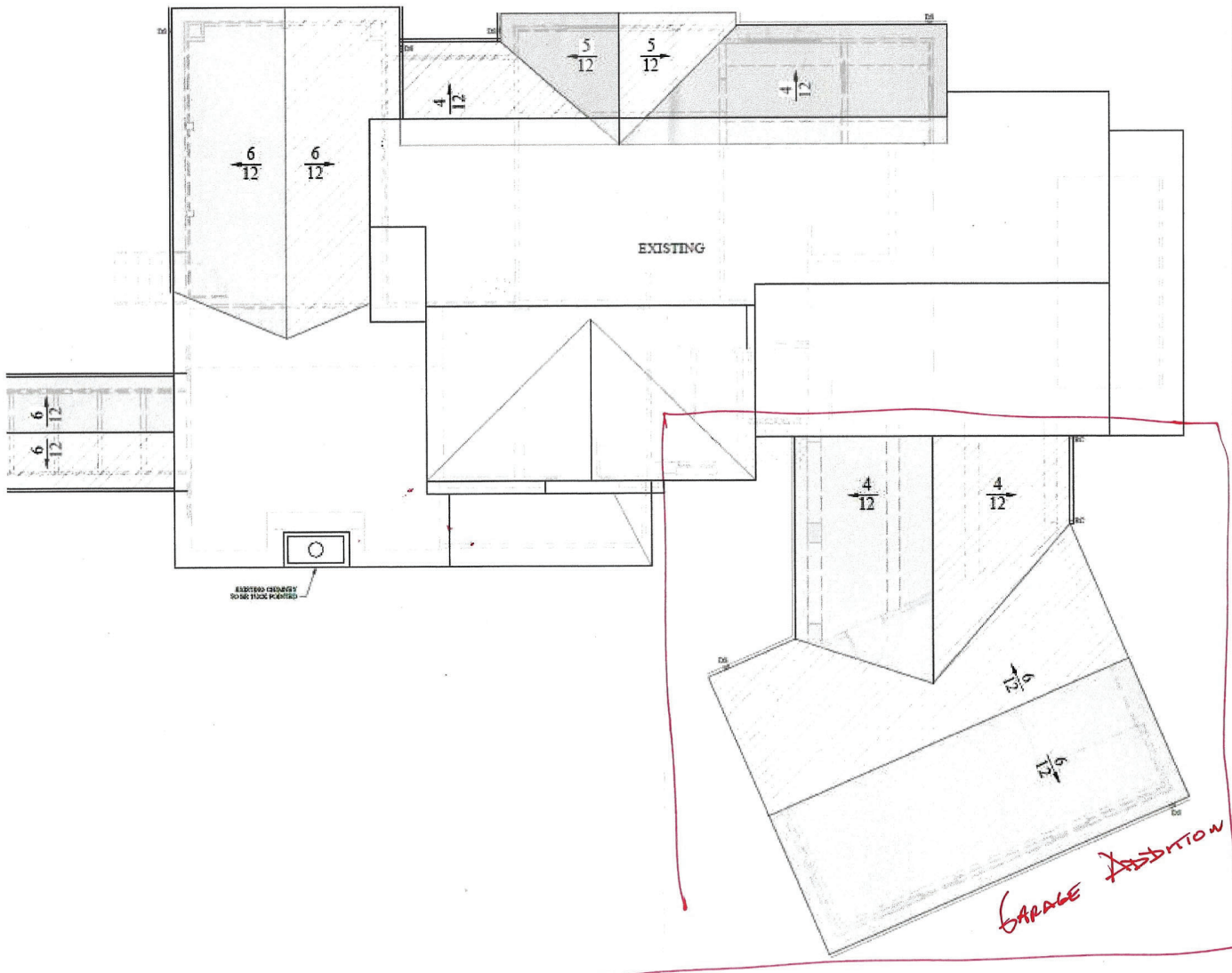
Casa Del Ponte Bella

8720 Old Trail Road, Lenexa, KS 66220



5 WEST ELEVATION
SCALE: 1/4" = 1'-0"





THREE DESIGN BUILD
 2214 1/2 Star Line Road, Lenexa, KS 66220

Casa Del Ponte Bella
 8720 Old Trail Road, Lenexa, KS 66220

NO.	DATE	REVISION
1	08-20-21	ISSUED
2	10-01-21	REVISED

PROJECT NO:
 CAD TITLE:

ROOF PLAN

A2.3



PLOT PLAN

WHISPERING HILLS ESTATES LENEXA, JOHNSON COUNTY, KS

PROPERTY DESCRIPTIONS

Kansas Warranty Deed Book 201910 Pg 002801

(8720 Old Trail Road)

Lot 63, WHISPERING HILLS ESTATES, a subdivision in the city of Lenexa, Johnson County, Kansas, except that part described as follows: Beginning at the most Easterly corner of said Lot 63, thence South 31 degrees, 08 minutes 38 seconds West along the easterly line of said Lot 63, a distance of 30.00 feet; thence South 74 degrees, 38 minutes, 38 seconds West, a distance of 95.00 feet; thence North 58 degrees, 51 minutes, 21 seconds West, a distance of 94.61 feet to a point on the Westerly line of said Lot 63; thence North 31 degrees, 08 minutes, 38 seconds East along the Westerly line of said Lot 63; thence South 58 degrees, 51 minutes, 21 seconds East along the Northerly line of said Lot 63, a distance of 160.00 feet

Part of Lot 62, WHISPERING HILLS ESTATES, a subdivision in the City of Lenexa, Johnson County, Kansas, more particularly described as follows:

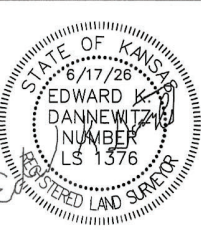
Beginning at the most Northwesterly corner of said Lot 62; thence South 58°51'22" seconds East along the Northeasterly lot line of said Lot 52, said line also being the southerly right-of-way line of Crickett Lane, a distance of 5.00 feet to a point thence South 44 degrees 30 minutes 59 seconds West, a distance of 20.56 feet to a point in the Northwesterly lot line of said Lot 62; thence North 30 degrees 26 minutes 23 seconds East along said Northwesterly lot line a distance of 20.00 feet to the Point of Beginning, said tract containing 50.00 square feet, more or less.

NOTES:

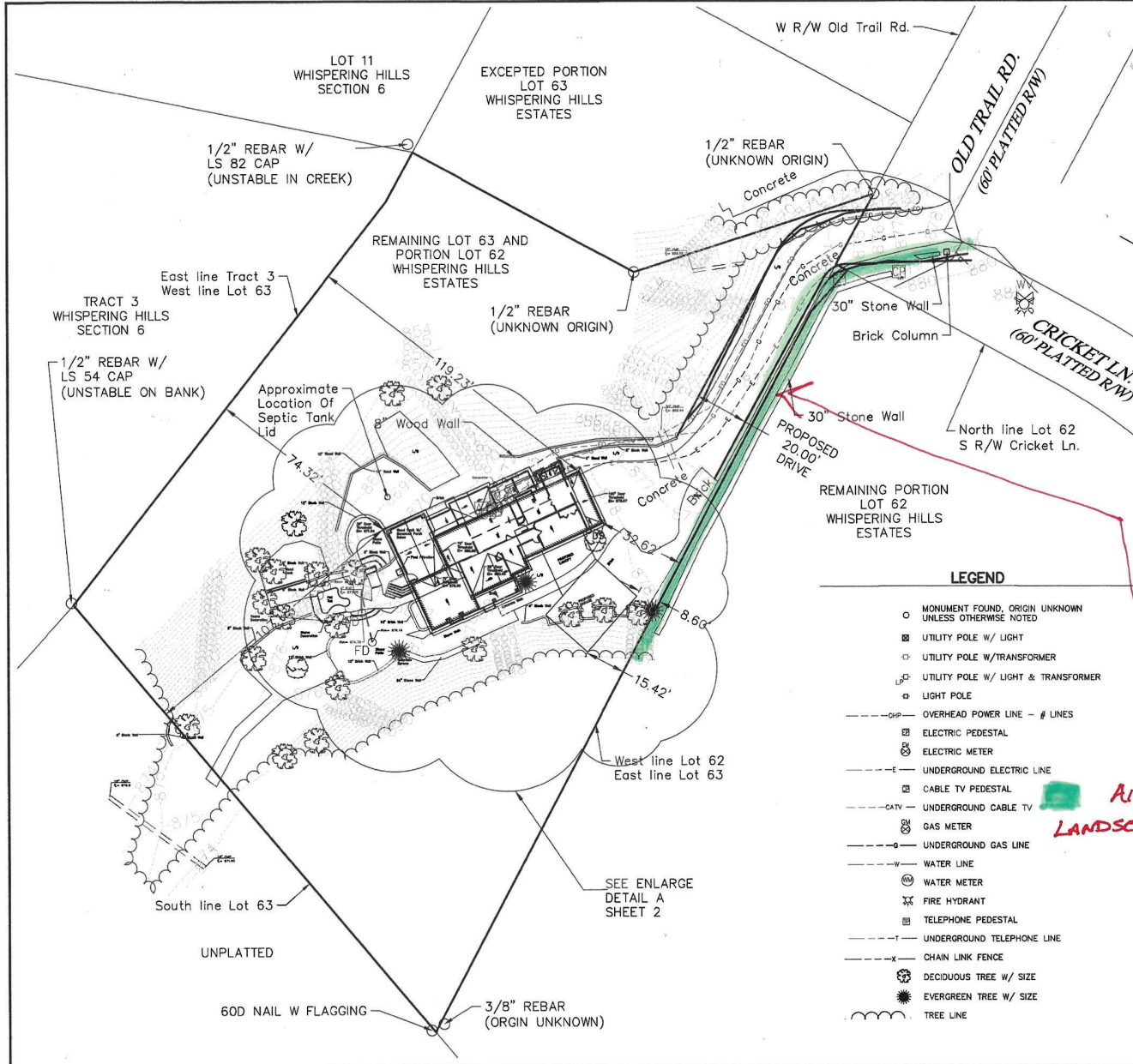
1. Refer to Architectural/Structural plans for exact house dimensions and proposed grading.
2. Proposed Roadway shall be designed and constructed with approved driving surface, capable of supporting the imposed load of fire apparatus weighing at least 75,000 pound or otherwise determined they be Fire Code official.
3. Bearings and distances are measured/calculated based on Kansas State Plane Coordinate system and NAVD83 Datum.

PROJECT NO.	2508-26
DRAWN BY	E. DANNEWITZ
DATE	3/23/2026
CFN	PLOT PLAN
SHEET	1 OF 2

9000 STATE LINE ROAD
LEAWOOD, KANSAS 66206
1006 PARALLEL PARKWAY
KANSAS CITY, KANSAS 66109
TEL. (913) 642-6642
FAX (913) 642-6941
www.ccengineers.com



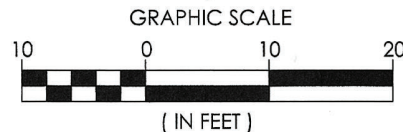
EDWARD K. DANNEWITZ
LS-1376
survey@ccengineers.com



LEGEND

- MONUMENT FOUND, ORIGIN UNKNOWN UNLESS OTHERWISE NOTED
- ▣ UTILITY POLE W/ LIGHT
- ◻ UTILITY POLE W/ TRANSFORMER
- ◻ UTILITY POLE W/ LIGHT & TRANSFORMER
- LIGHT POLE
- OHP --- OVERHEAD POWER LINE - # LINES
- ▣ ELECTRIC PEDESTAL
- ⊗ ELECTRIC METER
- E --- UNDERGROUND ELECTRIC LINE
- ▣ CABLE TV PEDESTAL
- CATV --- UNDERGROUND CABLE TV
- ⊗ GAS METER
- G --- UNDERGROUND GAS LINE
- W --- WATER LINE
- ⊗ WATER METER
- ⊗ FIRE HYDRANT
- ▣ TELEPHONE PEDESTAL
- T --- UNDERGROUND TELEPHONE LINE
- X --- CHAIN LINK FENCE
- ⊗ DECIDUOUS TREE W/ SIZE
- ⊗ EVERGREEN TREE W/ SIZE
- TREE LINE

All Existing Landscaping to Remain

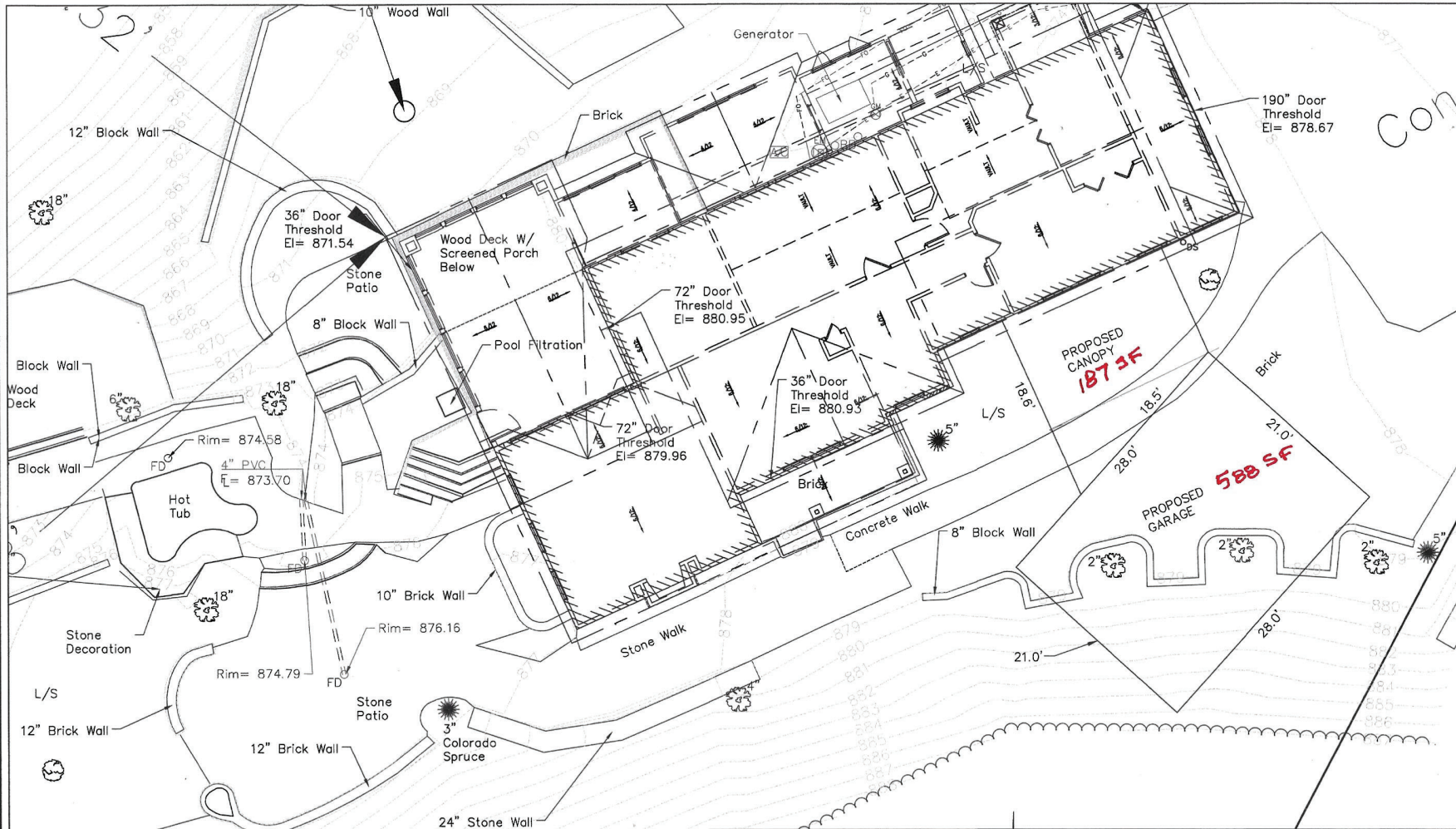


Updated 4/6/2026- Removed private hydrant information
Updated 6/17/2026- Added Garage & Canopy information

DETAIL A

PLOT PLAN

WHISPERING HILLS ESTATES
LENEXA, JOHNSON COUNTY, KS

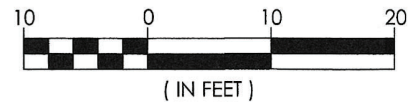


ZONING (Per JOCO AIMS)

Residential Single Family (RP1 per Title 4 UDC)

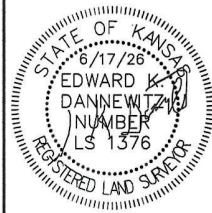
- Minimum setback from streets 30 feet/20 feet from side streets
- Minimum interior side setback 7 feet
- Minimum rear setback 20 feet
- Maximum height 35 feet
- Minimum open space 60 percent of the lot/district

GRAPHIC SCALE



PROJECT NO.	2508-26
DRAWN BY	E. DANNEWITZ
DATE	3/23/2026
CFN	PLOT PLAN
SHEET	2 OF 2

9000 STATE LINE ROAD
LEAWOOD, KANSAS 66206
10006 PARALLEL PARKWAY
KANSAS CITY, KANSAS 66109
TEL: (913) 642-6642
FAX: (913) 642-6941
www.ccengineers.com



EDWARD K. DANNEWITZ
LS-1376
survey@ccengineers.com

Richard and Jenny Wilson
8720 Old Trail Road
Lenexa Ks, 6620

June 17, 2026

Re: Deviation Application DV26-03 "Wilson Residence Garage Addition"

Narrative for Deviation Request per Planned Development Regulations Section 4-1-B-27-G:

Review Comment Request 1: "that such deviations will not adversely affect neighboring properties and surrounding areas"

The addition of the garage would not be visible from the street view or neighboring properties at the intersection of Old Trail Road and Crickett Ln. per the street photograph provided in the deviation request submittal. The neighboring property that is located diagonally to the northeast of the front of the Wilson home, located at 21001 Crickett Ln. and owned by Dan & Kathy Brayman, will have a view of the garage addition from their backyard. The existing screening trees and shrubs at the property line of the Brayman and Wilson homes will not be disturbed or removed. The Braymans have given written approval of the new garage addition and was included in the deviation submittal packet. The governing HOA for Whispering Hills estates has also given its approval for the garage addition.

Review Comment Request 2: "and where such deviations do not constitute the granting of a privilege that would not be universally appropriate for other similarly designed and situated developments"

We do not believe that the granting of the deviation request to be a special privilege to the Wilson's unique lot. The lot configuration and topography that is indicated on the submitted site plan shows the severe slopes and restrictions of the width of the lot that prevent a garage addition at the front of the home. The only possibility to fit a garage would be the request to encroach on the sideyard setback that is typically required by the city of Lenexa, in this case 25'. We believe that the city would be open and approving of other deviation requests with similar restricted qualities to that of the Wilson lot throughout the city, if applied for.

Review Comment Request. "Per section 4-1-B-27-G, explain how your deviation requests meet the outlined criteria."

4-1-B-27-G (F-1) That the deviation requested arises from a condition that is unique to the subject property, is not ordinarily found in the same zoning district and has not been created by the action of the landowner or applicant.

The Wilson's lot is very unique in regards to the severe slopes and topography to the northwest, that slopes to a creek below the home, and to the northeast that slopes severely uphill. These conditions created a lot that is very narrow at the entrance from the intersection of crickett lane and old trail rd. . It required the original developer to plat a lot that would only allow for a home to be approximately 200' from the street to the front of the home to give it enough width to fit between the slope to the northwest and and the setback and uphill slope to the northeast. Finally, the slopes to the rear of the home did not allow for the house to be situated further toward the back of the lot due to the sloping to the creek.

4-1-B-27-G (F-2) That the granting of the deviation will not adversely affect the rights of adjacent landowners or residents.

The Wilson's do not have any neighboring properties to the sides or rear of their lot that will be affected by the addition. The homes at the front of the property beyond Crickett lane and old trail rd., will be screened by privacy trees and shrubs located at the property line between 21001 Crickett lane and the Wilson property. The only property with a view, the Brayman residence at 21001 Crickett lane, have given their written permission for the approval of the garage addition.

4-1-B-27-G (F-3) That the strict application of the requirements of this Chapter would constitute an unnecessary hardship upon the landowner represented in the application.

The Wilson home, which was built 32 years ago, did not allow for a third car garage, or acceptable parking and maneuvering at the time of construction. The home is now undergoing significant improvements and the addition of the garage would dramatically improve the garage accessibility and storage for the home that is not available to them with the current garage.

4-1-B-27-G (F-4) That the deviation requested will not adversely affect the public health, safety or general welfare.

The garage addition is located on a lot with very little visibility to the public and neighboring homes. The nature of the lot, privacy, and isolation prevents any health, safety, or welfare concerns to other residents or passersby.

4-1-B-27-G (F-5) That the deviation will not conflict with the purpose and intent of this Code.

We believe that the intent and purpose of the code is to respect the privacy and sight lines of the neighboring properties. The deviation request to encroach on the sideyard setback will not be in conflict with the intent of the code, due to the unique nature and isolation of the addition from the surrounding property owners.

Review Comment Request. Note the total square footage of the addition in the narrative

Garage Addition: 588 square feet
Covered roof / Porte Cochere: 187 square feet

Review Comment Request. Denote the square footage and measurements of the garage

Garage Addition: 588 square feet	28ft x 21ft
Covered roof / Porte Cochere: 153 square feet	17ft x 11ft

Neighbor Letter of Support for Variance Request – Garage Addition

We have reviewed the plans with Richard & Jenny Wilson about placement of their new garage. We do not have any issues with it being placed close to our property line as long as no easement is needed.

Any questions you can call or email us,

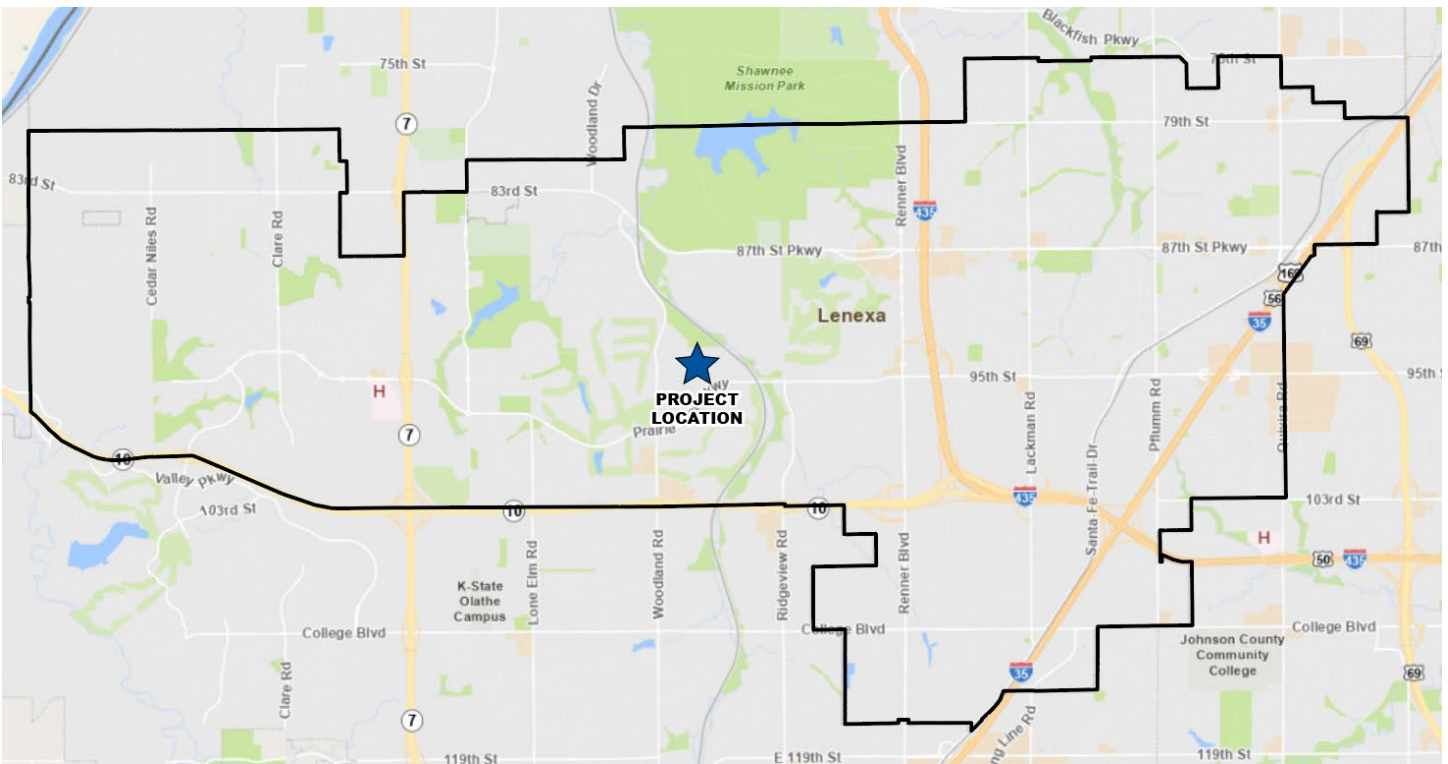
Dan and Kathy Brayman

21001 Crickett Lane, Lenexa KS

913-558-9830

MARMON ESTATES & WOODLAND LAKES LOT 2

Project #:	RZ25-09, PT26-02P, PT26-16F	Location:	Northeast corner of 95 th Street and Cherry Lane
Applicant:	Judd Claussen, Phelps Engineering	Project Type:	Rezoning, Preliminary Plat, and Final Plat
Staff Planner:	Noah Vaughan	Proposed Use:	Single-Family Residential



PROJECT SUMMARY

The applicant requests approval to rezone and plat approximately 28 acres for a two-lot single-family residential subdivision at the northeast corner of the intersection of 95th Street and Cherry Lane. One of the three existing parcels is currently zoned as RE, Residential Estate Zoning District, and is requested to be rezoned to R-1, Residential Single-Family Zoning District, to unify the zoning of the properties. Because of the minor nature of the proposed platting, the Planning Commission is able to consider the preliminary plat and final plat concurrently. The preliminary and final plat contain two resulting lots bordered by unplatted parcels and other residential subdivisions. While no deviations are requested, the applicant is requesting relief regarding the residential buffer fence requirement for a new subdivision in the R-1 Zoning District. A Public Hearing is required for the rezoning request. The plat includes dedication of a utility easement to the City of Lenexa.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The subject properties are three parcels along the north side of 95th Street near the intersection of Cherry Lane. The two western parcels are currently zoned R-1 and are unplatted. The smallest parcel is an undeveloped 1-acre parcel with an existing shed. The largest parcel is approximately 15 acres and is currently developed with a single-family residence addressed as 19520 W. 95th Street. The eastern lot is 12.5 acres and is zoned RE (Residential Estate). This eastern lot is currently platted as Lot 2 within the Woodland Lakes subdivision.

Other nearby developments include the Enclave at Woodland Lakes subdivision to the west, the Cherry Creek Addition residential development to the south, Craig Crossing Park to the northwest, and Lenexa Fire Station 5 to the southeast. A roadway connection to Prairie Star Parkway is also close by as well as nearby BNSF railway property to the east.

LAND AREA (AC)	LOTS	CURRENT ZONING	COMP. PLAN
Entire Development: 28.75	Existing: 3	R-1, RE	Low-Density Residential, Suburban-Density Residential
Proposed Rezoning: 12.5	Proposed: 2		

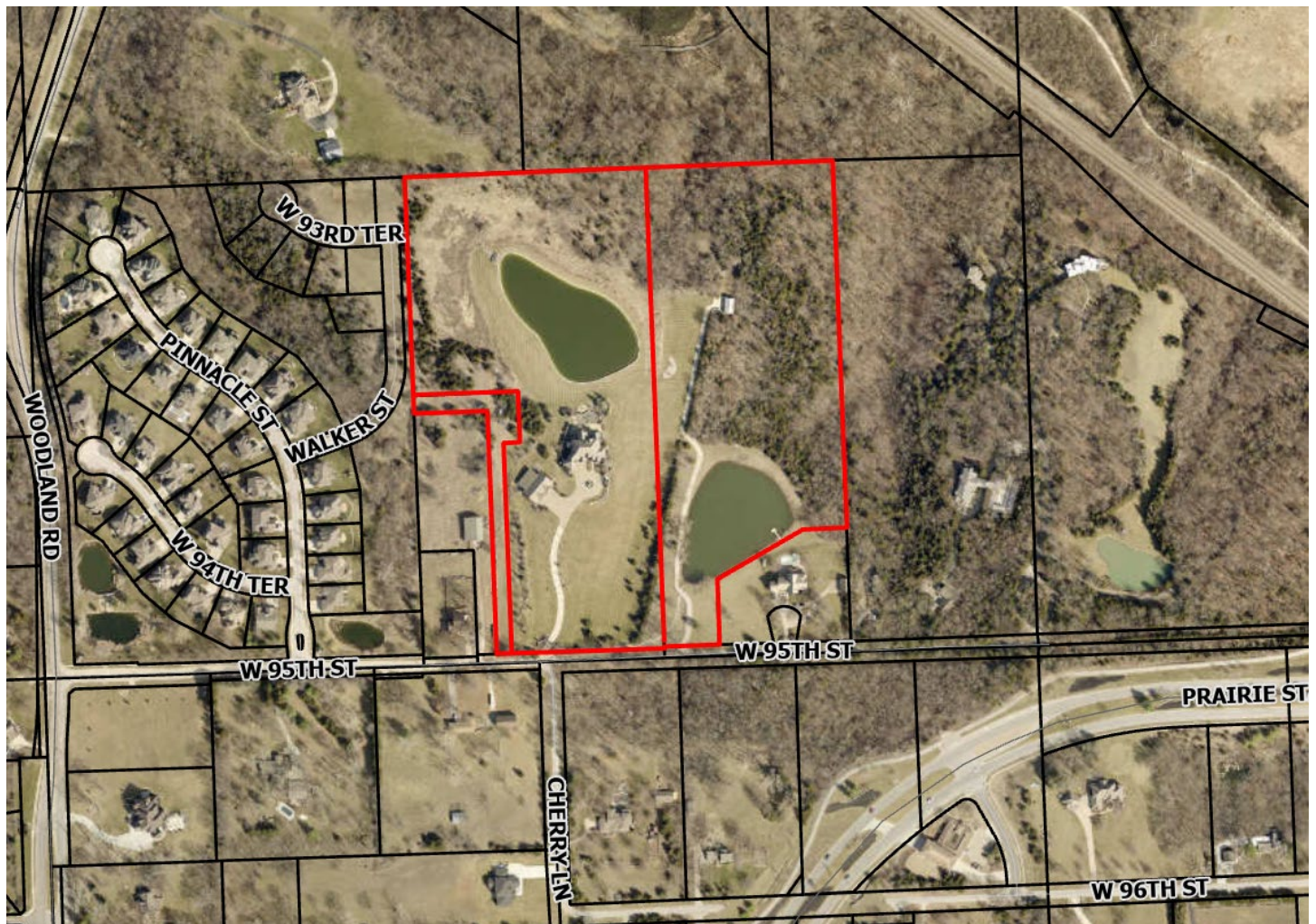


Exhibit 1: Aerial Image of Subject Site.

LAND USE REVIEW

The proposed development is for single-family residential use. The western parcels are classified as Suburban-Density Residential and the eastern parcel is classified as Low-Density Residential on the Future Land Use Map of the Comprehensive Plan. The R-1 District allows a maximum density of 3.5 dwelling units per acre. The current density of the combined parcels is 0.035 based on one dwelling and a total of 28.75 acres. As the applicant intends to market the second resulting lot as a single-family developable lot, the proposed subdivision yields a potential density of 0.07 dwelling units per acre. Based on the proposed changes to the zoning, the current eastern parcel will need to be updated to Suburban-Density Residential in the next annual Comprehensive Plan update to ensure that the designation is aligned with the zoning and use. The proposed use of single-family residential is appropriate for the area and the surrounding uses.

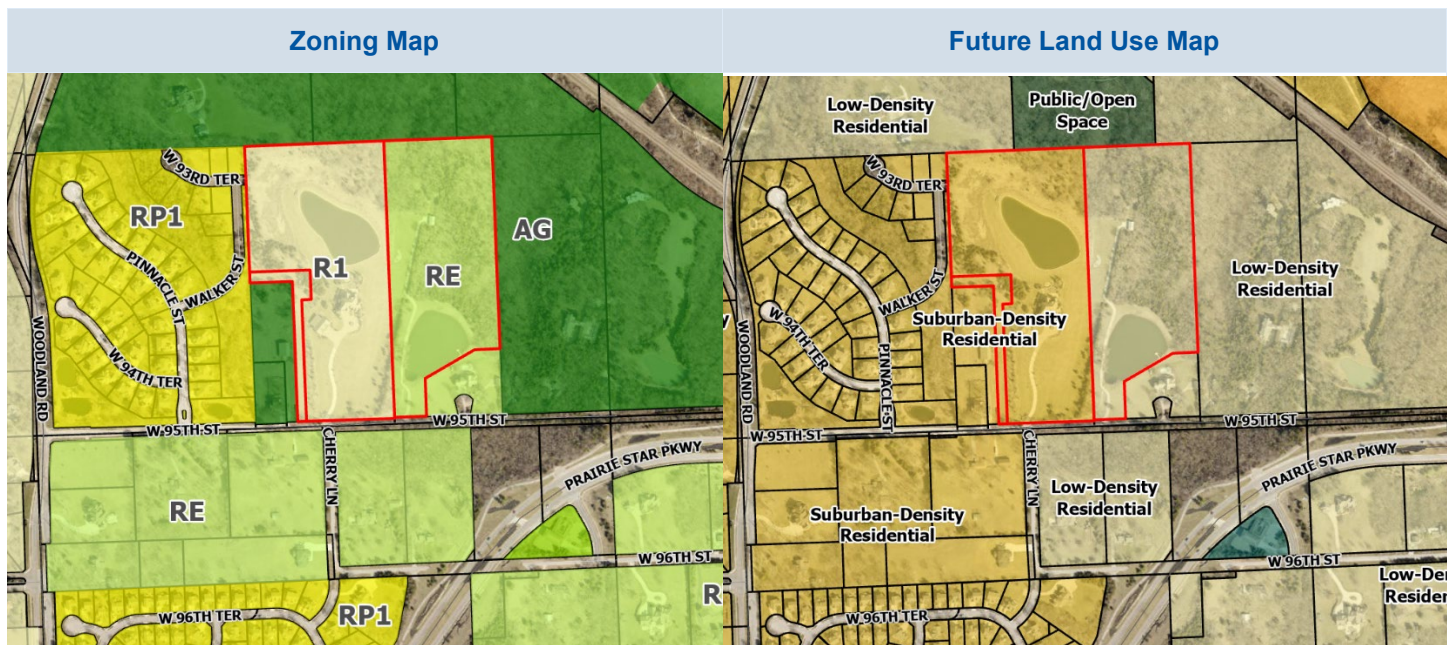


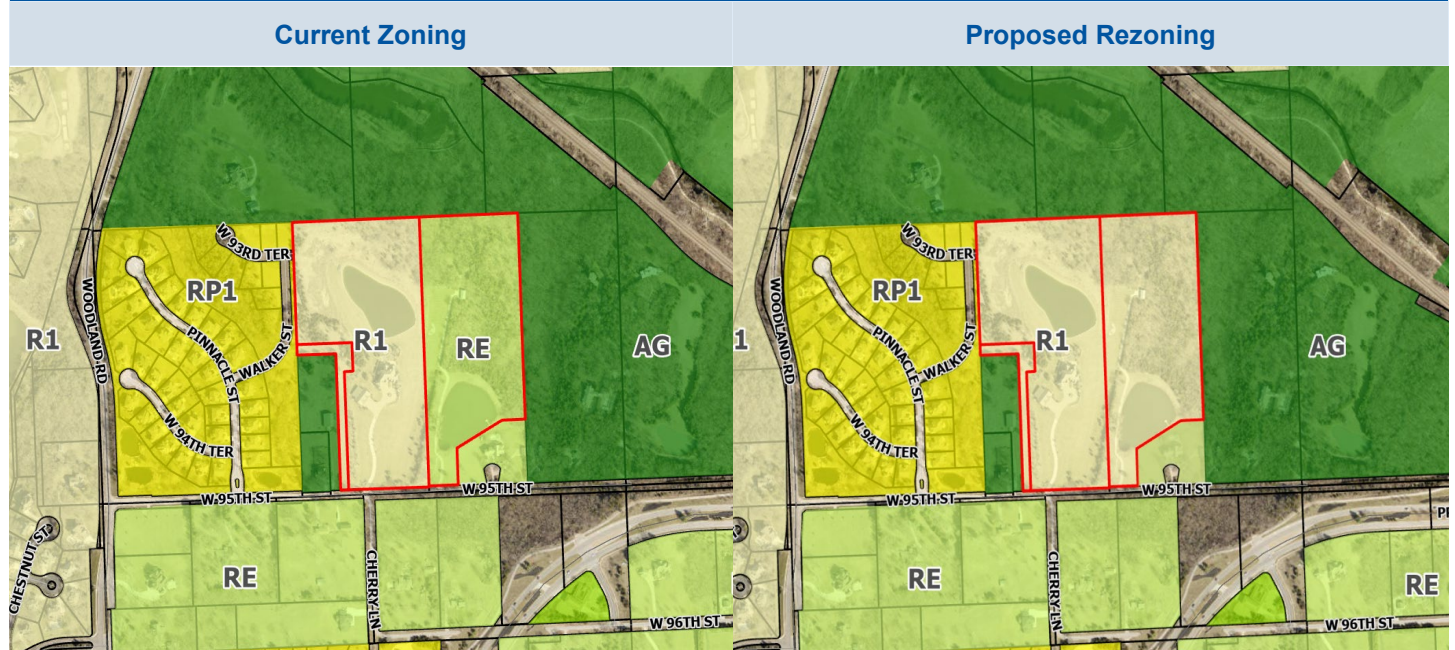
TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Classification	Zoning	Current Use
Subject Property	Low-Density Residential, Suburban-Density Residential	R-1, Single-Family Residential District, and RE, Residential Estate District	Single-Family Residential, Undeveloped
North	Low-Density Residential, Parks & Open Space	AG, Agricultural District	Undeveloped, Parks & Open Space
South	Low-Density Residential, Suburban-Density Residential	RE, Residential Estate District	Single-Family Residential
East	Low-Density Residential	AG, Agricultural District	Single-Family Residential
West	Suburban-Density Residential	RP-1, Planned Residential Single Family (Low-Density) District, AG, Agricultural District	Single-Family Residential

ZONING REVIEW

The applicant proposes to rezone the eastern parcel from the RE, Residential Estate District, to the R-1, Residential Single-Family District, to unify zoning across the commonly owned parcels.

TABLE 2: REZONING ANALYSIS



Staff provides the following analysis for the review criteria within Section 4-1-G-5 of the Unified Development Code (UDC).

1. The character of the neighborhood.

The subject properties are adjacent to a wide range of development and are located between pockets of residential and business park/light industrial areas, primarily serving as a transition between the two. Falcon Ridge, a single-family subdivision just west of Woodland Road, and Enclave at Woodland Lakes, another single-family development to the west of the subject properties, are relatively newer developments in this area of Lenexa near the site. Larger lot residential sites, primarily zoned AG and RE, border the properties on the north and south sides. The BNSF railway is just to the east of the properties.

Overall, the proposed single-family subdivision is keeping with the character of the area and the proposed density is compatible with established low residential densities in the vicinity. The general impact of the development is minimal, as the total amount of lots will be reduced if the plat and rezoning are approved, and the resulting lots are still intended for single-family residences.

2. The zoning and use of properties nearby.

The zoning and uses of the adjacent properties are predominantly large lot residential estate and suburban single-family residential subdivisions, as well as various agricultural, open space, and institutional uses such as Craig Crossing Park and Lenexa Fire Station 5. RP-1 and RE are the most predominant zoning districts in the vicinity of this site. Some neighboring properties are currently estate sized AG zoned lots used for residential purposes; the park property is also zoned AG. The zoning and land use of adjacent properties is noted in Table 1.

3. The suitability of the subject property for the uses to which it has been restricted.

The properties are currently restricted to low-density (1 du/ac maximum) and suburban-density single-family (3.5 du/ac maximum) uses based on their current zoning of R-1 and RE. In order to move forward with the plat changes requested by the applicant, a zoning change is necessitated to avoid creating a new property boundary with split zoning. The proposed R-1 zoning allows for suitable uses of the subject sites when comparing to existing uses within surrounding properties, the Future Land Use classifications within the Comprehensive Plan, and the goals of the Comprehensive Plan.

4. The extent to which the proposed use will detrimentally affect nearby property.

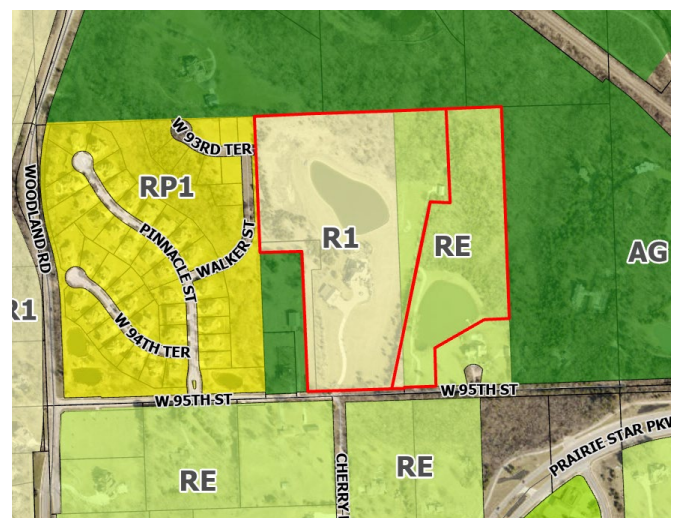
The proposed uses will not detrimentally affect nearby properties as appropriate streets, utility, and other infrastructure are already in place to support the resulting lots if the rezoning and plat were approved. Additionally, no physical changes are currently proposed as part of the plan. It is Staff’s opinion the proposed uses are compatible with the existing and planned uses in the vicinity.

5. The length of time the subject property has remained vacant as zoned.

The eastern parcel has remained undeveloped and zoned RE since being platted as a portion of Woodland Lakes in 1991.

6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.

It is Staff’s opinion that denial of this rezoning would have no gain to public health, safety, or welfare since the proposed development’s density and uses are compatible with surrounding development and appropriate infrastructure is available to serve the site. Denial of the application would restrict the eastern parcel to the existing zoning of RE, which would result in the proposed west parcel being partially zoned R-1 and partially zoned RE.



At Right: Exhibit 2: Graphic demonstrating the resulting proposed lots if the rezoning is not approved.

7. Recommendation of City’s permanent professional staff.

See Staff’s recommendation and the end of this report.

8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.

The Future Land Use classifications for the subject properties are Low-Density Residential and Suburban-Density Residential, intended for detached single-family uses. If the rezoning is approved, the resulting zoning of the property will be R-1, which limits density to 3.5 dwelling units per acre. The existing sites and designations are consistent with the current land use classification of the Comprehensive Plan. Should the rezoning be approved, the current eastern parcel will need to be updated to Suburban-Density Residential in the next annual Comprehensive Plan update to ensure that the designation is aligned with the zoning and use.

9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.

Adequate utilities and services are available to the development and currently serve the existing residence on the west parcel. Some utility work may be required on the newly-created undeveloped lot as part of the development process if the rezoning and plat were approved, but only connections to existing nearby utilities would need to be made.

10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.

The proposed subdivision will be serviced by 95th Street, an existing road that serves the existing residence and will act as the primary access road for the undeveloped parcel when it is ready to be developed residentially. The capacity or safety of the street network will not be burdened by the introduction of one additional future residence. All parking will be off-street and would be required on private property, as no on-street parking on 95th Street is permitted.

11. The environmental impacts the proposed use will generate including, but not limited to, excessive stormwater runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting, or other environmental harm.

The proposed rezoning is not anticipated to generate any environmental impacts exceeding the requirements of the UDC.

12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.

The site is subject to the UDC requirements for stormwater management and is required to meet the same standards as any new development.

13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.

The applicant requests relief from the fence and landscape buffer requirement for the R-1 subdivision detailed in Section 4-2-E-4 of the UDC. Staff's opinion is that existing conditions along 95th Street and the nature of the proposed development do not necessitate this buffer and Staff supports the requested relief from the requirement. This is not considered a formal deviation request. All other applicable requirements are met.

PRELIMINARY PLAT REVIEW

The preliminary plat primarily shows the difference between the existing conditions of the development and the resulting conditions after the adjustment to the boundary lines occur via final plat approval. There is no accompanying plan or proposal that is reflected on the plats, as no physical changes are proposed other than relocating a portion of fencing, and there are only a few changes to utilities and easements that will occur. A 10' utility easement from the edge of the south property line (abutting 95th Street) is proposed, and an existing temporary access easement on Lot 2 of Woodland Lakes will be removed. The graphic below demonstrates the existing conditions of the sites, the existing and proposed boundary line, as well as the proposed zoning changes.



Exhibit 3: Preliminary Plat.

FINAL PLAT REVIEW

The subject site is located near the northeast corner of the intersection of 95th Street and Cherry Lane, just east of Woodland Road. Access into both lots would be from the south, directly off 95th Street. Existing access drives have already been established off this road; one leads to the existing residence on the western parcel (19520 W. 95th Street), and the eastern parcel contains a gravel drive that leads to an existing shed on the property. A paved drive would need to be constructed on this lot to access any future residence.

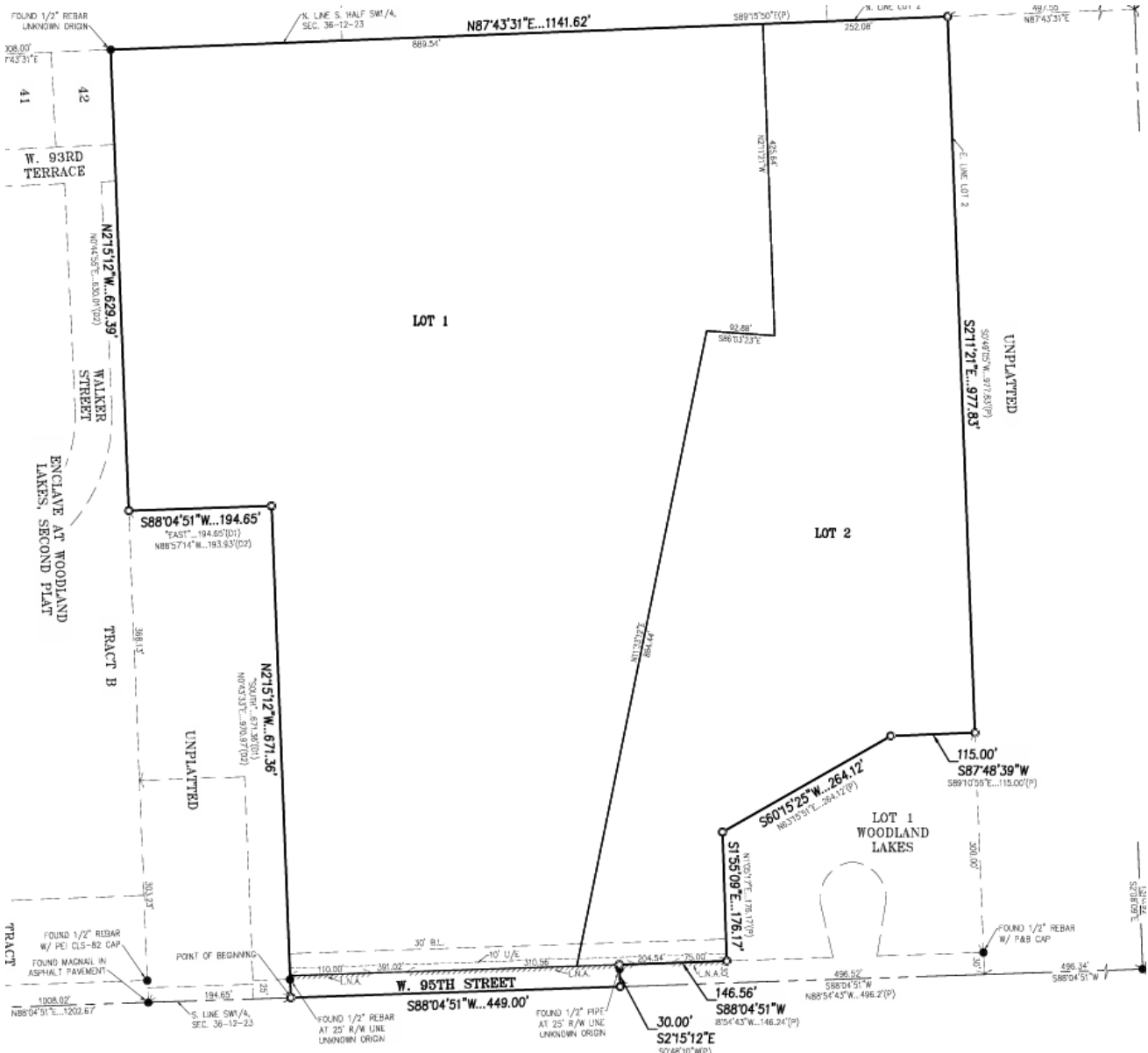


Exhibit 4: Final Plat

The plat proposes a new property line that adds acreage to the addressed property (Lot 1) from the adjacent unplatted parcel as well as a portion of acreage from the existing Lot 2 of Woodland Lakes (eastern parcel, Lot 2 of Marmon Estates). While Lot 2 will gain a small area of acreage to the south from Lot 1, the resulting lot will be smaller overall if the plat is approved. This will be due to the significant amount of acreage transferred to Lot 1 from Lot 2 on the north side of the plat. See Table 3.

TABLE 3: TOTAL LOT ACREAGE CHANGES

Lots	Existing Acreage	Resulting Acreage	Acreage Difference
Lot 1	14.94 ac	19.23 ac	+4.29 ac
Lot 2	12.49 ac	9.20 ac	-3.29 ac
Additional Parcel	1.00 ac	N/A	-1.00 ac

The final development will contain two single-family residential lots. Only one new dedication is proposed with the subdivision, which is a 10-foot utility easement along the 95th Street right-of-way. The City will need to accept this dedication. A 30-foot build line will also be established from the south property line. No tracts are proposed with the subdivision. The housing classification for the development is “C”, which has minimum ground floor area requirements of 1,800 SF, 1,250 SF, and 905 SF for 1 story, 1 & ½ stories, and 2 stories, respectively. The existing structure on Lot 1 meets this requirement and any future dwelling constructed on Lot 2 would be subject to this requirement.

DIMENSIONAL STANDARDS

The proposed subdivision meets all dimensional standards of Section 4-1-B-6 of the UDC for the R-1 Zoning District. Both lots exceed the minimum lot size requirement (8,000 SF), minimum lot width (70 feet), and minimum lot depth (115 feet). The residence on Lot 1 meets the setback and maximum height requirements. All existing accessory structures meet the setback requirements and accessory use development and performance standards of 4-11-B-24-F of the UDC. Additionally, the existing fence across the properties will be relocated to match the proposed new boundary line, if approved. The applicant must obtain a fence permit for the fence relocation.

Per Section 4-2-C-3-C of the UDC, all side lot lines shall be established as a right angle (perpendicular to the street) or radial to street lines. The side lot line that is proposed through this plat is at a diagonal angle when compared to the orientation of 95th Street, thus it does not meet this code requirement. However, due to the large lot nature of the development, it is not expected for this to impact lot development in the area. As such, an exception to this rule can be granted by the Community Development Director per 4-2-C-3-A of the same section of this code. Staff supports this exception for this development.

PUBLIC IMPROVEMENTS

No public improvements are proposed or required as part of this application. Any improvements requiring a permit through the City of Lenexa will undergo the standard permitting review process.

STORMWATER

There are no stormwater issues created with the replatting of these lots and any future improvements that would require a permit would have stormwater concerns evaluated at the time of that permit review.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of any future permitting.

LANDSCAPING

Single-family residential subdivisions require a fence and landscape buffer along collector and arterial streets per Section 4-2-E-4 of the UDC. The buffer is typically provided in a tract between lots and the street; however, the applicants are requesting relief from this requirement. Staff is supportive of this relief as there are currently existing trees and landscaping between the properties and 95th Street, so additional landscaping and a fence are unnecessary. Additionally, the large lot nature of this development reduces the need for this buffer requirement that may be necessary in higher density subdivisions or on smaller lot developments.



Exhibit 5: Image demonstrating the existing landscaping and trees along 95th Street. Subject parcels are outlined in blue and landscaping is outlined in red.

REVIEW PROCESS

- This project requires a recommendation from the Planning Commission and approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration by the City Council on July 21, 2026.
- The final plat must be recorded with Johnson County prior to permit(s) being released.
- The applicant must receive permit(s) prior to commencing any construction and/or relocating the fence.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

- ★ **Conduct a Public Hearing for the rezoning request.**

- ★ **Staff recommends approval of the rezoning, preliminary plat, and final plat for Marmon Estates and Woodland Lakes Lot 2.**
 - The final plat includes two single-family residential lots and dedication of a utility easement to the City of Lenexa.
 - Staff recommends updating the Comprehensive Plan from Low-Density Residential to Suburban-Density Residential to align with the proposed use of single-family residences and R-1 Zoning.
 - The project is consistent with Lenexa’s goals through ***Responsible Economic Development*** to create ***Vibrant Neighborhoods*** and ***Inviting Places***.

REZONING

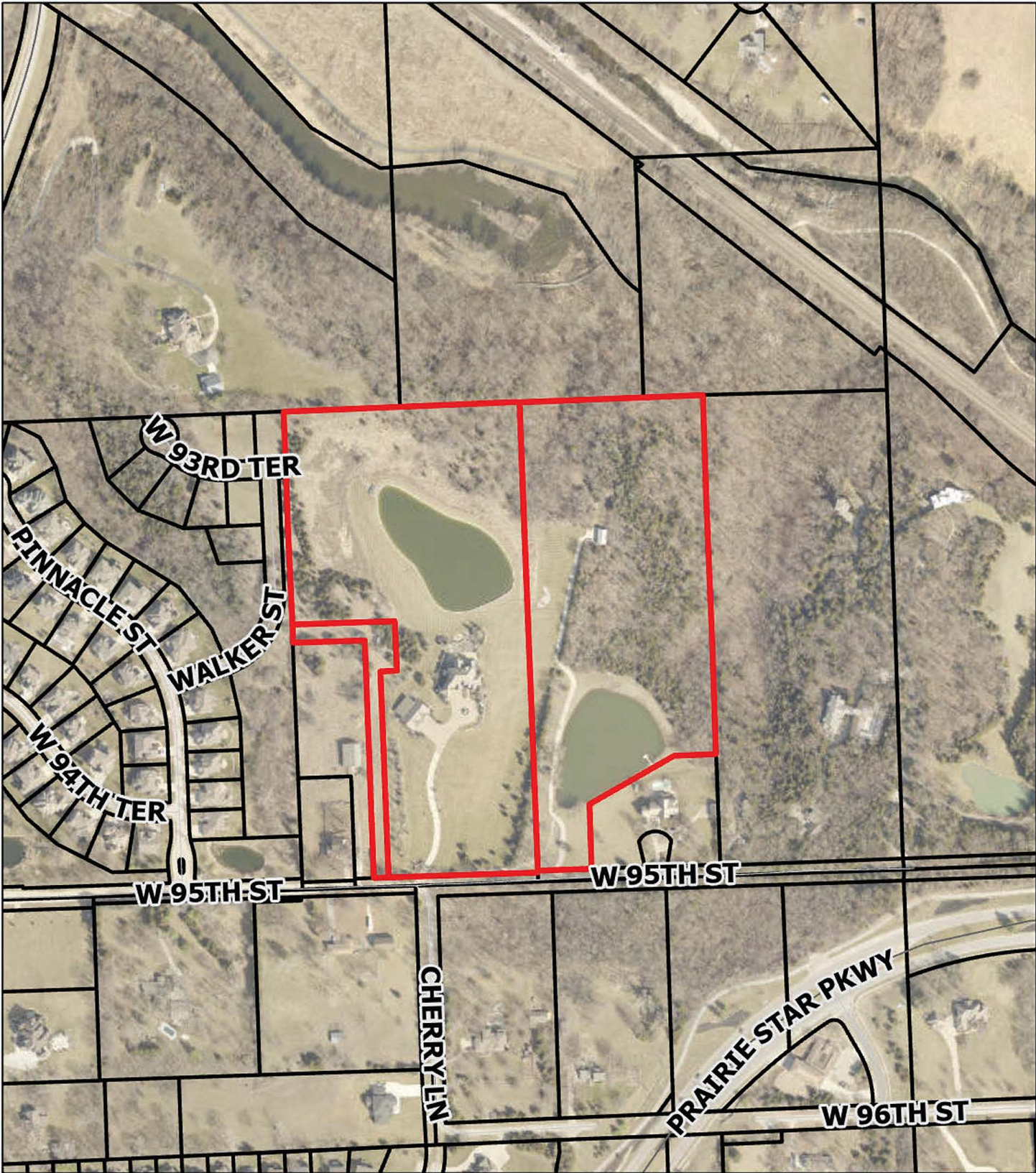
Staff recommends **APPROVAL** for rezoning property from RE to R-1 for **Woodland Lakes Lot 2** located at the northeast corner of 95th Street and Cherry Lane.

PRELIMINARY PLAT

Staff recommends **APPROVAL** of the preliminary plat for **Marmon Estates** located at the northeast corner of 95th Street and Cherry Lane for a single-family subdivision.

FINAL PLAT

Staff recommends **APPROVAL** of the final plat for **Marmon Estates** located at the northeast corner of 95th Street and Cherry Lane for a single-family subdivision.

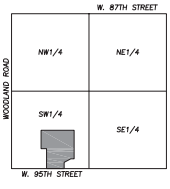


Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Marmon Estates & Woodland Lakes Lot 2



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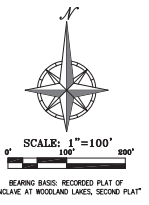


SCALE: 1"=200'
VICINITY MAP
SEC. 36-12-23

DESCRIPTION

This is a replat of all of Lot 2, WOODLAND LAKES, a subdivision, and part of the Southwest Quarter of Section 36, Township 12, Range 23, all being in the City of Lenexa, Johnson County, Kansas, described by Phelps Engineering, Inc. Project No. 294557 April 25, 2023, to wit:

Commencing at the Southwest corner of the Southwest Quarter of said Section 36; thence N 88°04'51" E, along the South line of the Southwest Quarter of said Section 36, a distance of 1002.87 feet, to the Point of Beginning; thence N 2°15'12" W, a distance of 671.56 feet; thence S 88°04'51" W, a distance of 194.65 feet, to a point on the East line of Trest B, ENCLAVE AT WOODLAND LAKES, SECOND PLAT; a subdivision in said City, County and State; thence N 2°15'12" W, along the East line of said Trest B, and its northern extension, and along the East line of Lot 42, of said ENCLAVE AT WOODLAND LAKES, SECOND PLAT, a distance of 629.39 feet, to the Northeast corner of said Lot 42; said point also being on the North line of the South Half of the Southwest Quarter of said Section 36; thence N 87°43'31" E, along the North line of the South Half of the Southwest Quarter of said Section 36, and along the North line of said Lot 2, a distance of 1141.62 feet, to the Northeast corner of said Lot 2; thence S 2°11'21" E, along the East line of said Lot 2, a distance of 977.83 feet, to the Southeast corner thereof; thence S 87°48'39" W, along the South line of said Lot 2, a distance of 115.00 feet; thence S 60°19'28" W, along the South line of said Lot 2, a distance of 284.12 feet; thence S 1°55'09" E, along the South line of said Lot 2, a distance of 176.17 feet, to the most Southerly Southeast corner thereof; said point also being on the North right-of-way line of West 95th Street, as now established; thence S 88°04'51" W, along the South line of said Lot 2 and along the North right-of-way line of said West 95th Street, a distance of 146.56 feet, to the Southwest corner of said Lot 2; thence S 2°12'12" E, along the Southerly extension of the West line of said Lot 2, a distance of 30.00 feet, to a point on the South line of the Southwest Quarter of said Section 36; thence S 88°04'51" W, along the South line of the Southwest Quarter of said Section 36, a distance of 449.00 feet, to the Point of Beginning, containing 26.4473 acres, more or less, 12.4982 acres of which is replatted land, and 16.2494 acres updated.



PARCEL	AREAS SQUARE FEET	ACRES	EXISTING SQUARE FEET
LOT 1	837739	19.2386	684548
LOT 2	425304	9.2066	540338
PLAT INVENTORY GROSS	1262448	28.7477	

UTILITY PROVIDERS:

Electric	Energy, Kansas Metro
Natural Gas	Altamir Energy
Water	No Service Provider - Septic
Sanitary Sewer	None
Cable	FCC Broadband Provider Summary
Stormwater/Public Works	City of Lenexa * stormwater service is automatic, no setup is required

FLOOD NOTE:

THIS PROPERTY LIES WITHIN ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF LENEXA, COUNTY NO. 20068, JOHNSON COUNTY, KANSAS, MAP NO. 2009IC00496, AND DATED AUGUST 3, 2009.

APPROVAL

Approved by the Planning Commission of the City of Lenexa, Johnson County, Kansas,
this ____ day of _____, 20__.

Chris Poss, Chairman

PHILIPS ENGINEERING, INC.
1401 W. 156th Street
Lenexa, KS 66151
(913) 799-1500
Fax: (913) 799-1504
www.phelps-engineering.com
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PRELIMINARY PLAT
MARMON ESTATES
CITY OF LENEXA, JOHNSON COUNTY, KANSAS
ADDRESS: 19520 W. 95TH STREET

PROJECT NO.	DATE	BY	NO.	DATE	REVISIONS:
260243	5/27/25				

Project Narrative:

Marmon Estates 5/4/2026

The property owner (James & Jeanne Marmon) desire to sell a portion of their property. They currently own three parcels:

IF231236-2009

To split the property a preliminary and final plat is required. The new lot created is to the east of the principle residence. The new lot will have the required 200 ft. frontage on 95th Street.

IF231236-2022 – “West Parcel” Unplatted Strip of land located west of the principle residence

IF231236-2009 – “Middle Parcel” which is the principle residence located at 19520 W 95th Street. This parcel is unplatted.

IP87700000 0002 – “East parcel” that was previously platted as Lot 2, Woodland Lakes

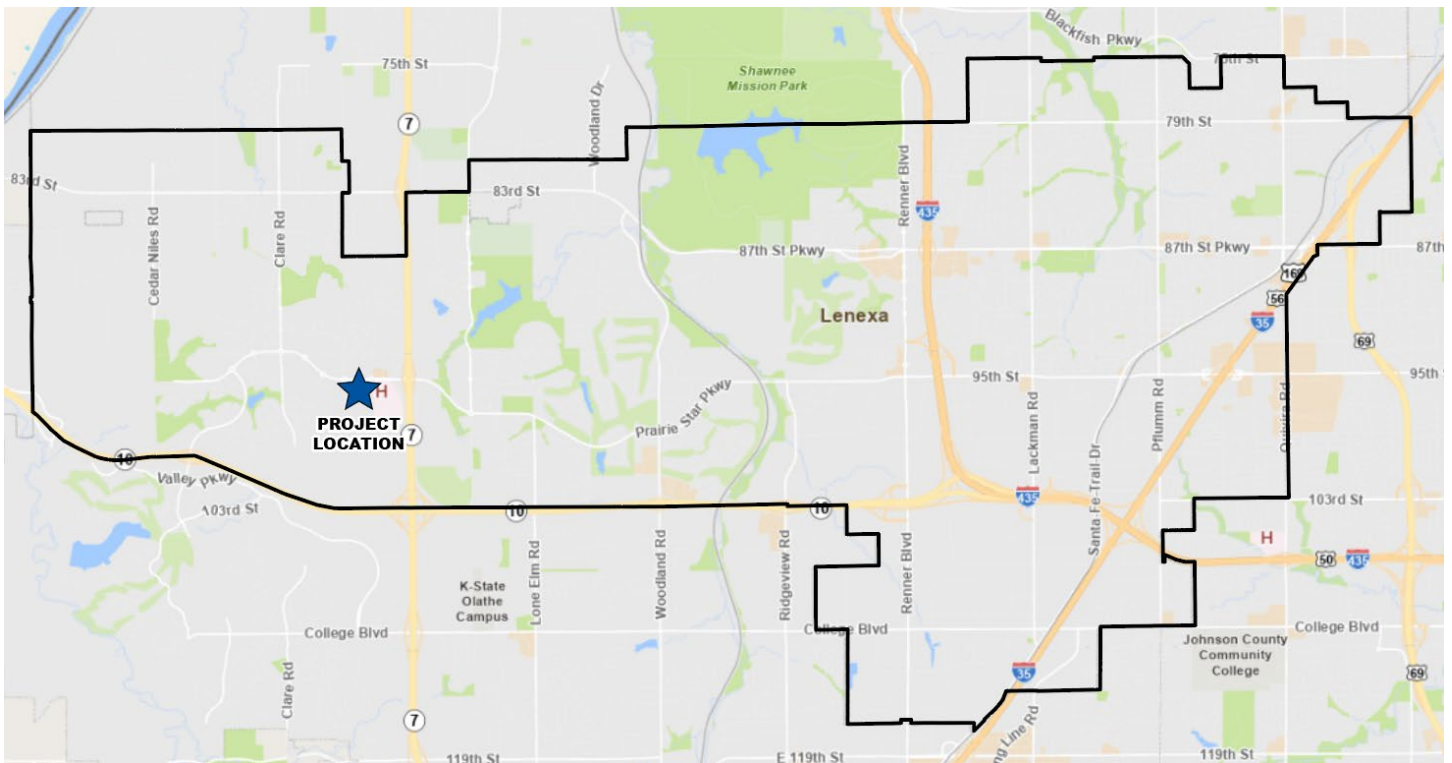
The below image shows these 3 existing parcels:



The Marmon’s desire to adjust the boundary line between the middle and east parcel such that it more closely follows the fence / gravel drive and includes the northeast shed. The new parcel would have 200’ frontage per zoning requirements. The east parcel would then be platted and could be legally sold to another party.

PRAIRIE STAR PARKWAY CENTER

Project #:	PL26-06P, RZ26-03	Location:	South of Prairie Star Parkway between Gleason Road and Dunraven Street
Applicant:	Henry Klover, Klover Architects	Project Type:	Rezoning and Preliminary Plan/Plat
Staff Planner:	Jessica Lemanski	Proposed Use:	Retail, Restaurant, Convenience Store, Gas Station, and Office



PROJECT SUMMARY

The applicant requests approval for a rezoning and preliminary plan/plat for a 17-acre retail and restaurant development south of Prairie Star Parkway between Gleason Road and Dunraven Street. The applicant proposes to rezone the property from the AG, Agricultural District to the CP-2, Planned Community Commercial District. The applicant requests approval of a companion preliminary plan/plat for the construction of nine pad sites of varying size for retail, restaurant, gas station, and office uses. The development includes construction of a new east-west internal drive, an intersecting drive from Prairie Star Parkway, a central pedestrian circulation area, and related infrastructure improvements. The request is not consistent with the Future Land Use classification of "Office/Employment Center" within the Comprehensive Plan, and Staff recommends denial of the request. A Public Hearing is required for the rezoning request.

STAFF RECOMMENDATION: DENIAL

SITE INFORMATION

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
17.08	74,300	AG	Office/Employment Center

The property is currently undeveloped land located on the south side of Prairie Star Parkway between Dunraven Street and Gleason Road, approximately 0.35 miles from the Prairie Star Parkway and K-7 Highway intersection. The property was annexed into Lenexa’s city limits in 1993 with an initial zoning of AG and has not been rezoned or developed since. The property is generally flat with clusters of vegetation throughout. A small pond exists on the southeast corner of the property.

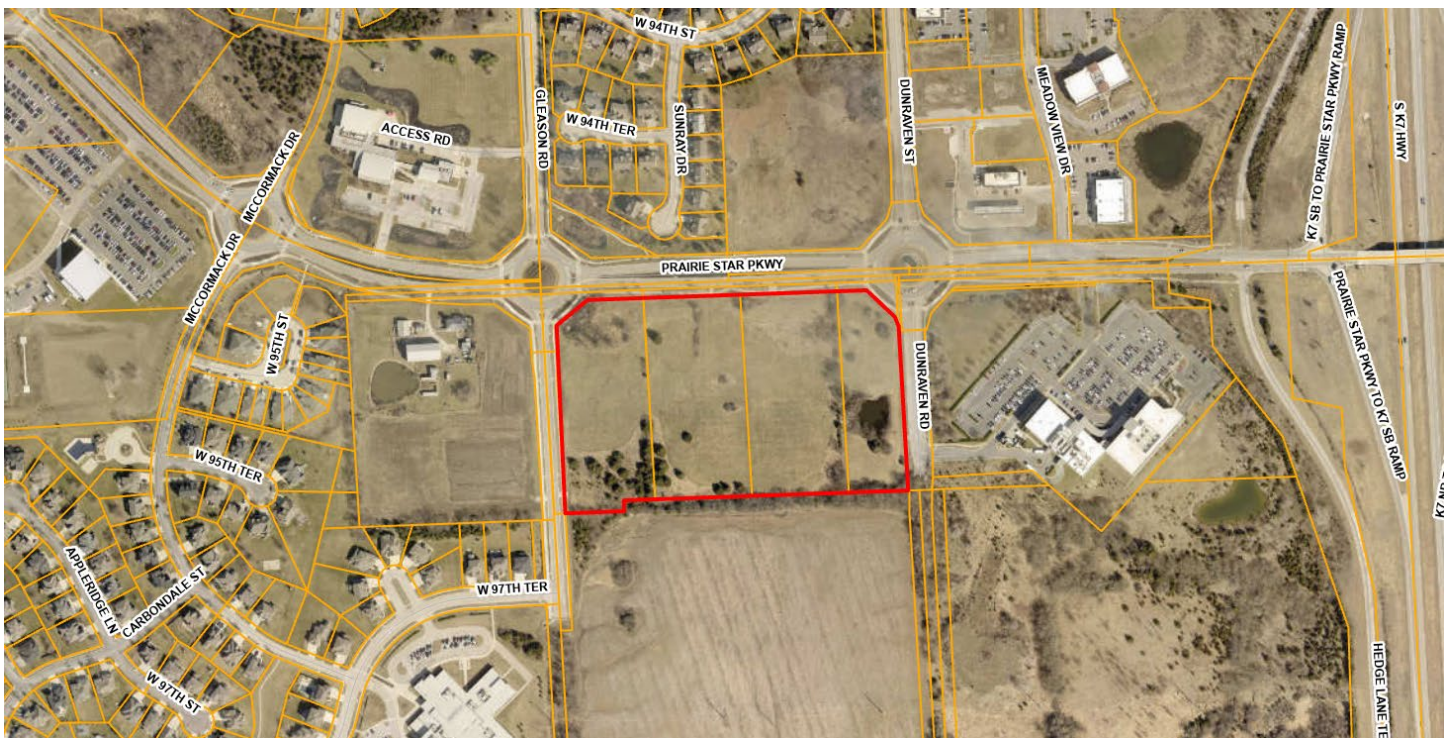


Exhibit 1: Aerial Image of Subject Site.



Exhibit 2: Birdseye Image of Subject Site (looking south).

LAND USE REVIEW

The proposed development is for retail, office, and restaurant uses. The Future Land Use Map (FLUM) within the Lenexa Comprehensive Plan, which was adopted in 2024, classifies this property as Office/Employment Center. According to the Comprehensive Plan, Office/Employment Centers are intended to accommodate a range of professional, administrative, research, and institutional activities that primarily occur within office-oriented environments and campus-like developments. These centers are intended for uses where little or no retail and commercial sales take place, instead focusing on employment-generating activities such as corporate offices, professional services, financial institutions, medical offices or campuses, research facilities, or other similar enterprises.

Development within this classification may take the form of campus-style office parks, business campuses, corporate headquarters, flex office development, or standalone office buildings occupied by a single tenant or multiple tenants. Office/Employment Centers are intended to support a concentration of jobs and provide opportunities for a diverse employment base. Buildings may vary in size and intensity, and development should incorporate complementary amenities such as meeting spaces, employee services, pedestrian connections, and landscaping.

The Comprehensive Plan also designates this property as a Neighborhood Node. Neighborhood Nodes are a strategy identified in the Vision 2040 Plan to help achieve the community’s long-term vision for fostering healthier residents, stronger social connections, and enhanced quality of life. These nodes are intended to create walkable, community-oriented destinations that provide opportunities for residents to gather, access services, and engage in civic and recreational activities close to where they live. Neighborhood Nodes may include a mix of recreation, civic, institutional, and neighborhood-scale commercial uses that serve surrounding neighborhood or employment centers to contribute to a more connected and vibrant community.

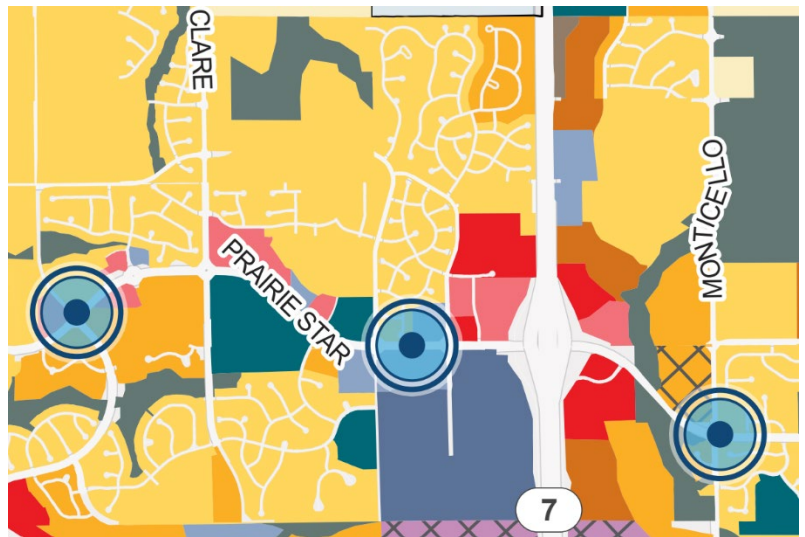


Exhibit 3: Excerpt from the Neighborhood Nodes Map.

An important component of Neighborhood Nodes identified in the Comprehensive Plan is the incorporation of third places, which are informal gathering spaces that exist outside of the home (the first place) and the workplace (the second place), providing settings where people can meet, interact, and build relationships. Traditionally, third places are areas that residents and visitors can utilize without needing to spend money. These spaces play a vital role in strengthening social cohesion, promoting community identity, and supporting physical and mental well-being of residents. Examples of third spaces can include pocket parks, plazas, public courtyards, outdoor seating areas, community greens, and other publicly accessible areas designed to encourage social interaction, pedestrian activation, and community activity. When thoughtfully designed, these spaces support community life by creating destinations that complement nearby neighborhoods, businesses, and employment centers. Over time, these nodes will help advance the City’s vision of creating places where residents can live, work, gather, and interact in ways that foster a strong sense of community and belonging.

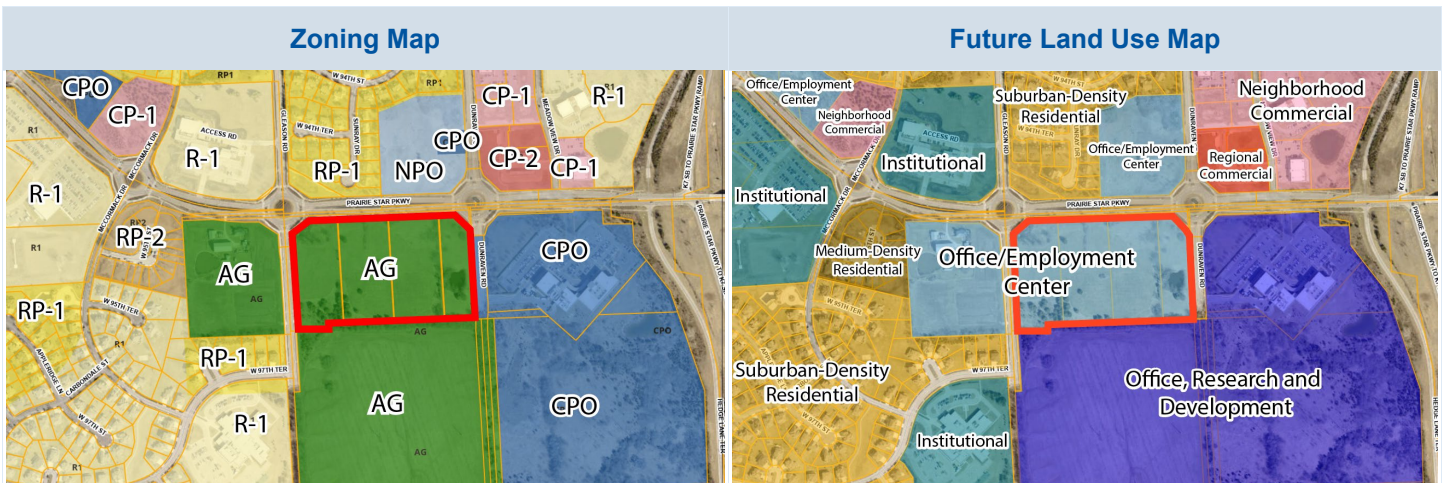


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Classification	Zoning	Current Use
Subject Property	Office/Employment Center	AG Agricultural District	Undeveloped
North	Suburban-Density Residential	RP-1 Residential Planned (Low-Density) District	Single-Family Residential
	Office/Employment Center	NP-O Planned Neighborhood Office District	Undeveloped
South	Office, Research & Development	AG Agricultural District	Undeveloped
East	Office, Research & Development	CP-O Planned General Office District	Medical Clinics, Hospital (AdventHealth Prairie Star)
West	Office/Employment Center	AG Agricultural District	Single-Family Residential

The AdventHealth Prairie Star Hospital was developed under the Office, Research & Development classification in the Future Land Use Map, and although a commercial development would not necessarily be in conflict with the medical use of the hospital, the intention of the Office/Employment Center Future Land Use classification is to provide employment-generating uses that can support the hospital’s operations and establish a cohesive employment district in this growing area of the City. The Comprehensive Plan contemplates that supporting commercial services may locate nearby; however, those uses are intended to complement, rather than replace, the employment-generating land uses planned for this corridor. The Office/Employment Center also may function as a buffer between existing single-family neighborhoods and the hospital by limiting the expansion of more intensive automobile-oriented commercial uses.

REZONING REVIEW

The applicant proposes to rezone the subject site from the AG, Agricultural District, to the CP-2, Planned Community Commercial District.

TABLE 2: REZONING ANALYSIS

Current Zoning	Proposed Rezoning

Staff provides the following analysis for the review criteria within Section 4-1-G-5 of the UDC.

1. The character of the neighborhood.

The character of the neighborhood is a mix of several types and intensities of uses. The site is located south of Prairie Star Parkway approximately 0.35 miles from the Prairie Star Parkway and K-7 Highway intersection with more intense land uses located immediately around the interchange and less intense uses located and planned as one travels away from the interchange.

2. The zoning and use of properties nearby.

The zoning and uses of the adjacent properties vary. The zoning and land use of adjacent properties is noted in Table 1. The surrounding area consists of a mix of single-family residential and duplex neighborhoods, limited commercial development such as the Casey’s gas station and convenience store at the northeast corner of Prairie Star Parkway and Dunraven Street, medical uses such as the AdventHealth Prairie Star Hospital directly east of the subject site, and undeveloped property to the north and south of the site.

3. The suitability of the subject property for the uses to which it has been restricted.

The property is currently zoned for agricultural uses. The AG Zoning District is a holding zone utilized upon annexation if no definitive plan is proposed for a site. The site is physically suited for development of varying uses and intensity due to its generally flat terrain and access to collector and arterial roadways. The site, while physically flat and undeveloped, is not well suited for agricultural uses due to its direct access to Prairie Star Parkway and its proximity to K-7, which is a state highway, in addition to its location amongst an emerging area of suburban development.

4. The extent to which the proposed use will detrimentally affect nearby property.

The proposed CP-2 zoning and its uses are equivalent to the Community Commercial land use classification of the Comprehensive Plan. Community Commercial uses typically generate greater activity levels and longer hours of operation than Office/Employment Center uses. While these are appropriate uses in locations planned for Community Commercial development, they represent a substantially different development pattern than the Office/Employment Center designation identified for this property in the Comprehensive Plan.

Although the proposed development can be designed to comply with the dimensional standards of the CP-2 Zoning District, staff finds that the requested rezoning would introduce a development pattern centered primarily on retail, restaurant, and automobile-oriented commercial activity rather than the employment-generating office and institutional uses envisioned for this area. As a result, the request is not consistent with the City's adopted long-range land use framework.

5. The length of time the subject property has remained vacant as zoned.

The property is currently undeveloped and has been zoned AG since its annexation in 1993.

6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.

Denial of the requested rezoning would provide a benefit to the public health, safety and welfare by maintaining the property's development opportunity for Office/Employment Center uses. The applicant is seeking approval for a commercial retail and restaurant development that would introduce land use impacts that are more intense than those anticipated under the FLUM within the Comprehensive Plan.

Commercial retail and restaurant uses permitted within the CP-2 District typically generate higher levels of vehicle traffic throughout the day and into the evening due to customer visits, merchandise deliveries, and other service-related trips. These businesses also tend to operate for longer hours, resulting in activity that extends into evenings and weekends. In contrast, Office/Employment Center uses generally operate during traditional business hours and generate more predictable weekday commuting patterns associated with employment activities, along with lower levels of evening activity and noise. While the surrounding transportation network is capable of accommodating either development scenario, the proposed commercial development represents a materially different land use pattern and operational character than the Office/Employment Center designation envisioned for this property in the Comprehensive Plan.

While denial of the applicant's rezoning request may limit the property owner's ability to develop the site for the specific commercial retail and restaurant uses within the CP-2 District, the property would retain reasonable development potential under alternative zoning that conforms with the adopted FLUM and Comprehensive Plan. The site remains suitable for the uses allowed under the NP-O, Planned Neighborhood Office and CP-O, Planned General Office Zoning Districts.

During the review process, Staff provided the applicant an alternative development concept that would have allowed a limited amount of neighborhood-serving retail and restaurant uses near Prairie Star Parkway while preserving the majority of the property for office and employment uses consistent with the Comprehensive Plan. Specifically, Staff suggested limiting the commercial component to approximately three acres at the northeast corner of the site under CP-1 zoning, with the remainder of the property

developed in accordance with the Office/Employment Center designation (NP-O or CP-O zoning) and designed with a strong emphasis on community gathering spaces, pedestrian circulation, and site activation. The application before the Planning Commission does not incorporate this concept, and the applicant did not indicate a willingness to pursue the alternative or provide an explanation for declining it.

7. Recommendation of City's permanent professional staff.

See Staff's recommendation at the end of this report.

8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.

The Comprehensive Plan's Future Land Use Map (FLUM) classifies this property as Office/Employment Center, which is intended to "accommodate buildings used for conducting business where little or no sales, manufacturing, or warehousing occur". This classification is intended to accommodate employment-generating uses such as offices, professional services, medical offices, research facilities, and similar activities where retail sales are limited and support the primary employment function. This FLUM classification notes that campus-type developments as well as standalone buildings are acceptable but should consist of uses with very little commercial and retail activity.

The Comprehensive Plan recognizes that successful employment centers often benefit from nearby restaurants, retail services, and other supporting commercial amenities; however, it addresses that relationship by distinguishing between employment-supporting commercial uses and community-scale commercial development. Rather than designating this property as Community Commercial, the Comprehensive Plan intentionally identifies it as Office/Employment Center, where commercial activity is expected to remain limited and subordinate to employment-generating uses. Staff therefore finds the proposed CP-2 Zoning District exceeds the level of commercial activity contemplated by the adopted land use framework.

The Future Land Use classifications within the Comprehensive Plan are intended to serve as the basis for consideration of zoning amendments within the city, and each zoning district within the UDC is intended to implement those classifications. In the case of the Office/Employment Center FLUM classification, the NP-O, Planned Neighborhood Office and CP-O, Planned General Office districts, along with their allowed uses, implement the Office/Employment Center classification as reflected in their purpose statements by limiting commercial and retail uses and allowing more medical, office, and other low traffic-generating uses.

Conversely, the CP-2, Planned Community Commercial District, is largely intended to accommodate retail and other service uses for the community, and the uses allowed under this zoning district would typically be seen in shopping and retail areas of the city. The CP-2 Zoning District's purpose statement calls for "well-designed developments that incorporate multi-modal transportation access and implementation of 'community shopping area' principles of the Comprehensive Plan". This is in direct conflict with the Office/Employment Center classification's specification that little or no retail uses should be present. Based on the proposed preliminary plan, which identifies almost exclusively retail and restaurant uses and proposes drive-thru restaurants for 3 of the 5 buildings along Prairie Star Parkway's frontage, the intended uses at this site are high-traffic uses that anticipate a high number of daily visits and are overwhelmingly sales-driven operations. Although some buildings are designated on the proposed plan as possible office/retail uses, any uses allowed within the CP-2 District would be permitted at this property if the rezoning application were to be approved, and there would be no mechanism to reserve a certain portion of the site for exclusively office uses. Moreover, the buildings identified as

office/retail exhibit the architectural character of multitenant commercial buildings rather than the office-oriented building forms typically associated with employment centers, reinforcing the overall commercial character of the proposed development. Although individual tenants may occupy these spaces for office uses, the requested CP-2 zoning would allow the buildings to transition over time to any use permitted within the CP-2 District.

Additionally, the land use pattern intended for the areas west of K-7 is to concentrate more intense, auto-oriented, high-traffic uses in locations adjacent to the K-7 and K-10 Highway interchanges as opposed to establishing them along the frontage of Prairie Star Parkway. Prairie Star Parkway is intended to be a meandering boulevard designed to buffer traffic from local pedestrian and neighborhood amenities and create a scenic atmosphere as one travels through the western part of the city. This is reflected in the FLUM, which recommends designations such as Suburban- and Medium-Density Residential, Institutional, and Neighborhood (small-scale) Commercial along Prairie Star Parkway outside of the Prairie Star Parkway and K-7 Highway interchange.

The Community Commercial and Regional Commercial FLUM classifications are largely reserved for major intersections along K-7, K-10, I-435, and I-35 and are provided in good supply with some parcels in the subject area of the City already receiving development approvals. Incrementally converting land designated as Office/Employment Center to more retail-heavy zoning districts and uses diminishes the inventory of sites available for job-producing office and employment development. Once commercially zoned and developed, the opportunity for this property to accommodate the employment-focused development envisioned by the Comprehensive Plan would likely be permanently lost. By preserving the Office/Employment Center classification on this property, the City can support the continued expansion of less intense uses, maintain an appropriate transition to adjacent neighborhoods, and encourage future auto-oriented, commercial-retail investment to locate in areas specifically planned and designated to accommodate higher-intensity retail and restaurant development.

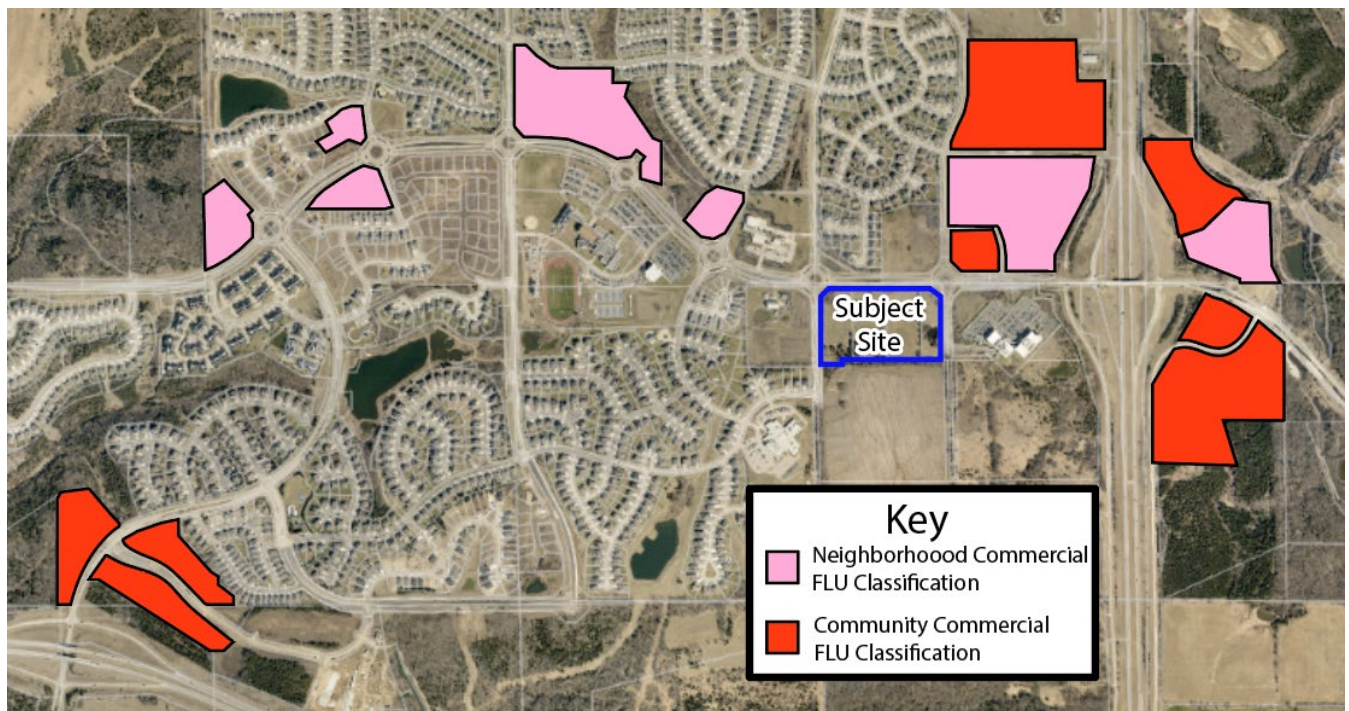


Exhibit 4: Map showing properties classified as commercial by the Future Land Use Map near the subject property.

The Comprehensive Plan identifies this property as part of a Neighborhood Node, which is intended to serve as a focal point for community activity through connected public spaces, pedestrian-oriented design, and amenities that encourage people to gather. While the preliminary plan includes sidewalks, an east-west walking trail, outdoor seating areas, and approximately 4,350 SF of community amenity space, the overall site design functions primarily as a collection of individual automobile-oriented pad sites separated by parking lots and internal drive aisles. Four drive-thru facilities line Prairie Star Parkway, parking areas occupy much of the street frontage between buildings, and most patios are located immediately adjacent to drive-thru lanes. Pedestrians traveling between destinations would be required to cross or walk alongside multiple parking lots and drive aisles, with several internal sidewalks located adjacent to dumpster enclosures. As viewed from Prairie Star Parkway, the development would read primarily as drive-thru buildings and parking areas rather than an integrated, walkable employment center or neighborhood gathering place. While the proposed pedestrian amenities represent an improvement over a conventional commercial center, they do not fundamentally alter the underlying land use pattern, which remains organized around retail pad sites, drive-thru facilities, parking lots, and internal drive aisles. Staff therefore finds the proposal does not achieve the balanced, employment-focused Neighborhood Node envisioned by the Comprehensive Plan.

Staff's concern extends beyond whether retail and restaurant uses could successfully operate at this location. The Comprehensive Plan intentionally identifies this property as part of a broader employment corridor that complements AdventHealth Prairie Star and preserves opportunities for future office, medical, research, and other employment-generating development. This policy direction is reflected not only in the Future Land Use Map, but also in the Comprehensive Plan's goals to foster a strong local economy by supporting quality office and employment areas (Goal 5), maintain high-quality working environments (Goal 6), and create neighborhood nodes that incorporate neighborhood-serving commercial uses as one component of a balanced, walkable development pattern (Goals 1 and 4).

Approval of the requested CP-2 Zoning District would introduce a fundamentally different land use pattern centered on community-scale retail, restaurant, and automobile-oriented commercial activity. While those uses are appropriate in locations planned for Community Commercial development, the Comprehensive Plan intentionally directs that form of development to designated commercial centers near major highway interchanges while reserving this property for employment-focused uses that reinforce the adjacent medical campus, provide long-term opportunities for job creation, and create an appropriate transition between surrounding neighborhoods and more intensive commercial areas.

9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.

Adequate utilities and services are or will be available to the subject property. The site is subject to the City's stormwater management requirements which are applicable to all development in the city. The site is within the Olathe School District.

10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.

The proposed development is not anticipated to adversely affect the overall capacity or safety of the adjacent street network. Based on Staff's transportation analysis, the surrounding street network can accommodate the traffic generated by the proposed development, and adequate on-site parking is provided in accordance with the applicable CP-2 zoning and dimensional standards.

Staff is aware of peak-hour issues regarding existing traffic congestion at the Prairie Star Parkway and K-7 Highway interchange. Traffic delays frequently occur at this interchange and the adjacent roundabouts along Prairie Star Parkway at peak travel times most often created by the dismissal of students at the end of the school day when St. James Academy is in session. The challenges associated with this interchange are primarily due to the existing constraints of the interchange, and are not attributable to any one development, school, or neighborhood. This interchange is under the jurisdiction of the Kansas Department of Transportation (KDOT), and any future improvements to the interchange would need to be implemented by and in coordination with KDOT.

While the proposed development would generate more traffic than the office and employment uses envisioned for this property, the surrounding transportation network is expected to accommodate either development scenario. Staff's concern is therefore not the operational capacity of the roadway system, but rather that the Comprehensive Plan anticipated this property developing with Office/Employment Center uses that generate more predictable weekday traffic patterns, fewer overall vehicle trips, and less evening and weekend activity than a community-scale commercial retail and restaurant center. As proposed, the development represents a materially different land use pattern and intensity than what was contemplated under the adopted long-range planning framework.

Overall, the proposed development is not expected to create unsafe conditions or overwhelm the street network, but it would introduce a higher intensity of traffic generation than what was envisioned by the Comprehensive Plan.

11. The environmental impacts the proposed use will generate including, but not limited to, excessive stormwater runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting, or other environmental harm.

The proposed rezoning is not anticipated to generate any environmental impacts exceeding the requirements of the UDC.

12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.

The site is subject to the UDC requirements for stormwater management and is required to meet the same standards as any new development.

13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.

The preliminary plan for the site is generally in conformance with the UDC requirements for the CP-2 Zoning District. The applicant is requesting deviations for eight internal setback encroachments, which are supported by Staff should the rezoning be approved, and are reviewed in more detail in the Deviation section of this report.

PRELIMINARY PLAN REVIEW

The overall development consists of nine separate pad sites, for a total of 74,300 SF of buildings, and an east-west and intersecting north-south drive aisle with sidewalks and a pedestrian circle. Of the nine buildings proposed, four include drive-thru configurations and are situated along Prairie Star Parkway, with parking oriented towards the internal drive aisle. The lot at the southwest corner of Dunraven Street and Prairie Star Parkway is proposed to be an eight-pump gas station and 5,000 SF convenience store. The larger 17,000 SF and 15,000 SF buildings are proposed at the south side of the site and are separated from the other pad sites by the east-west internal drive aisle, parking areas, and pedestrian circle.

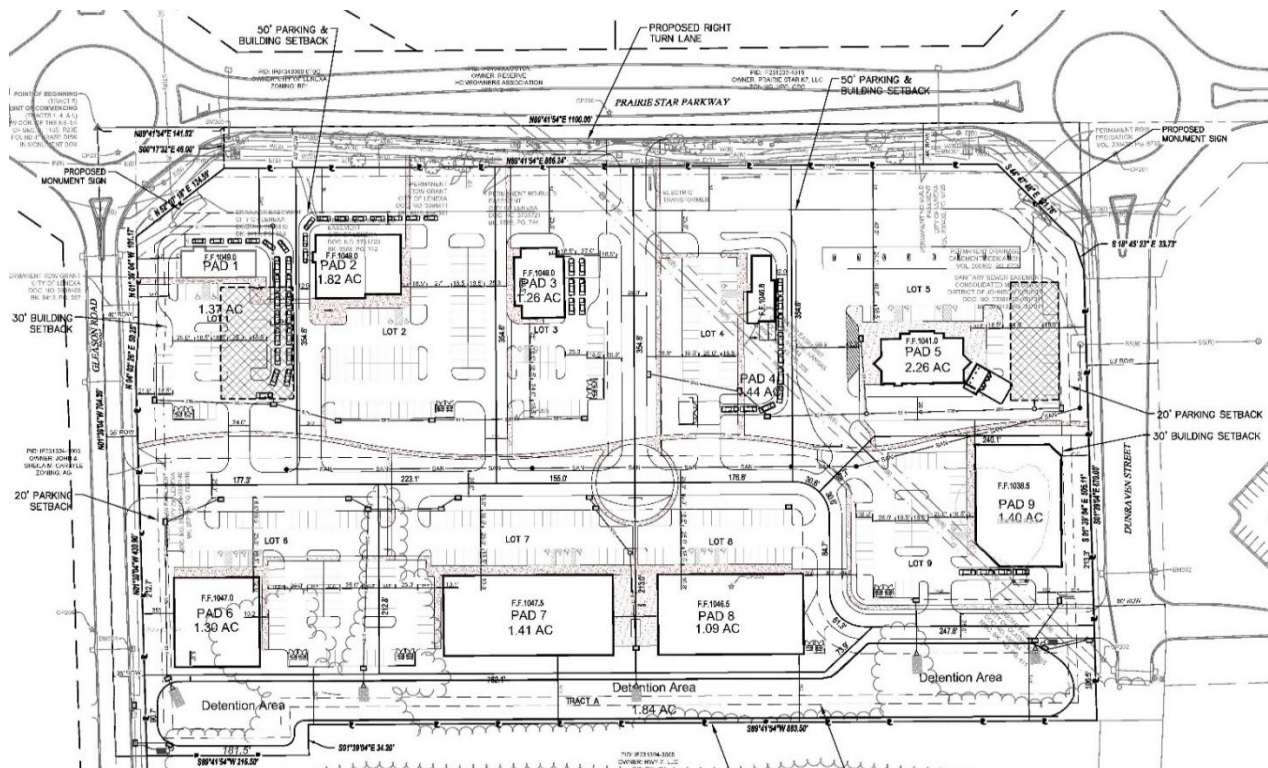


Exhibit 5: Preliminary Plan for Prairie Star Parkway Center

DIMENSIONAL STANDARDS

All nine proposed buildings meet the required setbacks from streets for the CP-2 Zoning District. Table 3 shows the required and proposed setbacks from streets.

TABLE 3: STREET SETBACK ANALYSIS			
Yard	Required Minimum Setback	Proposed Setback	Difference
From Gleason Road	30' for buildings 20' for parking areas	35'-49' for buildings 21.9' for parking areas	+ 5'-19'
From Dunraven Street	30' for buildings 20' for parking areas	31.1'-99' for buildings 29.3' for parking areas	+ 1.1-69'
From Prairie Star Parkway	50'	50'	0
From Internal Drive	30'	67' – 233'	36' – 203'

Deviations are requested for several buildings from internal lot lines. This is typical of a large commercial development such as this to accommodate shared access drives, internal circulation, and utility infrastructure, resulting in a coordinated site design. The requested deviations are reviewed more in depth in the Deviation section of this report. Table 4 shows each lot’s required and proposed internal setbacks (not adjacent to Prairie Star Parkway, Gleason Road, or Dunraven Street).

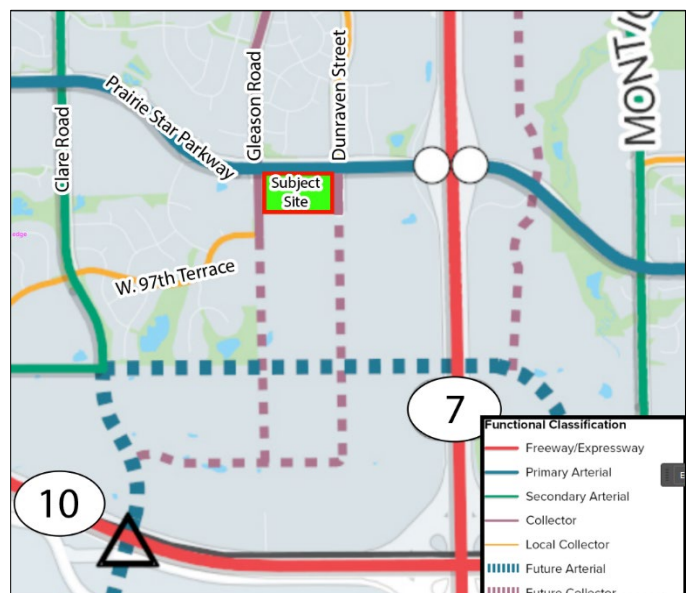
TABLE 4: INTERNAL SETBACK ANALYSIS			
Lot	Required Minimum Setback	Proposed Setback	Difference
1	30'	46' (east)	+ 16
2	30'	21.5' (west) 106' (east)	- 8.5' + 76'
3	30'	20' (west) 78' (east)	- 10' + 48'
4	30'	125' (west) 20' (east)	+ 95' - 10'
5	30'	58' – 91'	+ 28' - 61'
6	30'	9' (south) 133' (east)	-21' + 103'
7	30'	21' (south) 73' (west)	- 9' + 43'
8	30'	21' (south) 25' (west) 30' (east)	- 9' - 5' 0'
9	30'	10' (north)	- 20'

ACCESS, TRAFFIC, AND PARKING

The site will be accessed from Prairie Star Parkway to the north, Dunraven Street to the east, and Gleason Road to the west. An internal east-west main drive will bisect the site to allow access to internal lots and businesses. A north-south main drive will connect to the east-west drive, allowing cars entering the development from Prairie Star Parkway to access the various lots on the site. The preliminary plat shows the internal drives contained within their own tract to allow for one business entity for the development to retain ownership and maintenance responsibilities.

At Right: Exhibit 6: Excerpt from the Transportation and Mobility Network Map within the Comprehensive Plan.

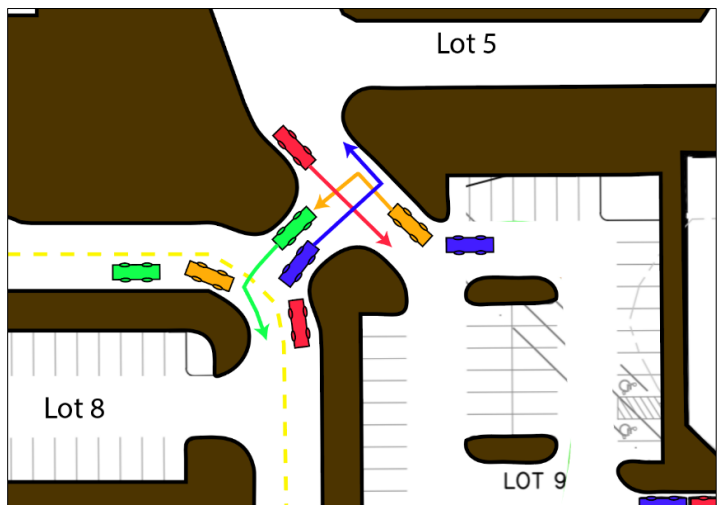
Prairie Star Parkway is classified as a primary arterial road, while Gleason Road and Dunraven Street are classified as collector roads. Existing Gleason Road continues past the subject property; however, existing Dunraven Street currently terminates at the southern property edge. Given the tapers at this termination, it is likely some minor modifications of Dunraven Street will be necessary to accommodate the new access for the development. Staff notes that the Transportation and Mobility Network Map within the Comprehensive Plan indicate that both roads are planned to extend southward toward a future east-west extension of 99th Street.



At the request of the City, the applicant prepared a traffic study for the proposed development. The study evaluated the street system adjacent to the development site including the roundabouts at Prairie Star Parkway and Gleason Road as well as at Prairie Star Parkway and Dunraven Street, the site’s access drives to Gleason Road and Dunraven Street, and the right-in/right-out access drive to Prairie Star Parkway. Also included in the analysis were the two ramp intersections at the K-7 Highway and Prairie Star Parkway interchange.

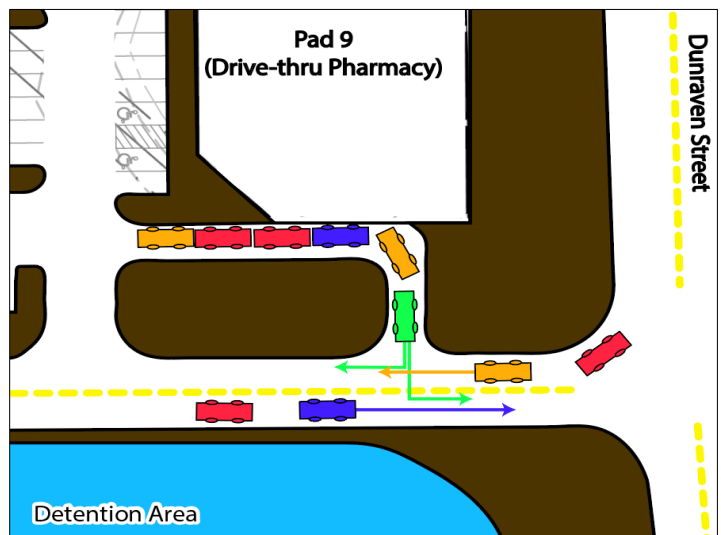
The study evaluated existing conditions, projected traffic conditions with the proposed development, and projected traffic conditions with the proposed development plus 20-years of growth in traffic on the adjacent street system. The analyses indicated that the existing roadway system can support the proposed development into the future with the exception of the interchange at K-7 and Prairie Star Parkway. This interchange is currently experiencing issues due to the limited amount of left turn storage on the bridge and limited capacity. Excessive queueing occurs along Prairie Star Parkway due to this limited capacity. The excessive queueing from the interchange has been observed to extend back through the roundabout at Dunraven Street for short periods of time when St. James Academy is dismissed. The analyses indicate that this will continue to be a problem until improvements are made to the interchange. It should be noted that the issues with this interchange are an existing issue that will need to be addressed regardless of what develops on this site. This interchange is under the jurisdiction of the Kansas Department of Transportation (KDOT), and any future improvements to the interchange would need to be implemented by and in coordination with KDOT.

Staff has concerns about two internal traffic patterns within the development. The first area of concern is located on the eastern half of the property where the internal drive turns south to create an “elbow” intersection and allow access to Lots 5 and 9. Exhibit 7 shows an example of how traffic conflicts could occur at this intersection and cause backups on the internal drive aisles.



At Right: Exhibit 7: Possible traffic conflicts at the elbow of the internal drive aisle.

The second area of concern is the exit from the drive-thru pharmacy on Lot 9. This drive aisle is an exit only for cars leaving the drive-thru pharmacy. However, due to its close proximity to the Dunraven intersection where cars will be entering and exiting the larger development, compounded with possible backups from the elbow intersection previously discussed, this may result in drivers not being able to exit the drive-thru. While this is more of an operational issue for the pharmacy than a safety concern for the intersection, and it may not be a frequent occurrence depending on the level of activity the drive-thru pharmacy receives, it is worth noting that if cars aren’t able to exit the drive-thru, the queueing may cause congestion into the parking area for Lot 9 and the internal thru street.



At Right: Exhibit 8: Possible pharmacy drive-thru exit issues.

Surface parking is provided for each pad site and is oriented away from the exterior streets adjacent to the site. A total of 408 parking spots are required for the development as a whole. Table 5 shows the required and provided parking spaces for each lot/pad site. Although each lot may not meet the specific parking requirement of the proposed uses, the larger development will function much like a regional shopping center where shared parking is common for multi-tenant spaces. At the time of Certificate of Occupancy, Staff reviews every tenant’s proposed use and required minimum parking ratio to ensure that the parking area can satisfy all tenants’ required parking minimums.

TABLE 5: PARKING ANALYSIS					
Lot/Building Number	Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
1	Restaurant	1 space per 75 SF	34	35	+1
2	Restaurant	1 space per 75 SF	88	78	-10
3	Banking Institution	1 space per 200 SF	20	23	+3
4	Restaurant	1 space per 75 SF	30	33	+3
5	Gasoline Sales & Convenience Store	1 space per 250 SF	20	30	+10
6	Office/Retail	1 space per 250 SF	36	74	+38
7	Office/Retail	1 space per 250 SF	68	60	-8
8	Office/Retail	1 space per 250 SF	60	31	-29
9	Retail (Pharmacy)	1 space per 250 SF	52	44	-8
Total			408	408	0

Queueing requirements for drive-thru facilities are established in UDC Section 4-1-D-1-N. Adequate queueing spaces are provided for all three drive-thru restaurant pad sites, as well as the bank proposed on Lot 3. A minimum queueing requirement is not established for a drive-thru pharmacy use as is proposed on Lot 9 and is to be determined by the City based on the provided traffic study for the development. Staff determined that the four queueing spaces provided on the preliminary plan are adequate for a drive-thru pharmacy.

PUBLIC IMPROVEMENTS

A right-hand turn lane will be required for the proposed right-in/right-out on Prairie Star Parkway. The existing drive approaches on Prairie Star Parkway and Gleason Road will be removed by the developer at the time of public improvement permit.

There is an existing multiuse trail along the site’s Prairie Star Parkway frontage.

STORMWATER

A preliminary stormwater management study was provided which shows the intent to meet the City’s stormwater standards and requirements. This will be accomplished through a combination of underground detention, above ground detention, and proprietary water quality treatment units. While Staff is still working through some of the technical design details of this system, as noted it does show the intent to meet the City’s requirements.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed code review will be conducted based on the adopted codes at the time of the building permit submittal.

LIGHTING

A photometric plan was provided by the applicant and is compliant with UDC Section 4-1-C-4-I. The site is adequately lit and not excessively bright.

LANDSCAPING

The landscape plan shows trees and shrubs installed around the site perimeter boundaries, street frontages, and the parking lot. A detailed landscape plan is typically not shown with the preliminary plan. The preliminary plan will show the applicant’s intent to meet the numerical quantity and placement requirements of the UDC. Exhibit 9 shows the proposed preliminary landscape plan.

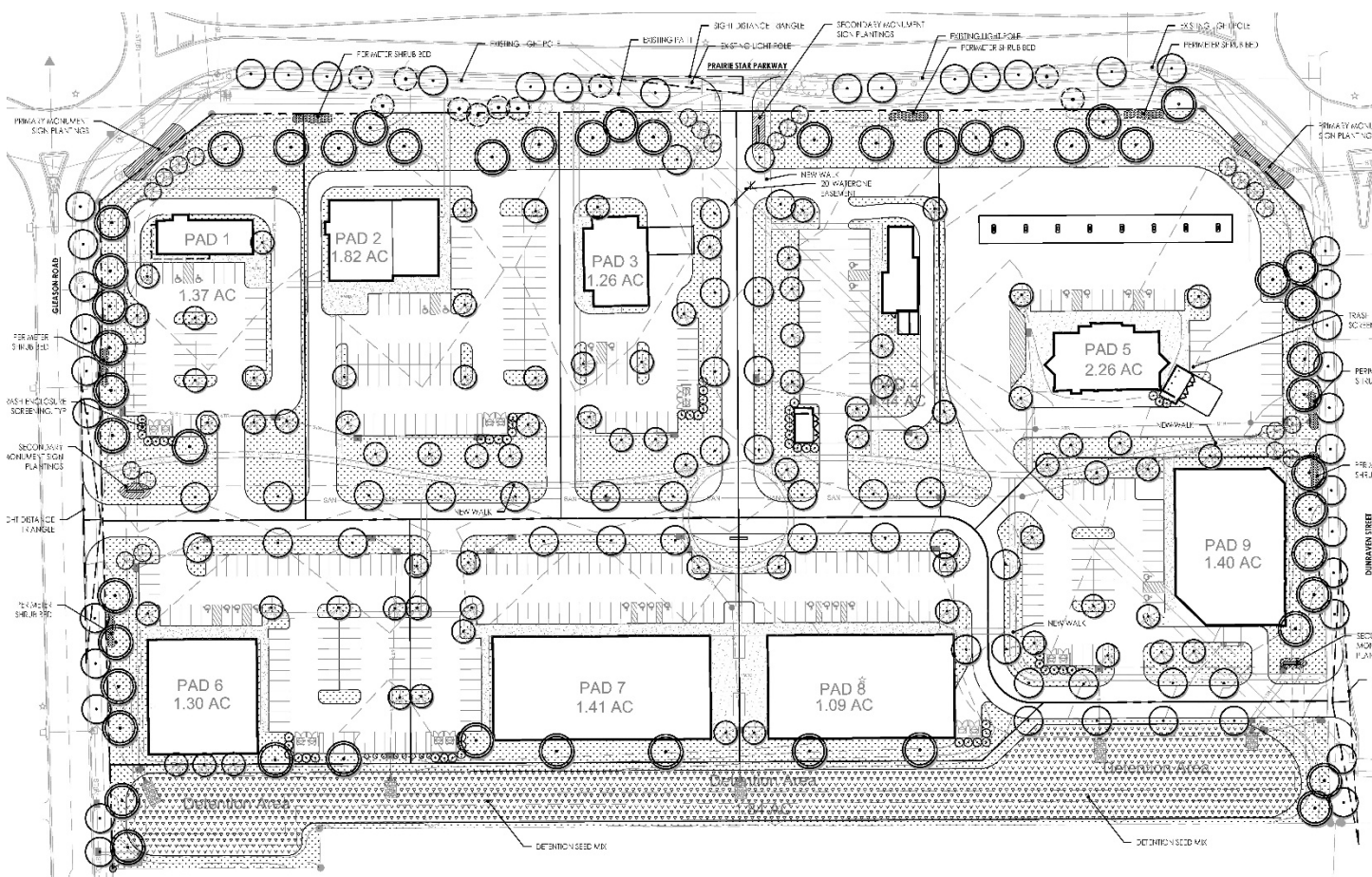


Exhibit 9: Preliminary Landscape Plan

The preliminary plan provides an east-west walking trail, small patios for the restaurant sites, and a roughly 4,350 SF “community amenity space” in an effort to increase the pedestrian connections at the site; however, the spaces provided do not offer meaningful activation aside from simply existing, and although incorporating sidewalks and spaces for customers to eat outside is certainly desirable for a retail and restaurant development, it does little to strengthen community ties as is called for at properties designated as Neighborhood Nodes. All

patios proposed for the restaurant pads are oriented directly adjacent to drive-thru lanes, and pedestrians would have to walk through parking areas to get from one building to another, even if they are on the sidewalk. The central “pedestrian circulation circle” does not show any seating areas, additional “above and beyond” landscaping elements, or permanent activation elements that might encourage pedestrians to spend time in the space. It is also located within what is likely to be the most highly trafficked intersection in the development.



Exhibit 10: Rendering of the site showing the pedestrian circle at the intersection of the two internal drives and the community amenity space between Lots 7 and 8.

ARCHITECTURE

The preliminary plan proposes several different building types depending on the proposed use for each pad. All buildings are made up of the same materials and general style to create a cohesive feel throughout the development. Buildings feature brick and siding exteriors with large glass storefronts, awnings, and stone wainscoting. The gas station pump island is proposed to be made of brick, with a canopy to resemble other gas station islands in the area. The orientation and specific design of each individual building is to be further detailed in the final plan for the property.

The convenience store is proposed to be a 5,000 SF one-story building at the southeast corner of Prairie Star Parkway and Dunraven Street and is shown in Exhibit 11. The 4,000 SF bank proposed on Lot 3, and the two 9,000 SF and 1,300 SF commercial buildings on Lots 6 and 9 are shown in Exhibit 12. The three drive-thru restaurant buildings range between 2,200 SF and 6,600 SF, and an example of their possible design is shown in Exhibit 13. The two larger 15,000 SF – 17,000 SF buildings on Lots 7 and 8 are shown in Exhibit 14. All buildings are compliant with the CP-2 Zoning District’s maximum height of 45’.

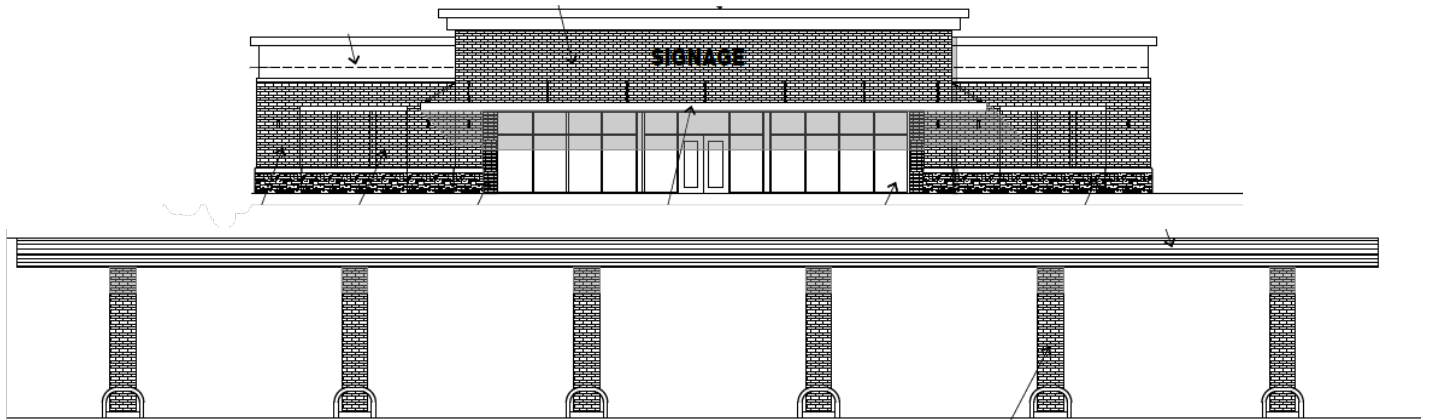


Exhibit 11: Elevation for the convenience store and gas station on Lot 5.

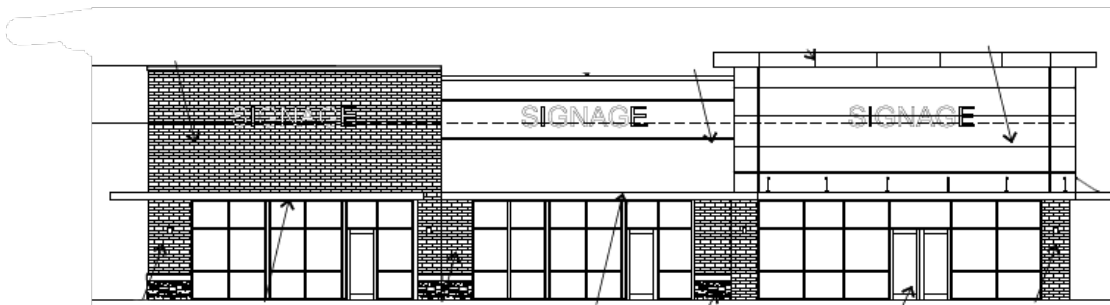


Exhibit 12: Elevation for the small commercial buildings on Lots 3, 6, and 9.

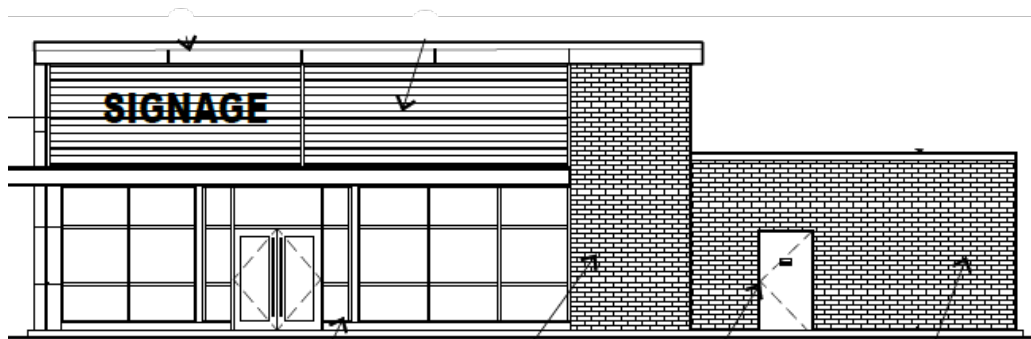


Exhibit 13: Elevation of the drive-thru restaurant buildings on Lots 1, 2, and 4.

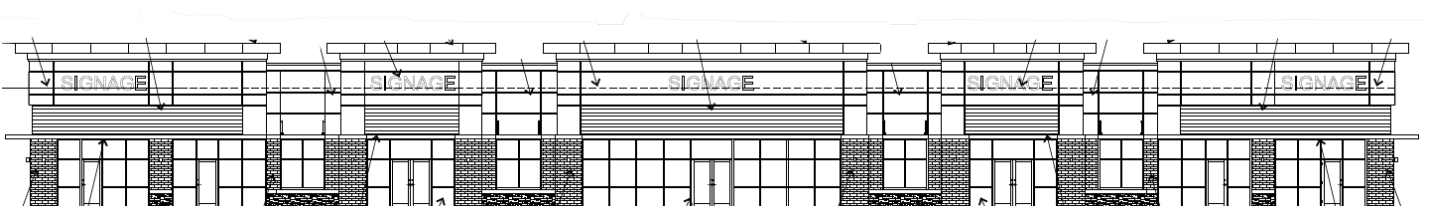


Exhibit 14: Elevation for the larger commercial buildings on Lots 7 and 8.

DEVIATIONS

The applicant is requesting deviations to reduce the minimum setback established for the CP-2, Planned Community Commercial District from 30' to a lesser setback ranging from 8.5'-25' as shown in Table 7. The requested deviations would only be applicable to internal lot lines, and all setbacks from Prairie Star Parkway, Gleason Road, and Dunraven Street comply with the dimensional standards of the CP-2 Zoning District.

TABLE 7: DEVIATION REQUESTS

Deviation Type	Lot	Requirement	Proposed	Deviation Request
Internal Setback	2	30'	21.5' (from west property line)	- 8.5'
Internal Setback	3	30'	20' (from west property line)	- 10'
Internal Setback	4	30'	20' (from east property line)	- 10'
Internal Setback	6	30'	9' (from south property line)	-21'
Internal Setback	7	30'	21' (from south property line)	- 9'
Internal Setback	8	30'	21' (from south property line) 25' (from west property line)	- 9' - 5'
Internal Setback	9	30'	10' (from north property line)	- 10'

DEVIATION ANALYSIS

According to Section 4-1-B-27-G-4 of the UDC, the Planning Commission shall have the authority to approve deviations from the minimum standards for setbacks where there is ample evidence that such deviations will not adversely affect neighboring properties and surrounding areas and where such deviations do not constitute the granting of a privilege that would not be universally appropriate for other similarly designed and situated developments.

1. That such deviation will not adversely affect neighboring properties and surrounding areas.

Staff does not anticipate any adverse impacts on neighboring properties and surrounding areas from the requested deviations. The proposed deviations are for setbacks that are internal to the larger site, and all setbacks to the adjacent street network are met with the proposed preliminary plan. The deviations do not create additional impacts related to traffic, noise, lighting or stormwater beyond those otherwise associated with the site's uses and overall design.

2. That such deviations do not constitute the granting of a privilege that would not be universally appropriate for other similarly designed and situated developments.

The requested deviation does not constitute a special privilege, as it reflects a condition commonly found on similarly designed and situated developments. The requested deviations involve reducing interior setbacks between buildings and interior lot lines within a unified commercial development.

Interior setback reductions are common in multi-building commercial and pad site developments, and flexibility in building placement is often necessary to accommodate shared access drives, adequate internal circulation, utilities and other infrastructure elements, and a coordinated site design. Such deviations allow the site to function efficiently. The requested deviations are consistent with those that would be considered for other commercial developments with comparable site layout and constraints.

NEXT STEPS

- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration by the City Council on July 21, 2026.
- The applicant must submit a final plan and final plat application prior to applying for permit(s).
- The final plat must be recorded with Johnson County prior to permit(s) being released.
- The applicant must receive permit(s) prior to commencing construction.
- The applicant must obtain a Certificate of Occupancy before opening for business.
- The applicant must obtain a Business License prior to opening for business.
- The applicant should inquire about additional City requirements and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

★ Conduct a Public Hearing.

★ Staff recommends denial of the proposed rezoning and preliminary plan/plat for Prairie Star Parkway Center.

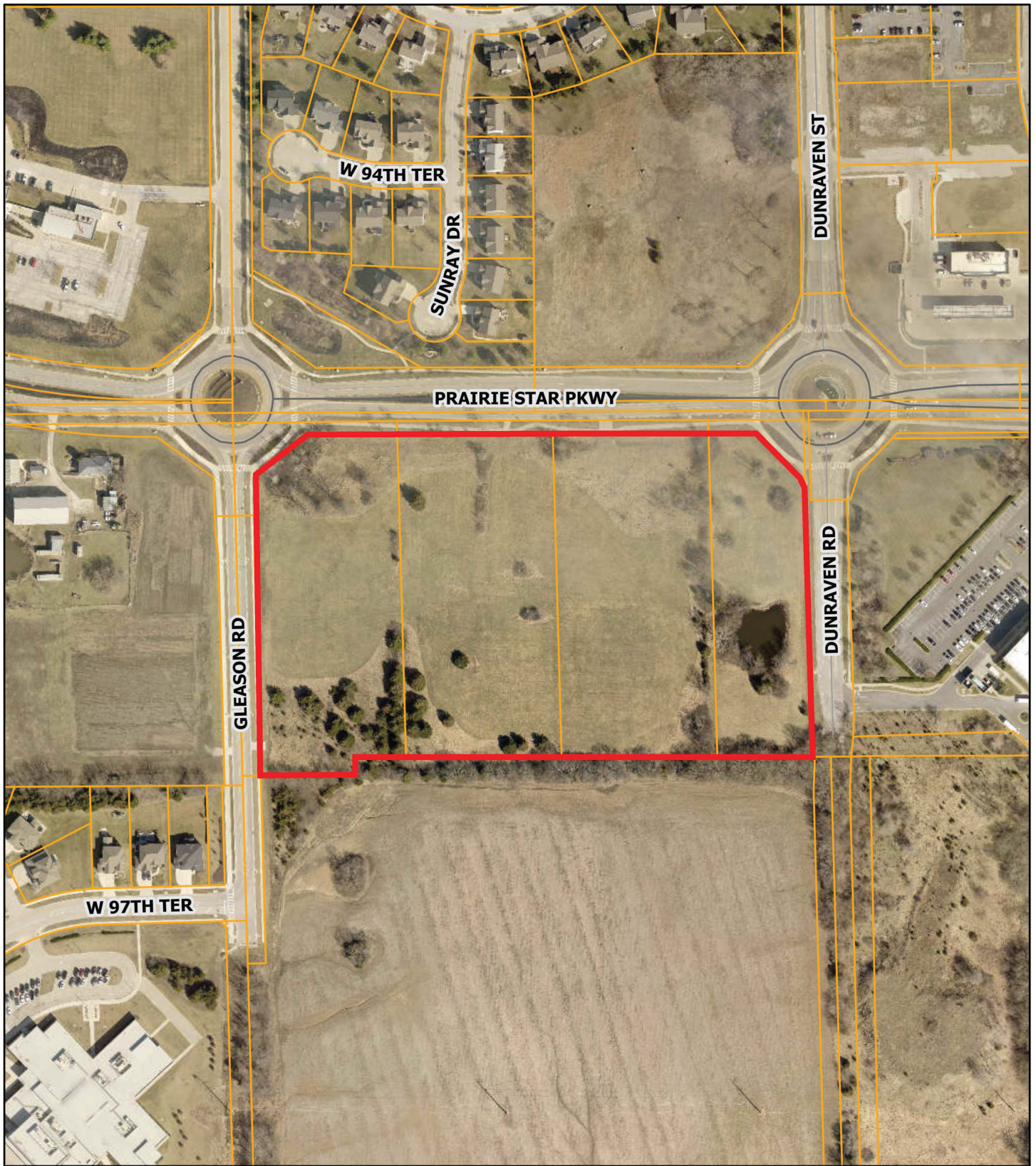
- Staff recognizes that the proposed development may represent a viable commercial project. However, the principal question before the Planning Commission is whether the requested zoning and associated land use pattern implement the City's adopted Comprehensive Plan. Based upon the analysis contained within this report, Staff finds they do not.
- The proposed CP-2 Zoning District and preliminary plan are inconsistent with the Comprehensive Plan's Office/Employment Center Future Land Use designation, which reserves the property for office, medical, research, and other employment-generating uses that reinforce the adjacent medical campus and support the City's long-term employment base.
- Approval of the request would replace the Comprehensive Plan's envisioned employment-focused development pattern with a community-scale commercial center, reducing opportunities for future job-generating investment and diminishing the City's ability to implement its adopted long-range land use strategy for this area of Lenexa.
- Although the proposal incorporates pedestrian amenities, the overall site design functions primarily as an automobile-oriented commercial center rather than the balanced, walkable Neighborhood Node envisioned by the Comprehensive Plan.
- Accordingly, the proposal does not advance the City's Vision 2040 goals of fostering a *Thriving Economy*, creating *Inviting Places*, and promoting *Healthy People*, nor does it further the Guiding Principles of *Responsible Economic Development* and *Strategic Community Investment*.

REZONING

Staff recommends **DENIAL** of rezoning property from AG to CP-2 for RZ26-03 – **Prairie Star Parkway Center** located south of Prairie Star Parkway between Gleason Road and Dunraven Street, for a retail and restaurant development.

PRELIMINARY PLAN/PLAT

Staff recommends **DENIAL** of the preliminary plan/plat for PL26-06P – **Prairie Star Parkway Center** located south of Prairie Star Parkway between Gleason Road and Dunraven Street, for a retail and restaurant development.



Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Prairie Star Parkway Center



0 175 350 700 Feet



Project Narrative

Date March 27th 2026

SW corner of Prairie Star and Dunraven
Lenexa, KS, 662127

Preliminary Development Plan application for: Prairie Star Parkway & K7 Development

The Prairie Star Parkway and K7 Development is located on the south side of Prairie Star Parkway between Dunraven and Gleason road.

The development is a mixed-use environment following the planning goals of Lenexa and will serve as a place for commerce offering additional retail, restaurant, c-store, and office uses for everyday trips and needs for the neighboring residential. With a pedestrian oriented design and public space amenities at the entrance of the buildings and intentionally planned throughout the site, the pedestrian experience is enhanced encouraging more people to consider walking for short trips.

The site features a primary east west spine with a northern right in right out entrance along Prairie Star. The plan meets the goals of the neighborhood node by providing an intentionally designed overall streetscape and connectivity featuring large open greenspaces with walking paths, central plaza nodes along the main site axis, and direct connections between all the different buildings on the site. The site slopes generally down from north to south and has detention along the south as a buffer to neighboring property.

Public open space

As noted there are several public open spaces around the site. There is a main central court along the main roads with planned plaza space and a winding east west connection with wide greenspace. There are also several different patio areas connected to the buildings for future restaurant users that further activate the pedestrian experience.

K7 and Prairie Star Parkway offers a thoughtfully designed retail commercial area that provides convenient, walkable access to everyday goods and services for the surrounding community. Anchored by familiar storefronts and complemented by inviting pedestrian spaces, it fosters a sense of place while supporting local needs and encouraging neighborhood connection.

End of Narrative

Excerpt from the Applicant's 06/22/2026 Response to Comments Letter:

“While the Future Land Use Map identifies the subject property as part of an Employment Center, the Comprehensive Plans ultimate objective is not office buildings for their own sake. The objective is to create employment-generating, economically productive development that supports major employment anchors and contributes to the City’s tax base. Many office users increasingly prioritize mixed-use environments with nearby amenities rather than isolated office campuses. The hospital functions as a major employment anchor and regional destination. Hospitals generate demand from:

- Employees
- Physicians
- Patients
- Patient families and visitors
- Vendors and contractors

The proposed commercial zoning advances those objectives by:

- Supporting employment generated by the adjacent hospital and medical campus
- Creating additional jobs and services that complement existing employment uses
- Increasing economic activity and tax revenue
- Providing amenities necessary to attract and retain employers and employees in the area

Thus, the rezoning furthers the underlying policy goals of the Employment Center designation.

The revised development plan represents an evolution from earlier concepts and was intentionally redesigned to better align with the Comprehensive Plan's vision for a Neighborhood Node and an integrated employment-focused environment. A primary objective of the redesign was to reduce the dominance of vehicular infrastructure and create a more connected, pedestrian-oriented development while still allowing for pad sites. To accomplish this, roadways and parking areas were consolidated and reduced where feasible, allowing for substantial increases in contiguous greenspace throughout the site. The resulting site plan is organized around a series of interconnected open space amenities that provide both visual character and meaningful opportunities for pedestrian movement. A primary north-south greenspace corridor serves as the central organizing feature of the development, creating a highly visible pedestrian spine that connects multiple areas of the project. This corridor culminates in a circular paved gathering area and amenity space designed to serve as a focal point for employees, visitors, and patrons of the development.

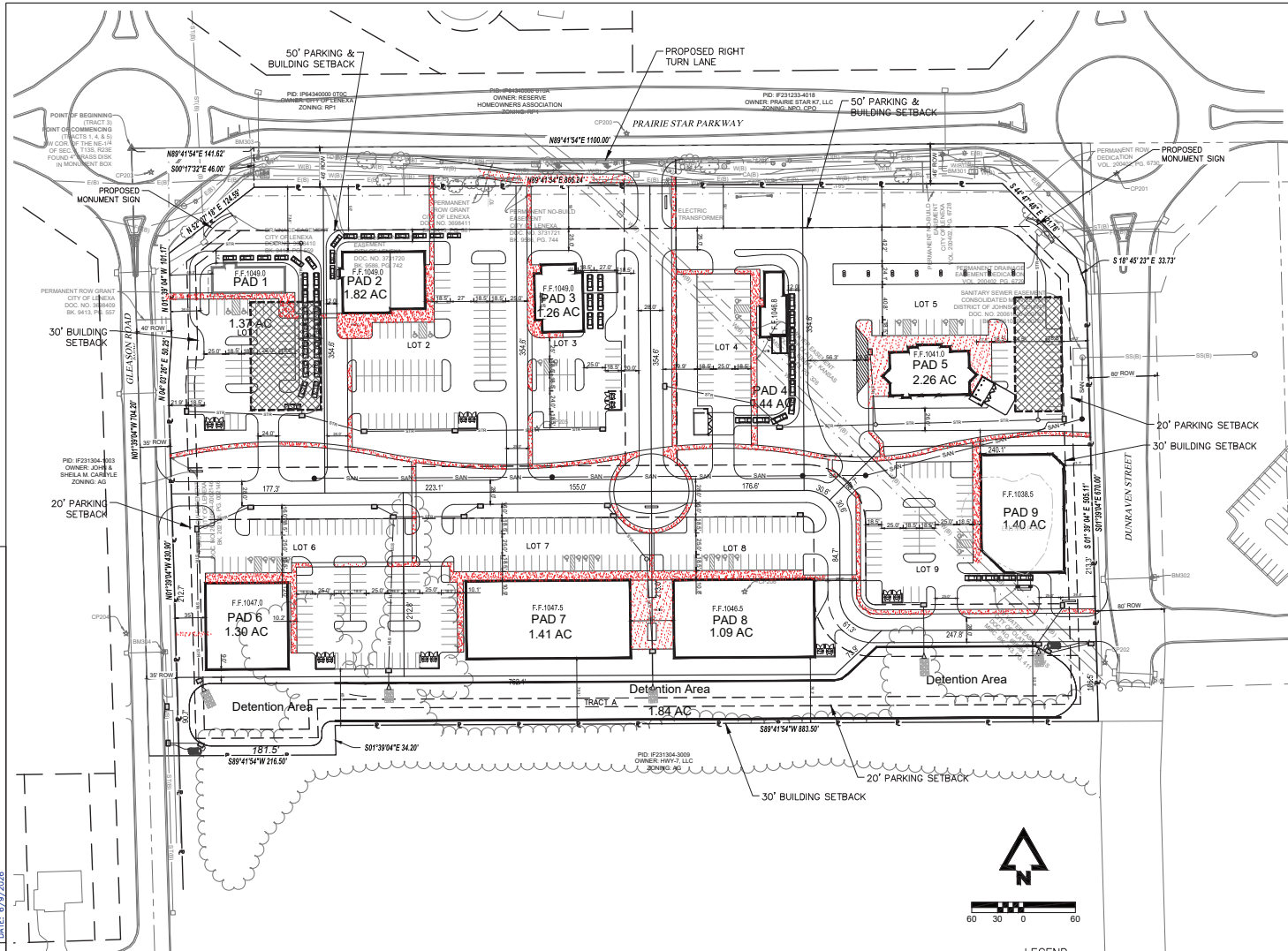
Complementing this feature is an enhanced east-west greenway corridor approximately 50 to 60 feet in width. This greenway incorporates a multi-use trail connection that links the various development pads and intersects with the north-south greenspace corridor, creating a cohesive network of pedestrian routes throughout the site. Rather than isolated sidewalks serving individual buildings, the revised plan establishes a continuous and scenic circulation system that encourages walking between destinations and provides a stronger sense of place.

Further strengthening the Neighborhood Node concept, the plan incorporates a programmed plaza area between Pads 7 and 8 on the south portion of the site. This space serves as the terminus of the north-south greenspace corridor and includes a trellis feature, landscaped seating areas, and gathering spaces intended to encourage social interaction and outdoor activity. These amenities were deliberately incorporated to create meaningful common areas that can support employee gatherings, informal meetings, and community interaction. Pedestrian connectivity throughout the development was comprehensively reevaluated during the redesign process. Sidewalk networks were expanded and reconfigured to provide direct access between buildings, parking areas, open spaces, and trail connections while minimizing pedestrian and vehicular conflict points. These improvements result in a development pattern that prioritizes walkability and internal connectivity rather than isolated building sites connected primarily by drive aisles.

Additionally, the reduction and consolidation of parking and internal drive lanes allowed for the creation of a larger, more contiguous stormwater management and open space area along the rear of the property. This not only improves the visual character of the development but also contributes to the overall open space framework and environmental quality of the site. Taken together, these revisions transform the development from a collection of individual building pads into a cohesive campus-style environment centered around interconnected greenspaces, pedestrian amenities, gathering areas, and recreational opportunities. The revised plan reflects a deliberate effort to create the type of connected, human-scaled development envisioned by the Comprehensive Plan's Neighborhood Node designation while providing an attractive and functional environment for future employment, service, and supporting commercial uses.

Today's successful office and employment developments rely upon integrated restaurants, coffee shops, fitness uses, and service-oriented commercial spaces that support employees throughout the workday. The adjacent hospital along K7 and Prairie Star Parkway makes the subject site a natural location for service-oriented commercial spaces to accommodate service needs of the hospital which has no retail or restaurant type uses on this side of Prairie Star Parkway. This development compliments the existing

commercial uses to the northeast while still allowing the remaining portions of Prairie Star Parkway to be developed with low intensity uses. The proposed development aligns with the goals of the land use plan and staff's comments of keeping auto-oriented uses close to K7 and Prairie Star Parkway intersection as much as possible while offering a complimentary use to the adjacent hospital employment center.”



DESCRIPTION:
A tract of land in the Northeast Quarter of Section 4, Township 13 South, Range 23 East, in the City of Lenexa, Johnson County, Kansas, and with the bearing calls stated herein being based on the Kansas Regional Coordinate System, Zone 11 (Kansas City) and with said tract being more particularly described as follows:

BEGINNING at the Northwest Corner of said Northeast Quarter of Section 4, thence North 89°41'54" East along the North Line of said Northeast Quarter of Section 4, a distance of 1100.00 feet to the Northwest Corner of SHAWNEE MISSION MEDICAL CENTER, a platted subdivision of land in the City of Lenexa, Johnson County, Kansas; thence departing said North Line of the Northeast Quarter, South 01°39'04" East, along the West Line of said subdivision, a distance of 670.00 feet to the Southwest Corner of said subdivision; thence South 89°41'54" West, parallel with the North Line of said Northeast Quarter, a distance of 883.50 feet; thence South 01°39'04" East, a distance of 34.20 feet; thence South 89°41'54" West, parallel with the North Line of said Northeast Quarter, a distance of 216.50 feet to a point on the West Line of said Northeast Quarter; thence North 01°39'04" West, along said West Line, a distance of 704.20 feet to the place of beginning.

McCLURE
making lives better.

11031 Strong Line Road
Lenexa, Kansas 66215
P 913-888-7800
F 913-888-4868

Total Gross Land Area = 744,196 SF = 17.084 ± Acres

NOTICE:
McClure Engineering Company warrants only and all responsibility and liability for problems which arise from failure to follow these Plans, Specifications, and the engineering intent they convey, or for problems which arise from failure to obtain and/or follow the engineers guidance with respect to any errors, omissions, inconsistencies, ambiguities, or conflicts which are alleged.

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KANSAS CERTIFICATE OF AUTHORITY NO. E2723
EXPIRES: DECEMBER 31, 2026

SITE DEVELOPMENT DATA

EXISTING ZONING: AG - AGRICULTURE DISTRICT
PROPOSED ZONING: CP2 - PLANNED COMMUNITY COMMERCIAL
SITE LAND AREA: GROSS = 17,084 ± ACRES
NET = 15,917 ± ACRES

BLDG/LOT	LOT AREA BLDG SF	USE	PARKING REQUIRED	PARKING PROVIDED
1	1.37 AC, 6,200 SF	RESTAURANT	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 25 SPACES (INCLUDES 2 ADA SPACES) BIKE 1 SPACE
2	1.02 AC, 4,000 SF	RESTAURANT	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 25 SPACES (INCLUDES 2 ADA SPACES) BIKE 2 SPACES
3	1.36 AC, 4,000 SF	BANK	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 25 SPACES (INCLUDES 2 ADA SPACES) BIKE 1 SPACE
4	1.44 AC, 2,200 SF	RESTAURANT	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 25 SPACES (INCLUDES 2 ADA SPACES) BIKE 1 SPACE
5	2.28 AC, 5,000 SF	CONVANCE STORE	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 35 SPACES (INCLUDES 4 ADA SPACES) BIKE 1 SPACE
6	1.30 AC, 6,000 SF	OFFICE/RETAIL	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 25 SPACES (INCLUDES 4 ADA SPACES) BIKE 1 SPACE
7	1.41 AC, 17,000 SF	OFFICE/RETAIL	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 60 SPACES (INCLUDES 4 ADA SPACES) BIKE 2 SPACES
8	1.09 AC, 10,000 SF	OFFICE/RETAIL	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 25 SPACES (INCLUDES 4 ADA SPACES) BIKE 1 SPACE
9	1.40 AC, 13,000 SF	DRUG STORE	CAR 175 SF = 34 SPACES BIKE 1,000 SF = 1 SPACE	CAR 35 SPACES (INCLUDES 2 ADA SPACES) BIKE 2 SPACES
TRACT A 1.84 AC (STORMWATER DETENTION BASIN TRACT)				

OVERALL PARKING SUMMARY:
TOTAL REQUIRED PARKING = 408 SPACES
TOTAL PROVIDED PARKING = 408 SPACES

SITE DEVELOPMENT OPEN SPACE AREAS

BLDG	OPEN SPACE AREA	OPEN SPACE %
1	28,038 SF	44%
2	21,869 SF	30%
4	30,145 SF	21%
5	32,550 SF	32%
6	14,433 SF	41%
7	13,029 SF	25%
8	12,576 SF	27%
9	19,691 SF	47%
TRACT A	79,972 SF	100%

Total Open Space Area = 281,041 SF = 43%
Total Impervious Area = 380,045,102 SF

PRAIRIE STAR PKWY & K-7 DEVELOPMENT
LENEXA, KANSAS
PRELIMINARY SITE PLANS

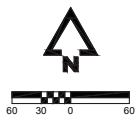
PRELIMINARY SITE PLAN & GENERAL LAYOUT

LENEXA, KANSAS
PROJECT NO. 202502531-000 PSD/IMV
ISSUE DATE: 3-30-2026

ENGINEER: PLK
DRINK: BGL
CHECKED BY: TWS
FIELD BOOK NO.:

DEVELOPER: HWY 7 LLC
23 FEDERAL STREET, STE. 1000
BOSTON, MA 02110-1937
ATTN: JEFF BOYCE
PHONE: (913) 227-7400
EMAIL: BOYCE@PEAKINVESTMENTS.COM

ENGINEER: McCLURE CO.
11031 STRONG LINE ROAD
LENEXA, KANSAS 66215
ATTN: TOM SMITH
PHONE: (913) 888-7800
FAX: (913) 888-7800
EMAIL: TOM.SMITH@McCLUREVISION.COM



LEGEND

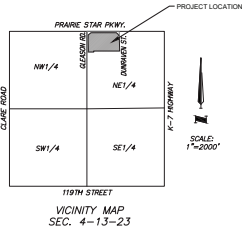
- CONCRETE SIDEWALK
- FH FIRE HYDRANT
- WV WATERLINE VALVE
- PROPERTY LINE
- WATER LINE
- SAN SANITARY LINE (PROP)
- SAN SANITARY SEWER (EXIST)
- STR STORM LINE
- UP UNDERGROUND ELEC.
- C GAS
- FO FIBER OPTIC SERVICE LINE
- UNDERGROUND TELEPHONE
- DW DOMESTIC WATERLINE
- FPL FIRE PROTECTION WATERLINE

CP#	NORTHING	EASTING	ELEV.	DESCRIPTION
200	547941.28	1181073.03	1048.94	SET 12" IRON ROD W/ MEC CAP
201	547898.87	11811280.27	1034.56	SET 1" CUT ON TOP OF CURB
202	547528.49	11811288.84	1029.56	SET 12" IRON ROD W/ MEC CAP
203	547896.50	11810182.97	1040.93	SET 1" CUT
204	547382.22	11810133.67	1045.38	SET 12" IRON ROD W/ MEC CAP
205	547599.32	11810609.26	1050.79	SET 12" IRON ROD W/ MEC CAP
206	547416.57	11810840.72	1046.89	SET 12" IRON ROD W/ MEC CAP

BMM	NORTHING	EASTING	ELEV.	DESCRIPTION
301	547916.58	11810185.89	1038.34	SQUARE CUT AT SW CORNER OF CURB
302	547428.83	11811231.47	1030.17	SQUARE CUT ON CURB INLET
303	547933.57	11810286.51	1041.92	SQUARE CUT ON CURB INLET
304	547430.89	11810718.94	1044.29	SQUARE CUT ON CURB INLET

NOTE:
Parking spaces shall be 9'x18.5'.
NOTE:
All existing wells, cisterns and septic systems shall be removed.
NOTE:
There are no known mined, underground space areas within this project.

GENERAL NOTES:
The subject property address is: Not Available according to the Johnson County AIMS website.
At the time of this survey there was no building on the subject property.



P: 202502531-000 CAL-DRAWINGS CIVIL/PLD DOCUMENTS 202502531-000 PSD/IMV LAYOUT: PSD-C-100 DATE: 6/9/2026 BY: GWATSON



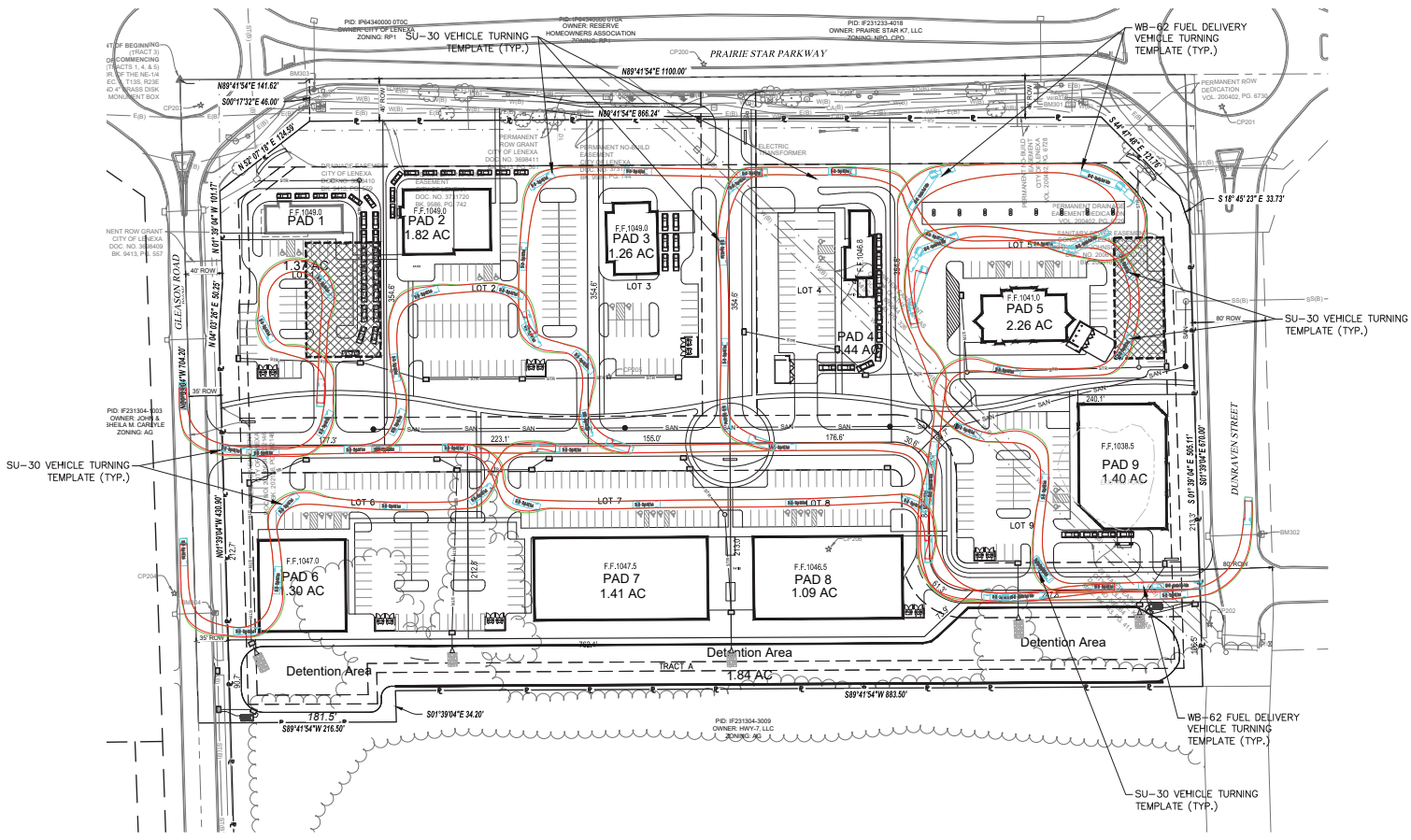
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11031 Strang Line Road
Lenexa, Kansas 66215
P 913-888-7800
F 913-888-4868

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Plans, Specifications, and the
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for problems which arise from
failure to obtain and/or follow
the engineers guidance with
respect to any errors, omissions,
inconsistencies, ambiguities, or
conflicts which are alleged.

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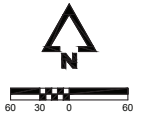
**KANSAS CERTIFICATE OF
AUTHORITY NO. E2723
EXPIRES: DECEMBER 31, 2026**



ZONING NOTE:
Existing Zoning: AG—Agriculture District.
Proposed Zoning: CP2 — Planned Community Commercial

NOTE:
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NOTE:
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NOTE:
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LEGEND

FH	FIRE HYDRANT
WV	WATERLINE VALVE
—	PROPERTY LINE
W	WATER LINE
SAN	SANITARY LINE (PROP)
SAN	SANITARY SEWER (EXIST)
STR	STORM LINE
UP	UNDERGROUND ELEC.
G	GAS
FO	FIBER OPTIC SERVICE LINE
T	UNDERGROUND TELEPHONE
DW	DOMESTIC WATERLINE
FPL	FIRE PROTECTION WATERLINE

DESCRIPTION:
A tract of land in the Northeast Quarter of Section 4, Township 13 South, Range 23 East, in the City of Lenexa, Johnson County, Kansas, and with the bearing calls stated herein being based on the Kansas Regional Coordinate System, Zone 11 (Kansas City) and with said tract being more particularly described as follows:

BEGINNING at the Northeast Corner of said Northeast Quarter of Section 4, thence North 89°41'54" East along the North Line of said Northeast Quarter of Section 4, a distance of 1100.00 feet to the Northwest Corner of SHAWNEE MISSION MEDICAL CENTER, a platted subdivision of land in the City of Lenexa, Johnson County, Kansas; thence departing said North Line of the Northeast Quarter, South 01°39'04" East, along the West Line of said subdivision, a distance of 670.00 feet to the Southwest Corner of said subdivision; thence South 89°41'54" West, parallel with the North Line of said Northeast Quarter, a distance of 883.50 feet; thence South 01°39'04" East, a distance of 34.20 feet; thence South 89°41'54" West, parallel with the North Line of said Northeast Quarter, a distance of 216.50 feet to a point on the West Line of said Northeast Quarter; thence North 01°39'04" West, along said West Line, a distance of 704.20 feet to the place of beginning.

Containing 744,196 square feet or 17.084 acres, more or less.

DEVELOPER:
HWY 7 LLC
73 FEDERAL STREET, STE. 1000
BOSTON, MA 02110-1937
ATTN: ME BOYCE
PHONE: (913) 227-7400
EMAIL: BOYCE@PEAKINVESTMENTS.COM

ENGINEER:
McClure CO.
11031 STRANG LINE ROAD
LENEXA, KANSAS 66215
ATTN: TOM SMITH
PHONE: (913) 888-7800
FAX: (913) 888-7868
EMAIL: TOM.SMITH@MCCCLUREVISION.COM

PRAIRIE STAR PKWY &
K-7 DEVELOPMENT
LENEXA, KANSAS
PRELIMINARY SITE PLANS

PRELIMINARY SITE PLAN
SU-30 TURNING PLAN

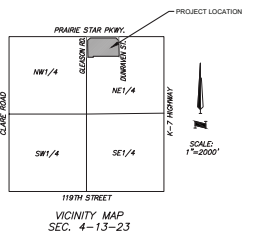
LENEXA, KANSAS
PROJECT NO. 2025002531-000
ISSUE DATE: 3-30-2026
REVISION:
6.09.2026-City Review Comments

ENGINEER
PLK
CHECKED BY
TWS

DRAWN BY
BGL
FIELD BOOK NO.

SHEET NO.
C - 1.02

P: 2025002531-000-CA--DRAWINGS-CIVIL-P&S DOCUMENTS 2025002531-000-P&S.DWG
LAYOUT: PSD - C-1.02
DATE: 6/9/2026
BY: GWATSON





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**KANSAS CERTIFICATE OF
AUTHORITY NO. E2723
EXPIRES: DECEMBER 31, 2026**

PRELIMINARY PLAT OF PRAIRIE STAR PARKWAY & K-7 DEVELOPMENT

PART OF THE NE 1/4 OF SECTION 3, TOWNSHIP 13, RANGE 23,
CITY OF LENEXA, JOHNSON COUNTY, KANSAS

DESCRIPTION:

A tract of land in the Northeast Quarter of Section 4, Township 13 South, Range 23 East, in the City of Lenexa, Johnson County, Kansas, and with the bearing calls stated herein being based on the Kansas Regional Coordinate System, Zone 11 (Kansas City) and with said tract being more particularly described as follows:

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Containing 744.196 square feet or 17.084± acres, more or less.

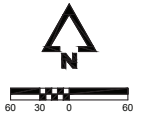
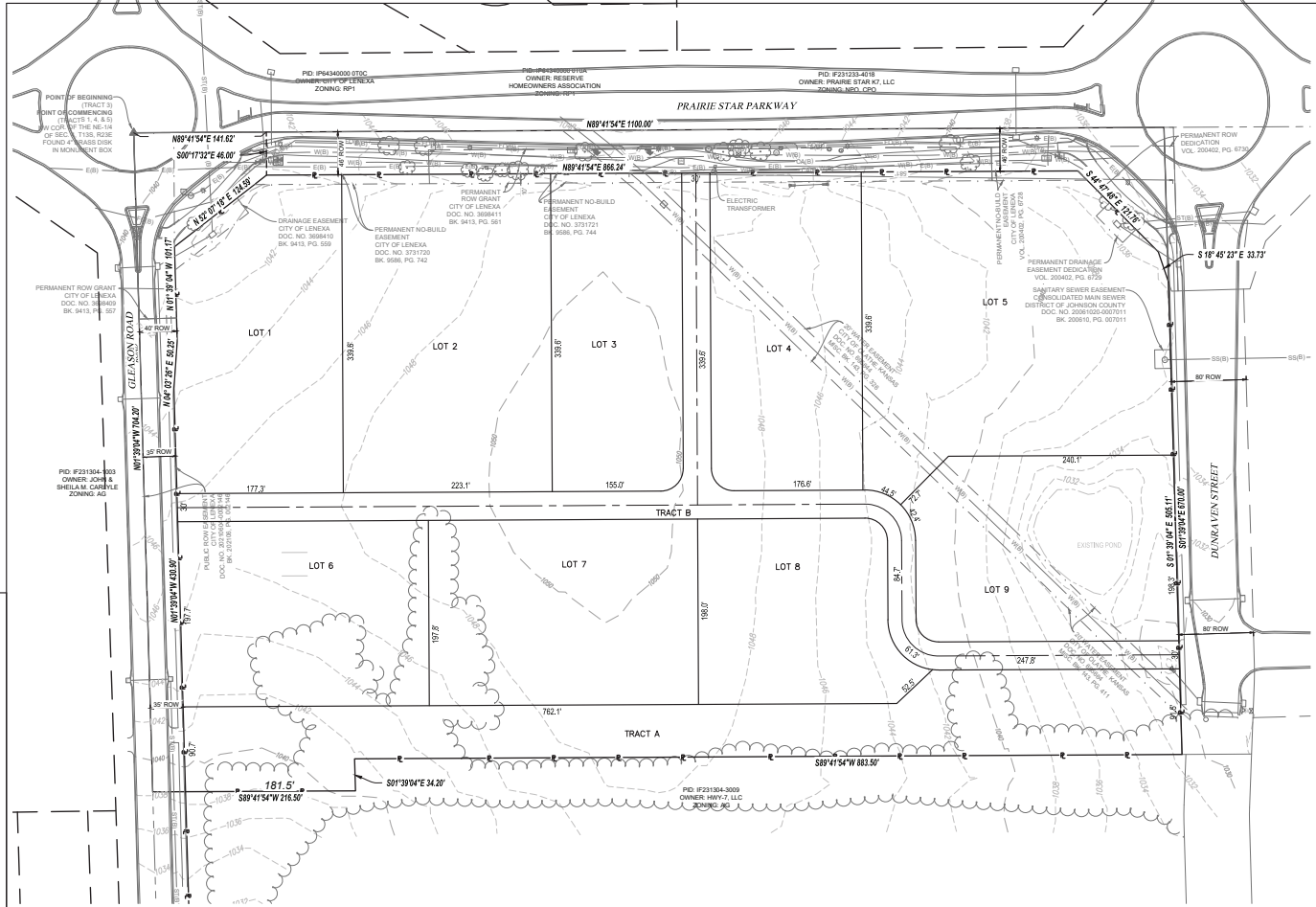
Gross Area = 744.196± Sq. Ft. or 17.084± Acres

Total Gross Land Area = 17.084± Acres

- Lot 1 Area = 1.31± Acres (NET area is based on all proposed and existing road rights-of-way)
- Lot 2 Area = 1.74± Acres (NET area is based on all proposed and existing road rights-of-way)
- Lot 3 Area = 1.09± Acres (NET area is based on all proposed and existing road rights-of-way)
- Lot 4 Area = 1.26± Acres (NET area is based on all proposed and existing road rights-of-way)
- Lot 5 Area = 2.25± Acres (NET area is based on all proposed and existing road rights-of-way)
- Lot 6 Area = 1.20± Acres (NET area is based on all proposed and existing road rights-of-way)
- Lot 7 Area = 1.31± Acres (NET area is based on all proposed and existing road rights-of-way)
- Lot 8 Area = 0.97± Acres (NET area is based on all proposed and existing road rights-of-way)
- Lot 9 Area = 1.25± Acres (NET area is based on all proposed and existing road rights-of-way)
- Tract A Area = 1.75± Acres (NET area is based on all proposed and existing road rights-of-way)
- Tract B Area = 1.06± Acres (NET area is based on all proposed and existing road rights-of-way)

PROJECT CONTROL POINTS TABLE				
CPI#	NORTHING	EASTING	ELEV.	DESCRIPTION
200	54794.128	1161071.533	1546.29	SET 12" IRON ROD W/ MEC CAP
201	54784.67	1161282.27	1534.89	SET 1" CUT ON TOP OF CURB
202	54738.48	1161266.64	1529.56	SET 12" IRON ROD W/ MEC CAP
203	54786.59	1161042.57	1540.93	SET 1" CUT
204	54782.22	1161033.67	1545.38	SET 12" IRON ROD W/ MEC CAP
205	54769.32	1161069.28	1550.79	SET 12" IRON ROD W/ MEC CAP
206	54743.67	1161044.72	1546.80	SET 12" IRON ROD W/ MEC CAP

PROJECT BENCHMARK TABLE				
BMM#	NORTHING	EASTING	ELEV.	DESCRIPTION
301	54791.887	1161076.88	1536.29	SQUARE CUT AT SW CORNER OF CURB
302	54763.83	1161021.47	1530.17	SQUARE CUT ON CURB INLET
303	54793.57	1161026.51	1541.82	SQUARE CUT ON CURB INLET
304	54740.80	1161076.94	1544.29	SQUARE CUT ON CURB INLET



LEGEND

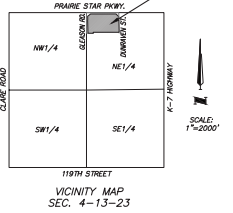
- FH - FIRE HYDRANT
- WV - WATERLINE VALVE
- - PROPERTY LINE
- W - WATER LINE
- SAN - SANITARY LINE (PROP)
- SAN - SANITARY SEWER (EXIST)
- STR - STORM LINE
- UP - UNDERGROUND ELEC.
- G - GAS
- FO - FIBER OPTIC SERVICE LINE
- T - UNDERGROUND TELEPHONE
- DW - DOMESTIC WATERLINE
- FPL - FIRE PROTECTION WATERLINE

ZONING NOTE:

Existing Zoning: AG—Agriculture District.
Proposed Zoning: CP2 — Planned Community Commercial

NOTE:
Parking spaces shall be 9'x18.5'.
NOTE:
All existing wells, cisterns and septic systems shall be removed.
NOTE:
There are no known mined, underground space areas within this project.

GENERAL NOTES:
The subject property address is: Not Available according to the Johnson County ARES website.
At the time of this survey there was no building on the subject property.



P: 202502531-0001-CA-DRAWINGS-CIVIL-PSD DOCUMENTS 20250223-1-000-PSD.DWG
LAYOUT: PRELIM PLAT C-2-00 EPT: GWATSON
DATE: 4/29/2026

PRAIRIE STAR PKWY &
K-7 DEVELOPMENT
LENEXA, KANSAS
PRELIMINARY SITE PLANS

PRELIMINARY PLAT

LENEXA, KANSAS
PROJECT NO. 202502531-000
ISSUE DATE: 3-30-2026
revised
6.09.2026-City Review Comments
6.25.2026-City Comment - add tract B

ENGINEER: PLK
DRINK: BGL
CHECKED BY: TWG
FIELD BOOK NO.

SHEET NO.
C - 2.00

DEVELOPER:
HWY 7 LLC
73 FEDERAL STREET, STE. 1000
BOSTON, MA 02110-1937
ATTN: ME BOYCE
PHONE: (913) 227-7400
EMAIL: BOYCE@PEAKINVESTMENTS.COM

ENGINEER:
MCCLURE CO.
11031 STRANG LINE ROAD
LENEXA, KANSAS 66215
ATTN: TOM SMITH
PHONE: (913) 888-7800
FAX: (913) 888-7808
EMAIL: TOM.SMITH@MCCLUREVISION.COM

LANDSCAPE NOTES

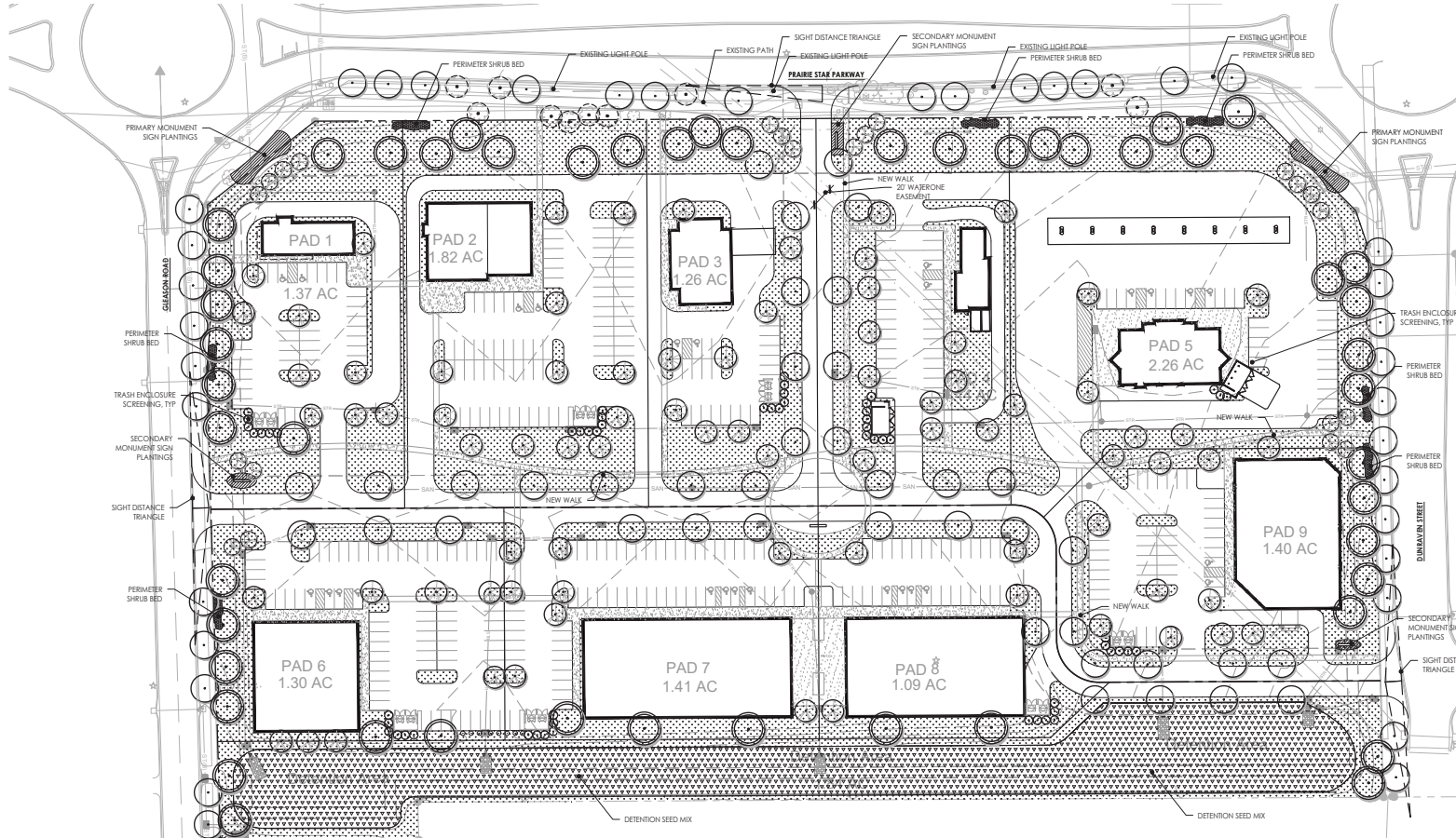
- All landscaping and buffering shall be installed according to standards of the most recent edition of American Standard for Nursery Stock, published by the American Association of Nurserymen.
- All landscape areas will comply with code requirements for landscape irrigation per Article 4-1-D-2 Landscaping, Buffering, and Site Design, R.3, Irigation.
- DEFINITION: SEED MIX: AS 327 Conservative Mix, CFA3 Prairie Sigs - Wet-Mix; 2020
 - 3.1 Big Bluestem • Fox sedge, Brown top sedge • Canada wild-rye • Dudley's rush • Switch grass • Indian grass • Stenderleaf fable long-awn • Meadow zernone • Spotted Joe-Pye weed • Common horsetail • Sarsaparilla • Great St. Johns wort • Great blue lobelia • Wild bergamot • Common evening primrose • 58ff goldenrod • Show goldenrod • Slender mountain mint • Yellow coneflower/flower-eyed susan • New England aster • Blue-veronica • Culver's root • Golden alexanders
 - 3.2 3 YEAR ESTABLISHMENT PERIOD.
 - 3.3 3.1 SOWING, DRILL SEED WITH HYDRIMULCH COVER.
 - 3.4.1 YEAR 1: MOW MONTHLY (12 HI) TO SUPPRESS WEEDS, LEAVE DEAD VEGETATION OVER WINTER FOR INSULATION
 - 3.4.2 YEAR 2: MOW IN MID-SPRING AS CLOSE TO THE GROUND AS POSSIBLE AND RAKE CUTTINGS.
 - 3.4.3 YEAR 3: IF BURNING IS NOT ALLOWED, MOW TO GROUND MID-SPRING AND RAKE CUTTINGS. THIS IS 60% AS EFFECTIVE AS BURNING.

PLANTING LEGEND

	EXISTING TREE TO REMAIN
	STREET TREE
	SHADE TREE
	PERIMETER TREE
	PARKING LOT TREE
	ORNAMENTAL TREE
	DECIDUOUS / EVERGREEN SHRUBS
	EVERGREEN SCREEN
	LANDSCAPE TURF
	DEFENTION SEED MIX

LANDSCAPE REQUIREMENTS - K7 & PRAIRIE STAR PARKWAY DEVELOPMENT

Code	Requirement	Location	Dimension	Required	Provided		
4-1-D-2 LANDSCAPING, BUFFERING, AND SITE DESIGN							
J - Street Trees	One (1) tree shall be planted for each 40 LF of public or private street frontage	Prairie Star Parkway	960 LF	24 Trees	18 Trees		
		Gleason Road	620 LF	16 Trees	13 Trees		
		Dunraven St	580 LF	15 Trees	13 Trees		
L - Perimeter Plantings	Street Frontage (Arterial & Collector, 35' min depth); Per 100 LF: 2 shade trees, 1 orn tree, 12 shrubs, & 75 SF bed	Prairie Star Parkway	960 LF	19 Sh. Trees 10 Orn Trees 115 Shrubs 720 SF Bed	22 Sh. Trees 15 Orn Trees 115 Shrubs 720 SF Bed		
		Gleason Road	620 LF	13 Sh. Trees 8 Orn Trees 75 Shrubs 465 SF Bed	13 Sh. Trees 8 Orn Trees 75 Shrubs 465 SF Bed		
		Dunraven St	580 LF	12 Sh. Trees 6 Orn Trees 70 Shrubs 435 SF Bed	14 Orn Trees 10 Shrubs 435 SF Bed		
		Building	467 LF	5 Trees	5 Trees		
	M - Internal Parking Lot Landscape Area	Trash Enclosure Screening, Typ	Parking Lot	124 LF	3 Trees	2 Trees	
		7.5% Internal Landscape Area (Office & Commercial)	Service Area, previous and	Service Area	0 LF	0	0
			Pad 1	7.5%	10%		
			Pad 2	7.5%	10%		
			Pad 3	7.5%	12%		
			Pad 4	7.5%	20%		
Pad 5			7.5%	12%			
Pad 6			7.5%	9%			
Pad 7	7.5%		9%				
N - Land Use Buffers	57.48% Overall Impervious Surface LU 7 (General commercial)	RP-1 (North); LU 3; Arterial Rd	505 LF	(LU: 7-4-4)	Not required		
		RP-2 (North); LU 3; Arterial Rd	455 LF	(LU: 7-4-4-3)	Not required		
		CPD (East); LU 6; Collector Rd	580 LF	(LU: 7-6-2-1)	Not required		
		AGL Future Office Research Devel. (South); LU 7	580 LF	(LU: 7-7-4)	Not required		
		AGL Future Office Employment Center (West); LU 6; Collector Rd	1,060 LF	(LU: 7-6-2-1)	Not required		
		AGL Future Office Research Devel. (South); LU 7	240 SF (est)	720 SF (est)	720 SF (est)		
		AGL Future Office Employment Center (West); LU 6; Collector Rd	60 SF (est)	240 SF (est)	240 SF (est)		
		AGL Future Office Research Devel. (South); LU 7	240 SF (est)	720 SF (est)	720 SF (est)		
O - Landscaping of Monument Signs	landscape area 3x the combined total area of the sign face and monument base (only one side)	Primary Monument (NW & NE)	40' x 6' HT (estimated)	720 SF (est)	720 SF (est)		
		Secondary Monument (SE & S)	15' x 4' HT (estimated)	240 SF (est)	240 SF (est)		
P - Site Features & Screening	screen trash receptacles, storage areas, service and loading areas, ground mounted utilities, ect	See Plan					



1 LANDSCAPE PLAN - PRELIMINARY DEVELOPMENT

LAND3

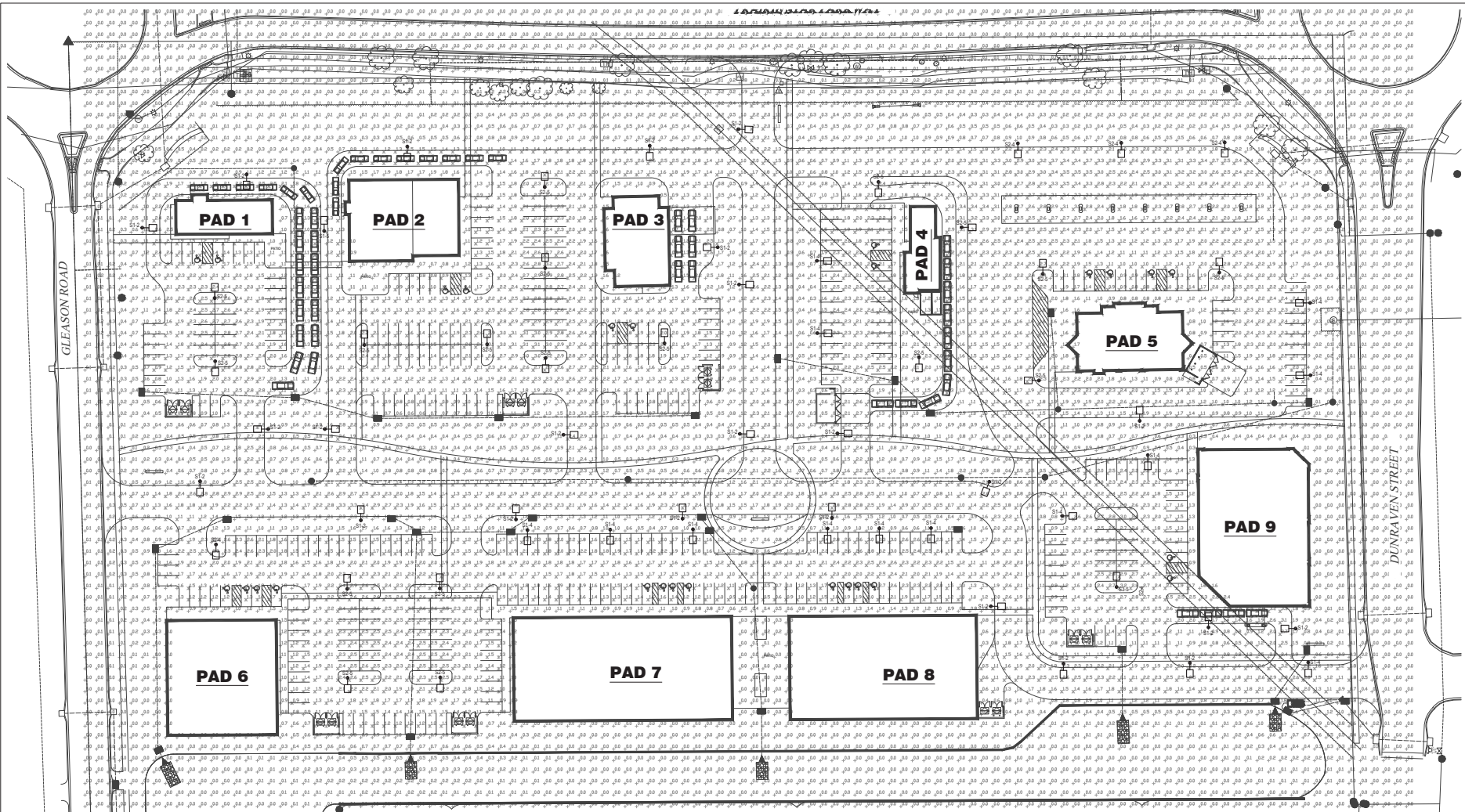
317 SE MAIN
LEES SUMMIT, MO 64063
816-207-6019

SCALE: 1" = 50'

0' 25' 50' 100'

25.03.001 | HWY 7 LLC

PRAIRIE STAR PKWY & K7 DEVELOPMENT | HWY 7 LLC | 06.12.26



STATISTICS (PAVED SURFACES)

Description	Avg	Max	Min	Max/Min	Avg/Min
ENTIRE SITE	1.9	3.7	0.5	7.41	3.81
PRIVATE DRIVE	1.8	3.4	0.5	6.81	3.61
PAD 1	1.8	2.9	0.7	4.11	2.61
PAD 2	1.9	3.2	0.7	4.61	2.71
PAD 3	1.8	3.0	0.5	6.01	3.61
PAD 4	1.9	3.2	0.5	6.41	3.81
PAD 5	2.0	3.7	0.6	6.21	3.31
PAD 6	2.0	2.9	0.8	3.61	2.51
PAD 7	1.6	2.3	0.7	3.31	2.31
PAD 8	1.9	3.3	0.5	6.61	3.81
PAD 9	1.9	3.3	0.5	6.61	3.81

SITE PHOTOMETRIC PLAN
SCALE: 1" = 40'-0"



ENGINEERS INCORPORATED
5720 Renclair
Shawnee, KS 66203
(913)262-1772

This drawing has been prepared by the undersigned and is based on the information and data furnished by the client. It is the responsibility of the client to provide accurate information and data. The undersigned does not warrant the accuracy or completeness of the information and data provided by the client. The undersigned is not responsible for any errors or omissions in this drawing.

PE COA #E-359



BC PROJECT #26490

A SITE PHOTOMETRIC FOR:

**K-7 AND PRAIRIE STAR PARKWAY
SOUTH OF PRAIRIE STAR PARKWAY
BETWEEN GLEASON AND DUNRAVEN
LEAWOOD, KS 66224**

ISSUE DATE:
06/16/2025

REVISION:

SHEET TITLE:
SITE PHOTOMETRIC PLAN

PH1

LIGHT FIXTURE SCHEDULE					
MARK NO.	MANUFACTURER & CATALOG NUMBER	VOLTS	WATTS	LIGHT SOURCE	DESCRIPTION
S1-2	KIM LIGHTING ALT1-2BL-45-4K-2 UNV-ASQ-XXXX ON SSS-K-25-4D-A-1-K2-XXX W 2'-0" CONCRETE BASE	UNV	85	LED-400K 10,000 LUMS	FULL CUT OFF POLE LIGHT WITH FLAT LENS, TYPE 2 DISTRIBUTION, 2P SQUARE STEEL POLE POLE ON 2'-0" CONCRETE BASE. FIXTURE HEIGHT AT 2'-0". FINISH TO BE SELECTED BY OWNER/DEVELOPER
S1-4	KIM LIGHTING ALT1-2BL-45-4K-4 UNV-ASQ-XXXX ON SSS-K-25-4D-A-1-K2-XXX W 2'-0" CONCRETE BASE	UNV	85	LED-400K 10,000 LUMS	SAME AS TYPE S1-2 EXCEPT TYPE 4 DISTRIBUTION.
S1-5	KIM LIGHTING ALT1-2BL-45-4K-5W UNV-ASQ-XXXX ON SSS-K-25-4D-A-1-K2-XXX W 2'-0" CONCRETE BASE	UNV	85	LED-400K 10,000 LUMS	SAME AS TYPE S1-2 EXCEPT TYPE 5 WIDE DISTRIBUTION.
S2-4	KIM LIGHTING ALT1-10L-180-4K-4 UNV-ASQ-XXXX ON SSS-K-25-4D-A-1-K2-XXX W 2'-0" CONCRETE BASE	UNV	160	LED-400K 20,000 LUMS	SAME AS TYPE S1-4 EXCEPT WITH HIGHER LUMEN OUTPUT.
S2-5	KIM LIGHTING ALT1-10L-180-4K-5W UNV-ASQ-XXXX ON SSS-K-25-4D-A-1-K2-XXX W 2'-0" CONCRETE BASE	UNV	160	LED-400K 20,000 LUMS	SAME AS TYPE S1-4 EXCEPT WITH HIGHER LUMEN OUTPUT.
S3-5	KIM LIGHTING ALT1-10L-200-4K-5W UNV-ASQ-XXXX ON SSS-K-25-4D-A-1-K2-XXX W 2'-0" CONCRETE BASE	UNV	200	LED-400K 25,000 LUMS	SAME AS TYPE S1-5 EXCEPT WITH HIGHER LUMEN OUTPUT.

KIM LIGHTING

DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATALOG #:

ALT2
ARCHITECTURAL AREA/SITE

Altitude

FEATURES

- Elegant form factor
- High performance optics up to 35,387 delivered lumens
- Diffusion lens for low glare comfort
- NX lighting controls options
- IP66 Rated

CONTROL TECHNOLOGY

LIGHT GRID

SPECIFICATIONS

CONSTRUCTION

- One piece die-cast, low copper (0.6% Cu) Aluminum Alloy 360 with vents and 3007 minimum wall thickness.
- Finish: fade and abrasion resistant, electrocoat applied, thermally cured, triglycid isocyanurate (TIGC) polyester powder-coat.
- Optional lens in impact resistant 18" tempered glass with anti-reflective coating.
- Optional lens has a one-piece extruded vulcanized silicone gasket.
- Optional lens frame one piece die-cast low copper (0.6% Cu) Aluminum Alloy 360 and 3007 minimum wall thickness.
- Lens frame has a one-piece O-ring extruded vulcanized silicone gasket.
- Mid-frame one piece die-cast, low copper (0.6% Cu) Aluminum Alloy 360 and 3007 minimum wall thickness.
- Mid-frame secures to Lens Frame and housing by stainless steel recessed Allen-head screws.
- Stainless gaskets throughout.
- All external fasteners are stainless steel.

OPTICS

- LEDs mount to a metal printed circuit board assembly (MCPCB).
- Optical lenses are clear injection molded PMMA acrylic.
- Optional Backlight Control on each LED module to completely control unwanted backlight.
- Optional fixture frame optical surfaces will not exceed 0.001" out of the standard finish. Finish will be greater than or equal to the recessed lumens of the optional matte black optical surface finish.

INSTALLATION

Fasteners shall be grounded in accordance with National, State and/or local electrical codes. Failure to do so may result in serious personal injury.

ELECTRICAL

- Universal voltage, 120 through 277V with a 50% tolerance. Direct is Underwriters Laboratories listed.
- High voltage configurations, 347/600V Direct has a 0.0V ohmic impedance for multi-level distribution (UL954, UL954A, UL954B, UL954C, UL954D, UL954E, UL954F, UL954G, UL954H, UL954I, UL954J, UL954K, UL954L, UL954M, UL954N, UL954O, UL954P, UL954Q, UL954R, UL954S, UL954T, UL954U, UL954V, UL954W, UL954X, UL954Y, UL954Z).

ELECTRICAL CONTINUED

- "Thermal Shield" secondary side, heatmaster provides protection for the sustainable life of LED module and electronic components.
- Chassis shall have greater than a 0.9 power factor, less than 20% harmonic distortion, and be suitable for operation in -40°C to 40°C ambient environments.
- Luminaire are capable of operating at 100% brightness in a 40°C environment. Luminaire shall not exceed 100% thermal protection that will dim the luminaire upon detection of temperatures in excess of 80°C ambient environments.
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- Modular wiring harness in the service area, provides user access to the dimming circuitry.
- Chassis factory programmed dimming profile, codes: Surge protection: 2000A @ 1 sec.
- SP for 120, 277, 347 Line volts.
- SP for 208, 240, 480 Line volts.
- Wiring No. BAWM (uses 100°C, wet rating).

(Specifications continued on page 5)

DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATALOG #:

ALT1
ARCHITECTURAL AREA/SITE

Altitude

FEATURES

- Elegant form factor
- High performance optics up to 15,500 delivered lumens
- Diffusion lens for low glare comfort
- NX lighting controls options
- IP66 Rated

CONTROL TECHNOLOGY

LIGHT GRID

SPECIFICATIONS

CONSTRUCTION

- One piece die-cast, low copper (0.6% Cu) Aluminum Alloy 360 with vents and 3007 minimum wall thickness.
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- Optional lens has a one-piece extruded vulcanized silicone gasket.
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- Lens frame has a one-piece O-ring extruded vulcanized silicone gasket.
- Mid-frame one piece die-cast, low copper (0.6% Cu) Aluminum Alloy 360 and 3007 minimum wall thickness.
- Mid-frame secures to Lens Frame and housing by stainless steel recessed Allen-head screws.
- Stainless gaskets throughout.
- All external fasteners are stainless steel.

OPTICS

- LEDs mount to a metal printed circuit board assembly (MCPCB).
- Optical lenses are clear injection molded PMMA acrylic.
- Optional Backlight Control on each LED module to completely control unwanted backlight.
- Optional fixture frame optical surfaces will not exceed 0.001" out of the standard finish. Finish will be greater than or equal to the recessed lumens of the optional matte black optical surface finish.

INSTALLATION

Fasteners must be grounded in accordance with National, State and/or local electrical codes. Failure to do so may result in serious personal injury.

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(Specifications continued on page 6)

KIM LIGHTING

DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATALOG #:

SSS-K Series Poles
SQUARE STRAIGHT STEEL

APPLICATIONS

- Lighting installations for side and top mounting of luminaires with effective projected area (EPA) not exceeding maximum allowable loading of the specified pole in its installed geographic location

CONSTRUCTION

- SHAFT:** One piece straight steel with square cross section, flat sides and minimum 0.238" radius on all corners. Minimum yield of 40,000 psi (ASTM A500). Grade 80. Longitudinal weld seams to appear flush with shaft. Self-drilled base plate with axial lock. Circle sizes verified flush 2" shaft (having minimum yield of 36,000 psi (ASTM A500))
- BASE COVER:** Two piece square aluminum base cover included standard
- POLE CAP:** Pole shaft supplied with removable cover when applicable. Top and post-top configurations also available
- HAND HOLE:** Rectangular 3rd steel hand hole frame (2.38" x 4.38" opening). Mounting provisions for grounding lug located behind gasketed cover
- ANCHOR BOLTS:** Four galvanized anchor bolts provided per pole with minimum yield of 55,000 psi (ASTM 1554). Galvanized hardware with two nipples and nuts per foot for leveling.

FINISH

- Durable thermoset polyester powder coat paint finish with minimal 1.0 mil thickness
- Available hardware with two nipples and nuts per foot for leveling.
- Powder paint prime applied over "white metal" steel substrate cleaned with mechanical shot blast method.
- Decorative finish coat available in multiple standard colors. Custom colors available; RAL number preferable.

ORDERING INFORMATION

Reference page 2 for available configurations

ORDERING EXAMPLE: **SSS-K - 20 - 40 - A/B/C - 2L - K2 - DBS - VM2**

SSS-K - square height
HEIGHT - pole height
SHAFT - shaft size
THICKNESS - shaft thickness
MOUNTINGS - mounting type
DRILL - drill type
FINISH - finish type
OPTIONS - options

MOUNTING ORIENTATION

ACCESSORIES - Order Separately

CONVERTER - for 120V to 277V conversion
DMT - for 120V to 277V conversion
DMT - for 120V to 277V conversion

Current

DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATALOG #:

POLE FOUNDATION DETAIL
SCALE: NONE

Current

KIM LIGHTING

DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATALOG #:

ALT2
ARCHITECTURAL AREA/SITE

Altitude

FEATURES

- Elegant form factor
- High performance optics up to 35,387 delivered lumens
- Diffusion lens for low glare comfort
- NX lighting controls options
- IP66 Rated

CONTROL TECHNOLOGY

LIGHT GRID

SPECIFICATIONS

CONSTRUCTION

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OPTICS

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INSTALLATION

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- Wiring No. BAWM (uses 100°C, wet rating).

(Specifications continued on page 6)

DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATALOG #:

ALT1
ARCHITECTURAL AREA/SITE

Altitude

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- Wiring No. BAWM (uses 100°C, wet rating).

(Specifications continued on page 6)

BC ENGINEERS INCORPORATED
3720 FRENCHMAY
SHAWNEE, KS 66203
(913)262-1772

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PROJECT #26490

6/11/2028

DR. HEINRICH HUBER
26301

BC PROJECT #26490

A SITE PHOTOMETRIC FOR:

K-7 AND PRAIRIE STAR PARKWAY
SOUTH OF PRAIRIE STAR PARKWAY
BETWEEN GLEASON AND DUNRAVEN
LEAWOOD, KS 66224

ISSUE DATE: 06/11/2028

REVISION:

SHEET TITLE: SITE LIGHTING SCHEDULE/DETAILS

PH2

From: Connie Hardy <conniehardy207@gmail.com>

Sent: Thursday, June 18, 2026 11:01 AM

To: CD Planning <Planning@lenexa.com>

Subject: Proposed residential development

I am writing regarding the planning commission meeting July 7th to discuss rezoning of AG land on Prairie Star Parkway between Dunraven and Gleason. I am not opposed to this development. However, plans need to include revisions to the roundabout at Dunraven and Prairie Star. When St.James Academy is in session there is a long back up for blocks already especially at dismissal time. We live in the area and it's a nightmare let alone more housing to add to the congestion. Please address this issue for those of us residents that live in the area.

Thank you

Sent from Gmail Mobile

CALL TO ORDER

Chairman Poss called the regular meeting of the Lenexa Planning Commission to order at 7:00 p.m. on Monday, June 1, 2026. The meeting was held in the Community Forum at Lenexa City Hall at 17101 W. 87th Street Parkway, Lenexa, Kansas.

ROLL CALL

COMMISSIONERS PRESENT

Chairman Chris Poss
Vice-Chairman Mike Burson
Commissioner Sunny Dharod
Commissioner Ben Harber
Commissioner Don Horine
Commissioner Jermaine Jamison
Commissioner Curt Katterhenry
Commissioner Cara Wagner
Commissioner David Woolf

COMMISSIONERS ABSENT

None

STAFF PRESENT

Scott McCullough, Director of Community Development
Stephanie Sullivan, Planning Manager
Tim Collins, Engineering/Construction Services Administrator
Andrew Diekemper, Assistant Fire Chief
Steven ShROUT, Assistant City Attorney
Dave Dalecky, Planner III
Jessica Lemanski, Planner II
James Molloy, Planner II
Noah Vaughan, Planning Specialist II
Oliver Zigmund, Planning Intern
Gloria Lambert, Senior Administrative Assistant

APPROVAL OF MINUTES

The minutes of the May 4, 2026 meeting were presented for approval. Hearing no changes or corrections to the minutes of the regular meeting, Chairman Poss entertained a motion to **APPROVE** the minutes as written. Moved by Commissioner Jamison, seconded by Commissioner Harber, and **APPROVED** by a majority voice vote.

CONSENT AGENDA

1. **Highlands at Clear Creek — Consideration of a final plat for a single-family residential subdivision on property located near the southeast corner of 86th Terrace and Clare Road within the RP-1, Planned Residential (Low Density) District. PT26-11F**
2. **Canyon Ridge — Consideration of a final plan for the multifamily residential component of a mixed-use development located at the northwest corner of K-10 Highway & Canyon Creek Boulevard within the PUD (Planned Unit Development) District. PL26-03F**
3. **USD 232 Cedar Trails Exploration Center Building Addition — Consideration of a revised final plan for an addition to the early childhood center and high school career center on property located at 8201 Mize Boulevard within the R-1, Single-Family Residential District. PL26-03FR**
4. **Arrived Outdoors — Consideration of a final plan for a building addition and site improvements for a club or lodge use on property located at 14144 Santa Fe Trail Drive within the BP-2, Planned Manufacturing District. PL26-04F**

Chairman Poss entertained a motion to **APPROVE** the Consent Agenda. Moved by Commissioner Horine, seconded by Commissioner Wagner, and carried by a unanimous voice vote.

REGULAR AGENDA

5. **Lenexa Fire Station 6 — Consideration of a preliminary plan for public safety and office uses on property located at 12350 West 87th Street Parkway within the CP-O, Planned General Office District. PL26-05P**

APPLICANT PRESENTATION

Ellen Foster of Finkle Williams Architecture presented the proposed Lenexa Fire Station No. 6 and IT Headquarters on behalf of the City of Lenexa. She explained that the project was designed around four guiding principles: occupant wellbeing, resiliency, civic pride, and efficient fire station operations, with emergency response needs serving as the primary driver of the site layout. Ms. Foster stated that the project will redevelop the former City Hall and Police Department site at 87th Street and Monrovia. Following a master planning study, the City determined that demolishing the existing building and constructing a new facility was the preferred approach. The site plan was influenced by several factors, including emergency vehicle access, existing utilities, the operational cell tower, preservation of landscape buffers adjacent to residential areas, and maintaining flexibility for future city development. Ms. Foster said that the proposed fire station and administrative headquarters will be located on the eastern portion of the site. Emergency vehicles will exit from Monrovia only, while staff and visitor parking will access from Both Monrovia and Long Streets. The design separates emergency operations from public traffic and preserves a portion of the site for potential future city facilities. Ms. Foster noted that the building includes firefighter living and operational spaces on the north side, Fire Administration and IT offices on the south side, and a shared public lobby between them. She also presented preliminary architectural renderings featuring brick, limestone, and other durable civic materials intended to reflect the building's role as a prominent public facility. Ms. Foster concluded by outlining the project schedule, stating that the design team is currently completing design development. Following approval of the Preliminary Development Plan, the project will return with a Final Development Plan, with bidding anticipated in October and construction expected to begin in November.

STAFF PRESENTATION

Dave Dalecky presented the Staff Report for the preliminary development plan for the former City Hall and Police Department site at 87th Street and Monrovia. He explained that the project involves demolishing the existing buildings while retaining the cell tower, with Fire Station No. 6 serving as the first phase of redevelopment. Mr. Dalecky stated that the property is zoned CP-O, Planned General Office District and that the proposed fire station, administrative offices, and future office and warehouse uses are permitted within the district. He noted that the site is designated Institutional on the Future Land Use Map, making the proposed office and Public safety services uses consistent with the City's Comprehensive Plan. Mr. Dalecky explained that the overall master plan includes three buildings, approximately 61,000 square feet of development, and 246 parking spaces. He also noted that a small ownership issue involving a portion of land at the northeast corner of the site will be addressed during the future platting process. He also described the proposed fire station as a one-story building with a strong civic character, using stone, brick, and glass materials that are carried consistently around the structure. He highlighted efforts to preserve existing trees, particularly along the northern property line adjacent to residential development. Mr. Dalecky concluded by stating that Staff found the proposal consistent with applicable zoning and land use requirements and recommended approval of the preliminary development plan.

COMMISSIONER DISCUSSION

Commissioner Horine asked about the current location of the Fire Department's administrative offices. Ellen Foster explained that they are currently housed at Fire Station No. 1 and are out of space. She noted that the existing space could be renovated and repurposed once staff relocate to the new facility.

Commissioner Horine also asked whether the future office building shown on the master plan would be used for City purposes. Ms. Foster stated that the current intent is for the building to accommodate City functions or partnerships while remaining on City-owned property. Commissioner Horine complimented the building design and site circulation plan but expressed concern about the removal of the existing fountain. Ms. Foster explained that maintenance requirements and direction from the building committee and City staff led to the decision to remove it, although the Arts Commission is considering replacing it with another feature or public art element. Commissioner Horine stated that he was disappointed to see the fountain removed, noting that it has long served as a recognizable entrance feature for the community.

Commissioner Dharod asked what aspects of the project were being approved with the current application and whether future phases would return to the Planning Commission for additional review. Ellen Foster explained that the Commission was being asked to approve the overall master plan for the site, including the general locations of the future buildings, as well as the architecture for the proposed fire station and administrative headquarters. She noted that when the future buildings are developed, they would likely return to the Planning Commission for review of their architectural design and any associated site improvements. Ms. Foster also confirmed that a plat for the entire property will be submitted in the future because the site is currently unplatted. She stated that the plat would come before the Planning Commission before the future buildings are developed.

Commissioner Wagner stated that she liked the overall site layout and believed the consideration given to traffic circulation and emergency vehicle access was well planned. She noted that the design appeared to support efficient response times, particularly for access to the 87th Street and Quivira Road area. She also expressed agreement with Commissioner Horine's comments regarding the removal of the existing fountain.

Commissioner Burson commented that redevelopment of the former City Hall site has been anticipated for some time and stated that he believed the proposal makes thoughtful use of the property. He

expressed support for the overall layout and architectural design of the project. Commissioner Burson then asked for clarification regarding the future building identified as potential office or warehouse space. He noted that the term "warehouse" raised some concerns given the location and asked what types of warehouse uses would be permitted by right within the CP-O Zoning District. Mr. Dalecky responded that warehousing would not be permitted as a principal use within the CP-O Zoning District. He explained that storage or warehouse-type space could be allowed only as an accessory use to an office or similar permitted use. As an example, he noted that the City or Fire Department could use such space to store equipment, supplies, or materials purchased in bulk for operational purposes. He clarified that storage is permitted in the district only when it is ancillary to a primary permitted use. Mr. Dalecky explained that exterior storage could be allowed as an accessory use if it is fully screened from view. He stated that, in this case, the expectation would likely be that any storage area would be enclosed by walls or similar screening elements. He noted that approval would depend on factors such as aesthetics, character, and the amount of site area devoted to the storage use but clarified that exterior storage could be permitted when it remains subordinate and incidental to a principal permitted use. Commissioner Burson stated that he wanted the discussion regarding warehouse and storage uses to be part of the public record so that any future development on the site would be reviewed with those limitations and expectations in mind. He expressed confidence that City staff would ensure compliance with the applicable requirements but reiterated that the term "warehouse" raised some concern given the location.

Commissioner Jamison commented on the significance of the existing fountain, noting that it has become a recognizable symbol for the community. He asked whether residents would have an opportunity to provide input on any future feature, artwork, or structure that may be installed in place of the fountain. Ellen Foster responded that the Parks and Recreation Department and the City's arts group are currently evaluating potential replacement features and determining what the public engagement process will look like. She stated that she would relay the Commission's feedback and discuss with the group the possibility of providing residents with an opportunity to participate in the decision-making process.

Commissioner Katterhenry expressed support for the proposed site layout, building design, and access configuration. He noted that he had intended to ask about the future of the existing fountain but that the topic had already been addressed. While leaving the decision to City staff, he stated that if the fountain is removed, he believes an appropriate replacement feature should be installed in its place.

Chairman Poss asked for clarification regarding the rubberized exercise turf shown on the site plan. Ellen Foster explained that this is an outdoor exercise space for the firefighters and it would function similarly to the outdoor turf area at the Justice Center. Chairman Poss inquired about the future office pad sites and whether the area would be fully graded as part of the project. Ms. Foster confirmed that the site would be completed in a finished condition and would not contain disturbed or ungraded areas. Chairman Poss also asked about preservation of existing vegetation. Ms. Foster explained that most perimeter trees would remain, although several mature ash trees would likely be removed due to ongoing treatment needs and poor long-term viability. Limited tree removal may also occur for utility installation. Chairman Poss expressed support for removing the ash trees and commented that the project represented a thoughtful reuse of the property. He noted that planning for the site had been ongoing for several years, acknowledged the public engagement process, and stated that a fire station at this location made sense from a public safety standpoint. He also complimented the building's architectural design.

MOTION

Chairman Poss entertained a motion to recommend **APPROVAL** of the preliminary plan for PL26-05P – **Lenexa Fire Station 6** located at 12300 87th Street Prairie Star Parkway, for Office and Public safety services uses.

Moved by Commissioner Harber, seconded by Commissioner Burson, and carried by a unanimous voice vote.

6. Proposed Amendment to the Unified Development Code — Regulations related to medical or dental clinic and personal services uses in the NP-O, Planned Neighborhood Office and CP-O, Planned General Office Districts. UDC26-02

STAFF PRESENTATION

City staff member Noah Vaughan presented a proposed amendment to the Unified Development Code (UDC) regarding medical or dental clinics and personal service uses. He explained that these uses currently require a special use permit (SUP) in the Neighborhood Planned Office (NP-O) District, and personal service uses also require an SUP in the Commercial Planned Office (CP-O) District. Mr. Vaughan noted that medical, dental, and personal service uses account for approximately 27% of all SUP requests over the past six years, making them the most frequently requested SUPs. He stated that none of the requests had been denied and that the City had received no complaints regarding approved uses. Staff reviewed the UDC sections that would be affected and explained that the proposed amendment would change these uses from special uses to permitted uses by right within the applicable zoning districts. No supplementary use regulations would be modified. Mr. Vaughan stated that the amendment would streamline the development process by eliminating the need for applicants to obtain SUP approval, removing renewal requirements for existing SUP holders, and reducing review time and administrative burdens for Staff, the Planning Commission, and the City Council. Based on the frequency of approvals and the lack of documented concerns, Staff recommended approval of the text amendment. If recommended approval, it will go before the Governing Body on June 16th.

PUBLIC HEARING

Chairman Poss **OPENED** the Public Hearing and asked if anyone wished to speak on this item. No one from the public spoke.

Chairman Poss entertained a motion to **CLOSE** the Public Hearing. Moved by Commissioner Wagner, seconded by Commissioner Burson, and carried by a unanimous voice vote.

COMMISSIONER DISCUSSION

Commissioner Katterhenry stated that the Planning Commission has reviewed many similar requests over the years and that they have generally been appropriate for their locations. He noted that parking has been the primary concern raised during past reviews; however, those issues have typically been resolved, and the City has not received complaints regarding approved uses. Commissioner Katterhenry expressed support for the proposed amendment, stating that it made sense based on the City's experience with these uses.

Commissioner Burson noted that during his more than ten years on the Planning Commission, these types of special use permits have never been denied and have not generated complaints. He stated that the process creates a significant administrative burden for both applicants and Staff, often causing confusion for applicants. While expressing support for the amendment, he asked whether Staff would continue to review site-specific issues, such as parking, through the development review process. Noah Vaughan replied yes, as part of the general Certificate of Occupancy review that is conducted for any business, Staff would calculate the applicable parking requirements. If any issues arise, Staff will address them in the same manner as any other business. Staff would work with the business to identify potential solutions to meet the parking requirements. If the requirements cannot be met, the business would need

to find an alternative location. Commissioner Burson stated that, based on that answer, he supported the proposal. He noted that it would reduce the administrative burden on City Staff and allow them to devote their time to higher-priority matters.

Commissioner Horine asked when the proposed amendment would become effective if approved by the City Council on June 16. Noah Vaughan explained that, while the amendment would be approved by the City Council, Staff would still need to update the City's Code. However, he noted that applications submitted after the amendment's approval would not be required to obtain a Special Use Permit (SUP) for the affected uses. Steven Shrout clarified that, if approved on June 16, 2026, the Ordinance would become effective upon publication, approximately seven days later, around June 23rd. Commissioner Horine asked whether applicants submitting after that date would be aware of the change through the updated code. Mr. Shrout responded that the online code would be updated as quickly as possible. He noted that larger code amendments sometimes require a delayed effective date to allow time for codification, but because this amendment is minor, the update process should be completed promptly once the ordinance becomes effective.

Commissioner Harber stated that the analysis was well done, particularly the review of previous complaints. He agreed that parking has historically been the primary issue associated with these uses, although discussions have occasionally included trash enclosure concerns. He expressed his full support for the proposed amendment, stating that it is a sensible change.

Commissioner Dharod expressed agreement with the other commissioners, particularly regarding the efficiency of the proposed amendment. He asked whether there would be any opportunity for members of the public to raise questions or concerns about a potential business located in Lenexa. Noah Vaughan responded that if a use is permitted by right under the zoning regulations, the review would be conducted administratively by City Staff. As a result, there would not be a public approval process through which residents could formally oppose the establishment of that use. Scott McCullough stated that Staff occasionally receive complaints from neighboring tenants regarding issues such as odors, noise, or lighting associated with new businesses. He noted that the uses under consideration are low-impact and that Staff has established processes to address concerns and require mitigation when necessary.

Chairman Poss stated that he agreed with the comments made by the other commissioners and expressed strong support for the proposed amendment. He noted that improving efficiency and creating a more business-friendly environment benefits the community and stated that the proposed changes are reasonable, particularly given that the affected businesses provide services commonly used by residents.

MOTION

Chairman Poss entertained a motion to recommend **APPROVAL** of the Text Amendment to Sections 4-1-B-11, 4-1-B-12, and 4-1-B-22 of the Unified Development Code relating to regulations for Medical or dental clinics and Personal services as drafted within the Staff Report.

Moved by Commissioner Jamison, seconded by Commissioner Dharod, and carried by a unanimous voice vote.

STAFF REPORT

Stephanie Sullivan reported two recent promotions within the Planning Division. Dave Dalecky was promoted from Planner II to Planner III, and Noah Vaughan was promoted from Planning Specialist I to Planning Specialist II. She congratulated both employees on their accomplishments.

Ms. Sullivan also introduced the department's summer Planning Intern, Oliver Zigmund. Mr. Zigmund addressed the Commissioners and stated that he is a junior at the University of Kansas pursuing undergraduate degrees in geography and history and plans to earn a master's degree in urban planning. He noted that he is a Lenexa resident and previously worked for the City's Parks and Recreation Department. He shared that he moved to Lenexa from Juneau, Alaska, in 2024 and has enjoyed the community, particularly its trail system.

ADJOURNMENT

Chairman Poss ended the regular meeting of the Lenexa Planning Commission at 7:46 p.m. on Monday, June 1, 2026.

DRAFT