



PLANNING COMMISSION AGENDA

JANUARY 9, 2023 at 7:00 PM

Community Forum at City Hall

17101 W. 87th Street Parkway

Lenexa, KS 66219

CALL TO ORDER

ROLL CALL

APPROVE MINUTES

CONSENT AGENDA

All matters listed within the consent agenda have been distributed to each member of the Planning Commission for review, are considered to be routine, and will be enacted by one motion with no separate discussion. If a member of the Planning Commission or audience desires separate discussion on an item, that item may be removed from the consent agenda and placed on the regular agenda.

- 1. City Center Lenexa Area A (Restaurant Row) - Consideration of a final plan for property located near the southeast corner of W. 87th Street Parkway & Scarborough Street within the CC, Planned City Center, Zoning District. PL23-02F**
Consideration of a final plan for the development of two restaurant/retail buildings (A1 and A4) on a 4.51 acre site in Lenexa City Center. Building A1 is 9,191 square feet and Building A4 is 8,534 square feet.
- 2. Redevelopment Project Plan 1J for the City Center TIF District - Consideration of a resolution for property located near the southeast corner of W. 87th Street Parkway & Scarborough Street within the CC, Planned City Center, Zoning District.**
Consideration for adopting a resolution and making findings that the proposed Project Plan 1J is consistent with the City's Comprehensive Plan.
- 3. Southlake Twenty-First Plat (DataBank Expansion) - Consideration of a final plat for property located at 11200 Lakeview Avenue within the BP-2, Planned Manufacturing District. PT23-04F**
Consideration of a final plat to replat two existing lots and one tract to facilitate the expansion of a data center and equipment yard in the BP-2, Planned Manufacturing District.
- 4. Lenexa Logistics Centre, 8th Plat - Consideration of a final plat for property located at the northwest corner of 113th & Renner Boulevard within the BP-2, Planned Manufacturing District. PT23-01F**
Consideration of a final plat for a 5.96 acre industrial development.
- 5. Lenexa Logistics Centre North, Sixth Plat - Consideration of a final plat for property located near the northwest corner of College Boulevard & Mill Creek Road within the BP-2, Planned Manufacturing District. PT23-03F**
Consideration of a final plat for a 6.04 acre industrial development.
- 6. Prairie Star Medical Office Building - Consideration of a final plan for property located at 9471 Meadow View Drive within the CP-1, Planned Neighborhood Commercial District. PL23-03F**
Consideration of a final plan for the development of an 11,641 square foot medical office building on a 1.35 acre site.

7. Watercrest Landing Amenity Center - Consideration of a final plan for a subdivision amenity on property located at 9096 Monticello Road within the RP-1, Planned Residential (Low-Density) District. PL23-01F

Consideration of a final plan for the development of a private residential subdivision amenity center on a 2.28 acre site in the Watercrest Landing subdivision.

REGULAR AGENDA

8. Belmont Estates - Consideration of a preliminary plat and final plat for property located at approximately 8110 Belmont Drive.

Consideration of a preliminary plat to replat six existing lots into two lots for the development of two single-family residences and a cul-de-sac at the north end of Belmont Drive. PT23-01PR

Consideration of a final plat to replat six existing lots into two lots for the development of two single-family residences and a cul-de-sac at the north end of Belmont Drive. PT23-02F

9. Cedar Canyon West - Remanded to the Planning Commission by the City Council. Consideration for rezoning and a concept plan for property located at the northeast corner of Canyon Creek Boulevard & K-10 Highway.

Consideration for rezoning property from the AG, Agricultural, and CP-2, Planned Community Commercial, Zoning Districts to the CP-2, Planned Community Commercial, CP-O, Planned General Office, BP-2, Planned Manufacturing, RP-2, Planned Residential (Intermediate Density), and RP-4, Planned Residential (High-Density) Zoning Districts. RZ22-09

Consideration of a concept plan for the development of a mixed-use site consisting of commercial, office, business park, and residential uses on approximately 112 acres. PL22-04CP

10. Stone Ridge North - Consideration of rezoning and a preliminary plat for property located at the northeast corner of W. 83rd Street & Cedar Niles Road.

Consideration for rezoning property from the NP-O, Planned Neighborhood Office, RP-1, Planned Residential (Low Density), CP-1, Planned Neighborhood Commercial, and AG, Agricultural, Zoning Districts to the RP-2, Planned Residential (Intermediate Density) and RP-1, Planned Residential (Low Density), Zoning Districts. RZ22-05

Consideration of a preliminary plat for the development of 381 single-family and 53 two-family residential lots on 168.35 acres. PT22-01P

STAFF REPORTS

ADJOURN

APPENDIX

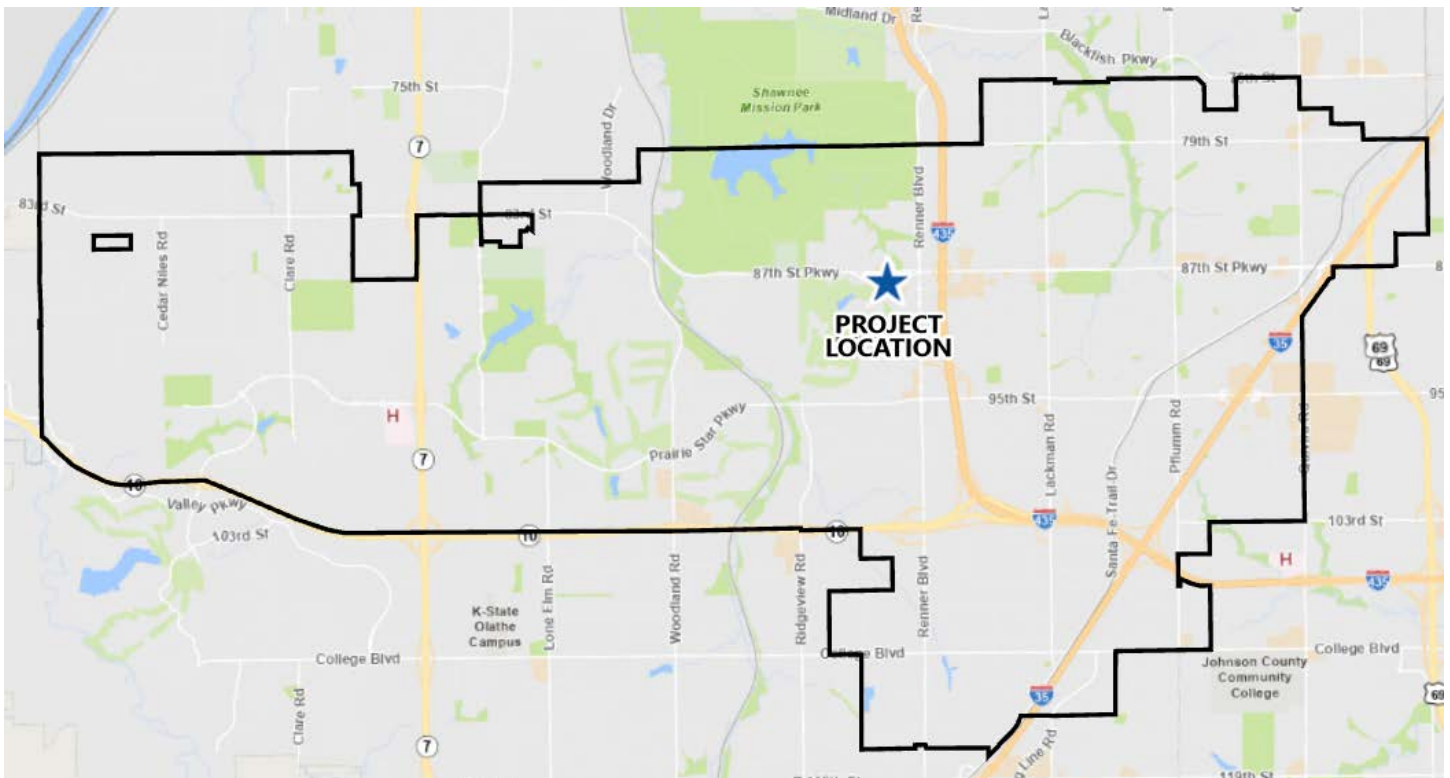
11. December 5, 2022 Planning Commission Draft Minutes

If you have any questions about this agenda, please contact Stephanie Kisler, Planning and Development Administrator, at skisler@lenexa.com.

*If you need any accommodations for the meeting, please contact the City ADA Coordinator at 913-477-7550 at least 48 hours prior to the meeting.
Kansas Relay Service: 800-766-3777*

AREA A RESTAURANT ROW – A1 & A4

Project #:	PL23-02F	Location:	Southwest corner of 87 th Street Parkway and Renner Boulevard
Applicant:	Henry Klover	Project Type:	Final Plan
Staff Planner:	Christa McGaha, AICP	Proposed Use:	Mixed-use development (to include restaurant, office, and retail uses)



PROJECT SUMMARY

The applicant proposes to construct a mixed-use development at the southwest corner of 87th Street Parkway and Renner Boulevard. This is the first phase of the five building Restaurant Row development in Area A of City Center. This application is for two buildings, A1 and A4, located on the northwest and southeast corners of the site and first phase of the parking area. The development will include retail and restaurant tenants. Building A1 is 9,191 square feet and A4 is 8,534 square feet. The development includes construction of a 192-stall surface parking area. A two-level parking structure is required to be constructed as part of the next phase of the development. The applicant requests approval of a final plan for buildings A1 and A4 of Restaurant Row. The proposed final plan is consistent with the approved preliminary plan (PL22-04PR), which was approved by the Governing Body on December 6, 2022. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The final plan is for the block in City Center bordered by 87th Street Parkway on the north, Renner Boulevard on the east, Scarborough Street on the west, and City Center Drive on the south. The block is part of City Center and is referred to as Area A, and more specifically as Restaurant Row. This block has been the subject of previous plans from the inception of City Center. The various plans have been modified over the past several years with the consistent expectation of the block containing multiple restaurant tenants. Most recently, a revised preliminary plan (PL22-04PR) was approved by the Governing Body on December 6, 2022 with the condition that Buildings A1 and A4 are permitted to be constructed and occupied as part of Phase 1 with surface parking. Buildings A2, A3, and A5 shall not receive final plan approval except in conjunction with final planning and construction of the parking structure.

The street pattern for the block was established early in the development of City Center creating the triangular block shape with the long sweeping curve of Scarborough Street and City Center Drive.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
4.512	A1: 9,191 A4: 8,534	CC	City Center Core



Exhibit 1: Aerial Image of Subject Site

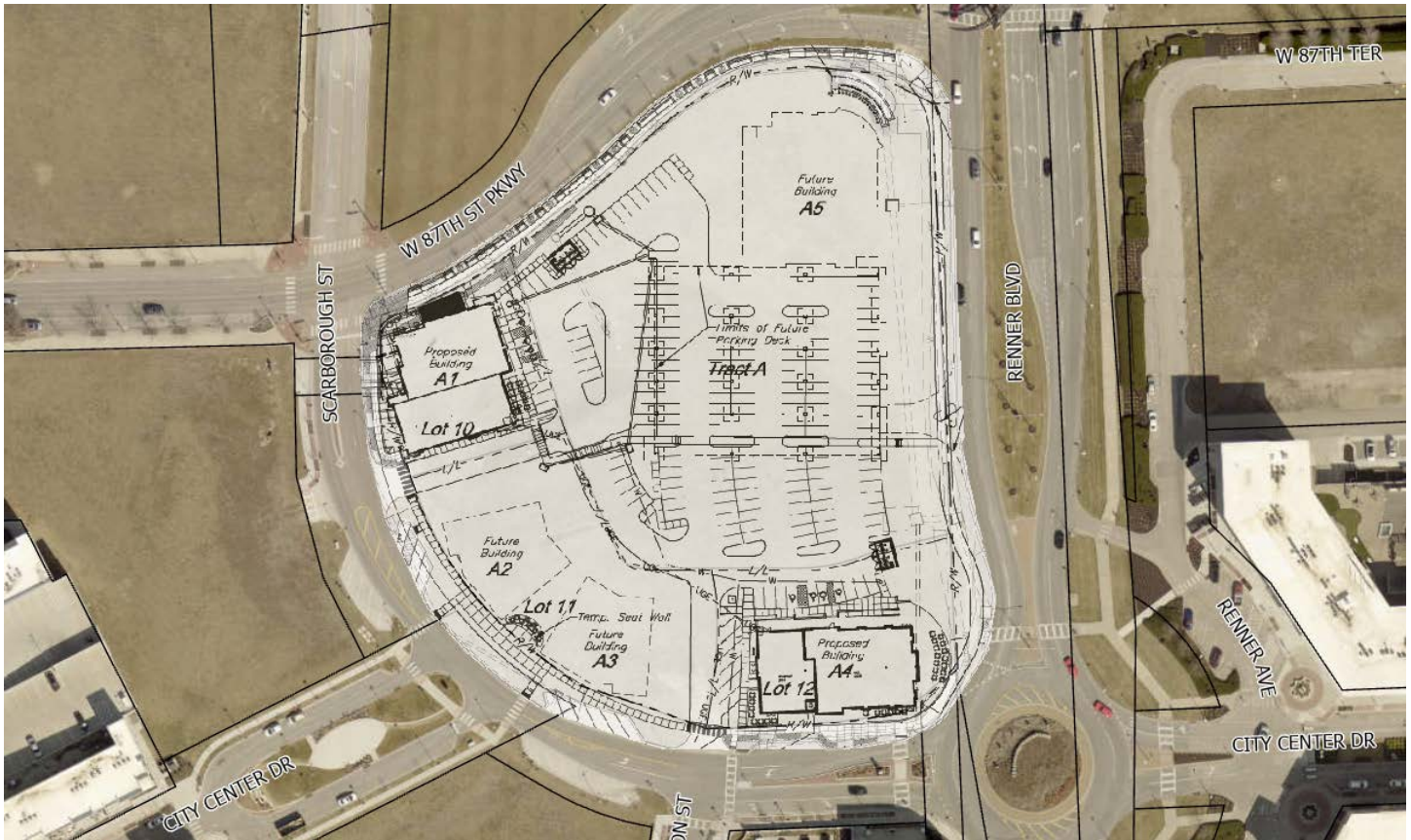
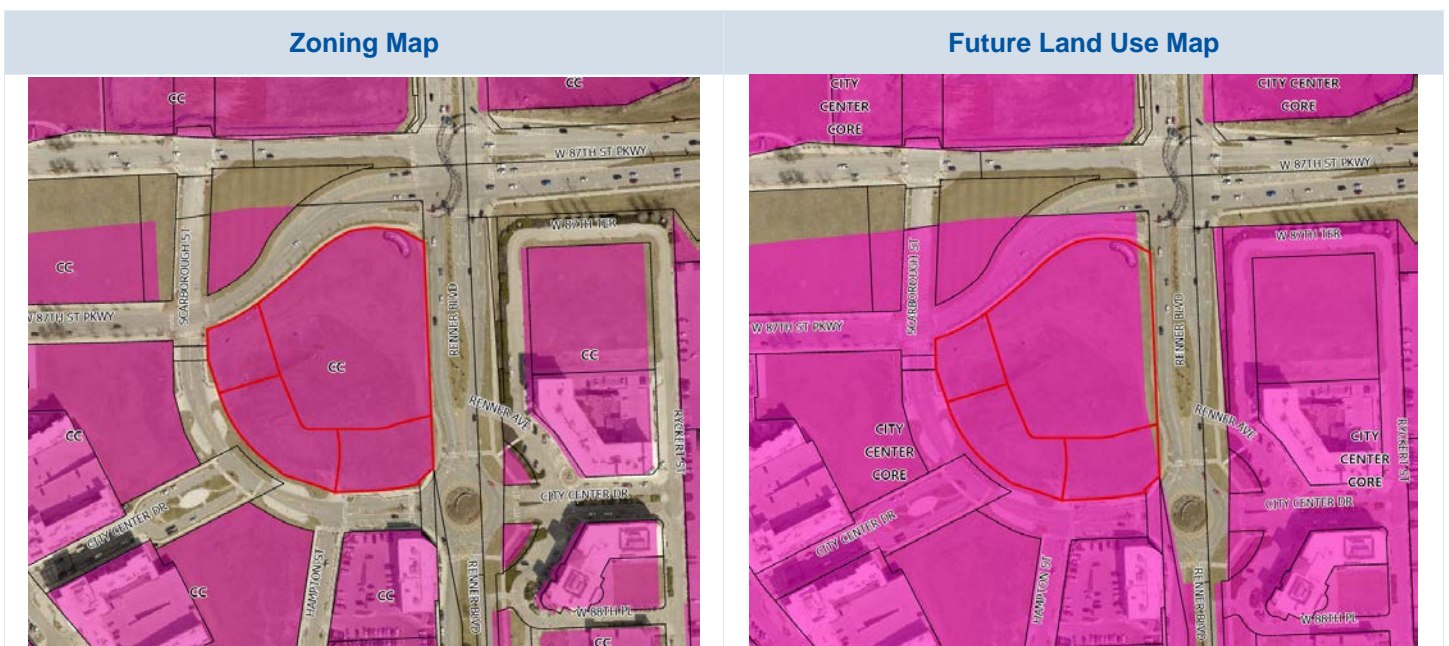


Exhibit 2: Georeferenced Site Plan

LAND USE REVIEW



The proposed first phase will have two buildings (A1 and A4) totaling 17,903 square feet and 229 parking stalls. The first phase will include 192 spaces in the surface parking lot, 24 street parking spaces along Scarborough Street, and 13 spaces on City Center Drive. The proposed land uses are restaurant and retail.

The proposed uses are compatible in context of the existing land uses and the overall plan for the City Center area. Nearby land uses include mixed-uses such as the Lofts at City Center to the east and Martin City Brewing Company to the south of the subject site.

FINAL PLAN REVIEW

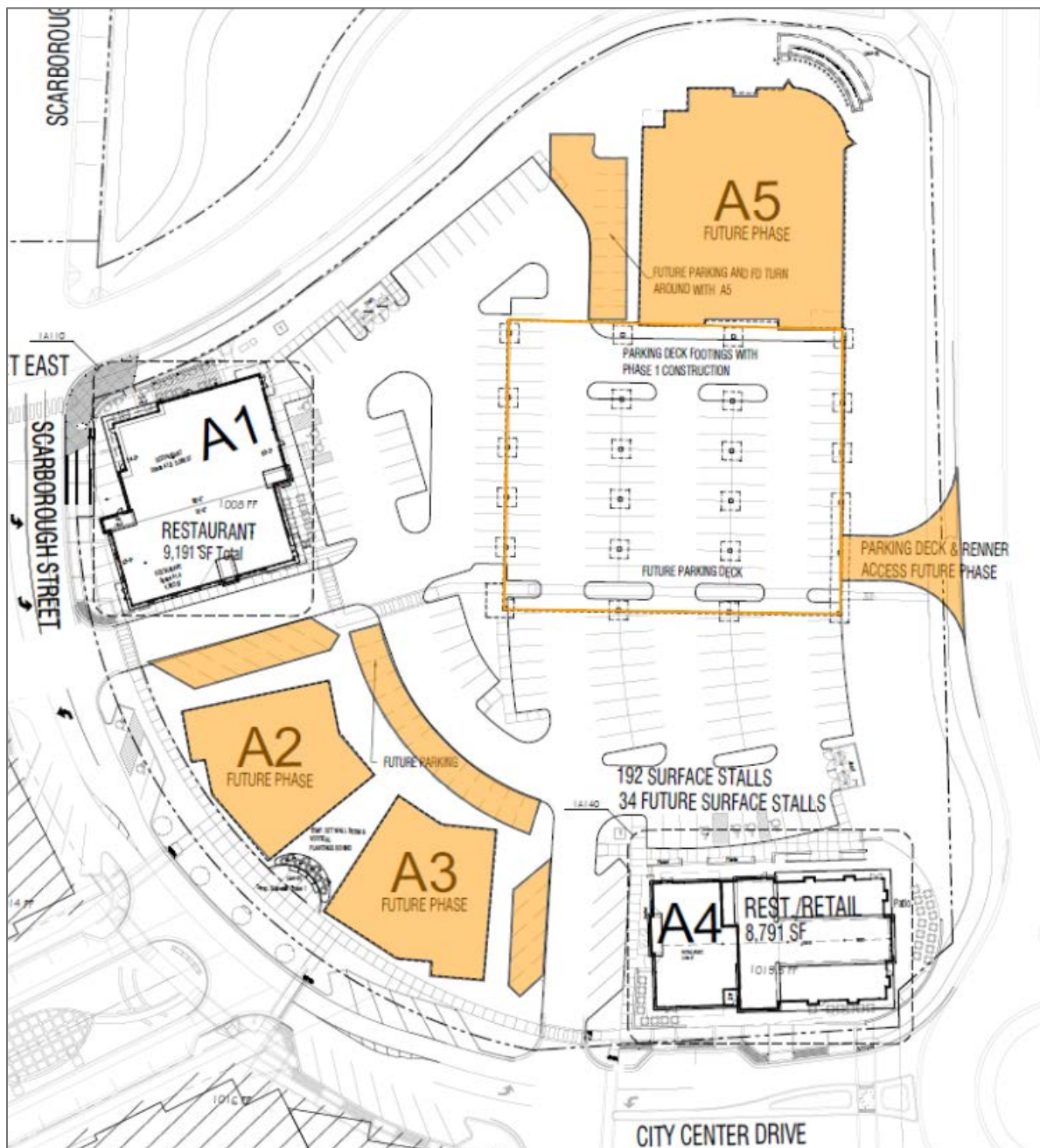


Exhibit 3: Phase 1 and Future Phases (Shown in Orange)

The proposed final plan is for Lots 10 and 12 within Area A, which consists of two restaurant/retail buildings (known as Buildings A1 and A4) at the east and west corners of Scarborough Street/City Center Drive.

The Revised Preliminary Plan (PL22-04PR) included five buildings labeled A1, A2, A3, A4, and A5. This labeling convention has been used for City Center since the initial plans were developed. Buildings A1 through A4 are intended to be restaurants (on the ground floor), while Building A5 and the upper floors of Buildings A2 and A3 are intended to be office or retail uses.

Phase 1 (current application) includes Buildings A1 and A4 and a surface parking area. The parking lot plans include structural piers with this phase to prepare for the construction of the raised level of the parking deck. Installing the structural piers as part of Phase 1 will result in minimal disruption in the future phase when the parking deck is constructed. The two-level parking deck with 105 spaces and 34 additional surface parking spaces will be constructed as part of the next phase (Phase 2) of the development for a total of 368 parking spaces for the development at full buildout.

PUBLIC IMPROVEMENTS

- There will be the complete amenity package within the site, including decorative pavers and modular planters along the streets.
- A sidewalk connection along the street outside of the developed project area will be installed with Phase 1. This includes:
 - A pedestrian seat wall area will along Scarborough Street between future building A2 and A3.
 - Installation of sidewalk and streetlights along Scarborough Street.

The area between the buildings and the street is referred to the pedestrian amenity zone. City Center has a distinctive pattern using rectangular modular blocks of contrasting brick pavers, patterned concrete, and landscape planters. Additional enhancements such as a special score pattern, grade changes using stairs and walls, and bringing seating patios into the pedestrian amenity zones are encouraged to create unique features and special spaces. The proposed streetscape along Scarborough Street and City Center Drive implements these elements in front of the restaurant tenant spaces.



Exhibit 4: Decorative Pavers and Modular Planter Pattern along Streets in City Center

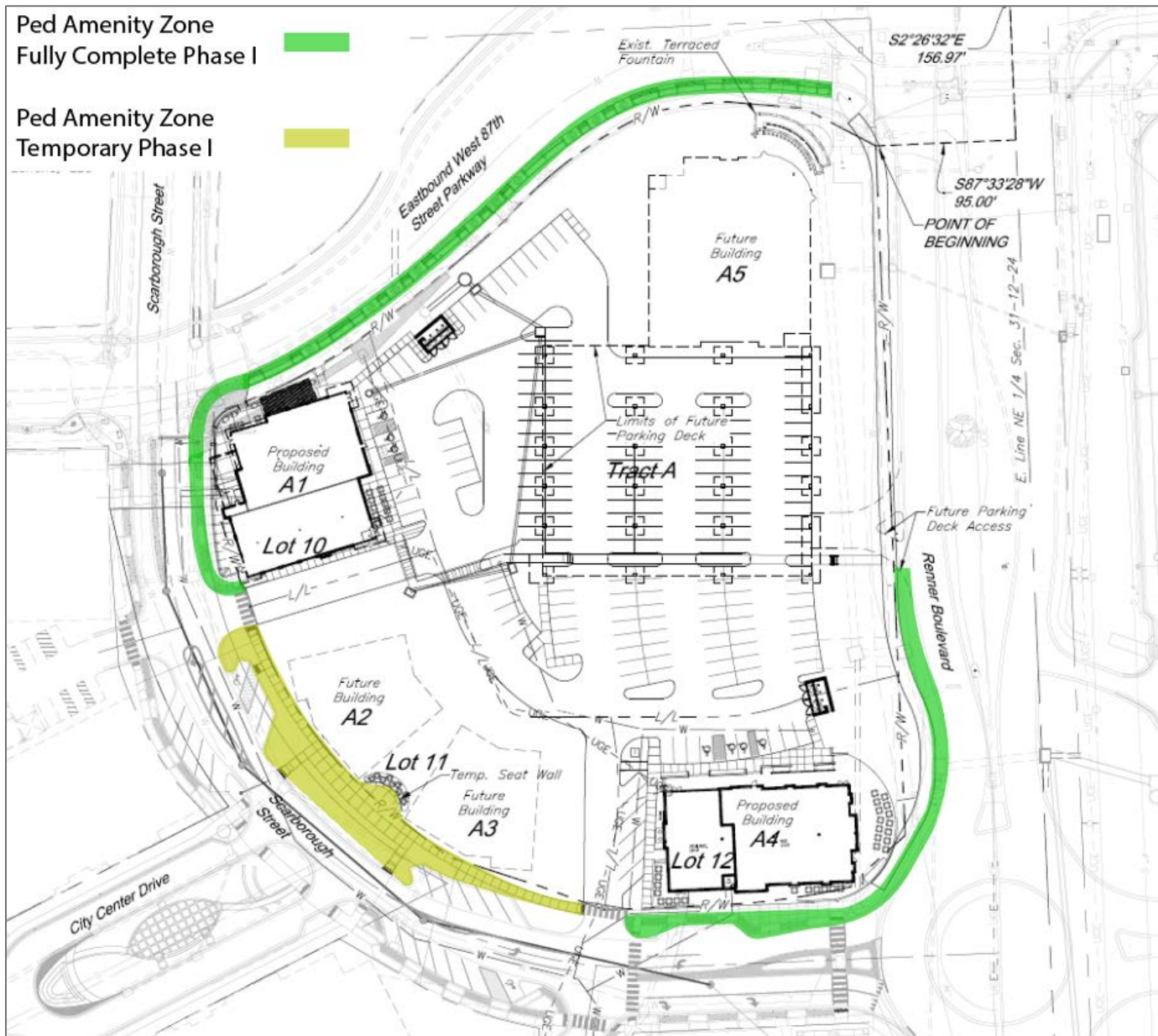


Exhibit 5: Pedestrian Amenity Areas to be Completed with Phase 1 (Current Phase)

Building A1 and 87th Street Amenity Zone – To be fully completed with Phase 1. This zone incorporates a patio with seating along the north side of the building and large patio space leading out to the pedestrian amenity zone. This space will include grade changes from the street level to the patio level. The sidewalk continues around all sides of the building to connect to a second patio space for another restaurant tenant at the building's southeast corner. The sidewalk along 87th Street Parkway is required to be updated to include the same geometric pavement pattern used in the other pedestrian amenity zones throughout the majority of City Center.

Building A4 and South Renner Boulevard Amenity Zone – To be fully completed with Phase 1. This zone incorporates seating patio spaces on the east, south and west sides of the buildings. This space will also use low walls to enclose the patio spaces and allow for grade changes from the street to the building. All seating patios are designed to integrate the space where customers of the restaurants are visible from the pedestrian amenity zones. This type of activity helps create an active presence along the street. The permanent sidewalk along Renner Boulevard is a 10-foot-wide concrete path; the sidewalk along the east side of the site is currently

a temporary asphalt surface. The wide sidewalk is considered a link in the City's Park Trail system. The developer will be required to reconstruct the sidewalk to the City standard 10-foot-wide concrete path north from Building A4's amenity area along Renner to the location of the future parking deck entrance.

Buildings A2 & A3 and Scarborough Street/City Center Drive Amenity Zone – To be completed to a temporary condition with Phase 1. A temporary sidewalk connection with semi-circular plaza area will be constructed along Scarborough Street with Phase 1 in front of future buildings A2 and A3. This amenity area is to be built out to ultimate permanent standards during the future phase when the buildings are constructed. Ultimate buildout of this amenity area will be an extension of the covered outdoor space between buildings A1 and A4 built with Phase 1.

Building A5 and North Renner Boulevard Amenity Zone – To be completed with Phase 2. Building A5, the two-story office building at the northeast corner of the block, will have a patio space to connect to the sidewalk along 87th Street Parkway. The patio space will also incorporate a grade change and will have a stair section that allows for the grade transition from the building to the street. This pedestrian area and patio space will be further developed with the final plan for the building.

TRAFFIC, ACCESS, AND PARKING

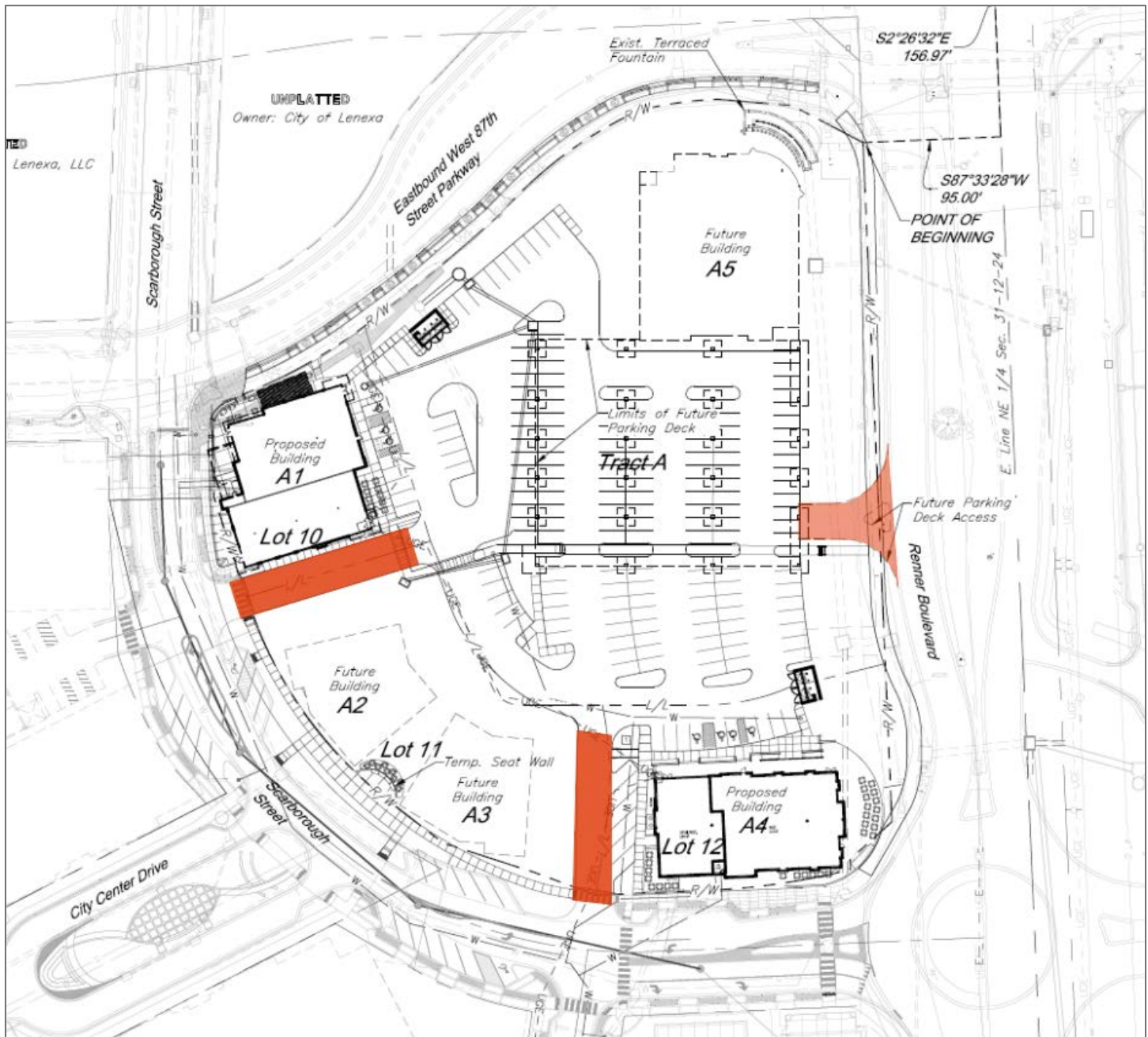


Exhibit 6: Access Points

- West 87th Street Parkway and Renner Boulevard are classified as Major Arterial roads.
- Scarborough Street and City Center Drive are classified as a Local Road.
- There are two access points into the site from Scarborough Street/City Center Drive right-of-way, which will be constructed with Phase 1.
- The top level of the future parking deck will only have access from Renner Boulevard (Phase 2) and includes a required right turn lane (not shown, also Phase 2).

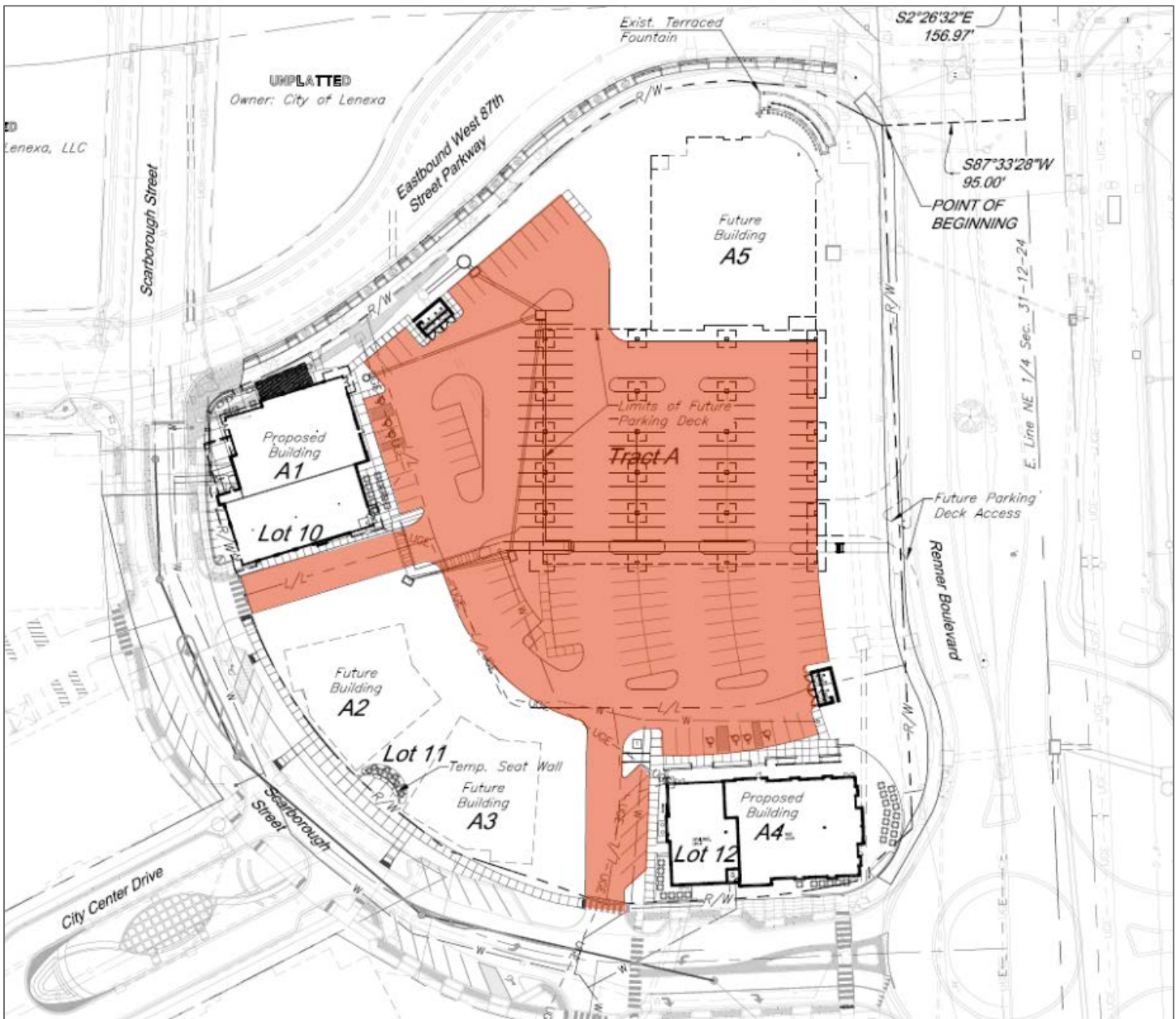


Exhibit 7: Phase 1 Proposed Parking Area

The first phase will include 192 spaces in the surface parking lot, 24 street parking spaces along Scarborough Street, and 13 spaces on City Center Drive.

It is the expectation that users will share parking and that visitors and customers will park in parking lots and structures nearby then walk among the different areas in City Center. The block does not provide for the number of parking stalls as typical requirement based on the square footage of the restaurant and office uses. The City Center Guidelines allow for the applicant to provide a plan that reflects the anticipated demand for parking and for the Planning Commission and Governing Body to approve of the proposed number of parking stalls. It is Staff's opinion that the proposed number of parking stalls are sufficient based on the complementary uses of office for daytime visitors and restaurants for evening visitors. The applicant provided a parking model of office and retail uses and noted the demand for parking being different based on peak operation times. If additional

parking is needed for the uses, City Center is designed so that alternate parking areas are nearby and visitors may park off-site and walk to the destination.

STORMWATER

This project does not require stormwater improvements. Stormwater has been accounted for within the entirety of the City Center development.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

LIGHTING

Proposed lighting includes internal parking lot lighting in the parking lot area as well as public street lighting along Scarborough Street/City Center Drive. The photometric plan is in compliance with the performance standards of [Section 4-1-C-4-I](#) of the Unified Development Code (UDC).

LANDSCAPING

The landscape plan will be installed with Phase 1 of this development. The plans include landscaping within the pedestrian amenity zone in the geometric shaped planters along the curbs and sidewalk. Planters will be designed in the spaces next to the building that provide grade changes and include ramps and stair sections. The building tenants will also use potted plants in the outdoor seating patio and plaza spaces.

The portion of the site that has surface parking out to the edge of the block will incorporate berms, and potentially low retaining walls, to reduce the visibility into the surface parking lot. The ability to use long sections of retaining walls is limited by the location of underground utilities along Renner Boulevard and 87th Street Parkway. Where retaining walls are used, the wall will face into the parking area because the surrounding streets are at a higher elevation.



12 of 14

ARCHITECTURE

The buildings are all a contemporary architectural style. The buildings use a mix of brick, random and cut stone, stucco, metal, and glass. The materials are applied in varying amounts and in various application to create variety and individuality among the group of buildings in the development. Different types of details are used to further develop the unique character among the buildings. Both proposed buildings with this phase are one-story and are designed with a taller roof line and use architectural elements to appear like they have a second story.

BUILDING A1		
Height	Square Footage	Units
29' 2"	9,191	2 tenant spaces



Exhibit 9: Building A1 Architectural Rendering Looking East from Scarborough.

Building A1 will be predominantly red brick and glass with a contrasting gray color brick column element on the façades. This element will provide for a plane change and roof line change. A stone pattern detail will be applied to the gray brick column element. Metal awnings will cantilever over the building entrances. A series of clerestory windows are shown atop the taller sections of the building. The cornice element along the roofline changes with the articulation and material changes of the building avoiding a continual design to this detail of the building.

BUILDING A4		
Height	Square Footage	Units
24' 4"	8,534	2 tenant spaces



Exhibit 10: Building A4 Architectural Rendering Looking Northeast from Scarborough.

Building A4 uses both red and gray brick colors on to differentiate the articulated elements along the building façade. Building A4 incorporates a vertical brick pattern detail with a tan color brick. This detail is used as a relief element in the columns. The roof form is a butterfly roof where the roof plane slope back into the middle of the building revealing a clerestory element down the central spine of the building. The roof feature is a striking design that provides interest in the roof plane and places focus on the series of clerestory windows. The proposed building meets architectural standards as outlined in [Section 4-1-C-5](#) of the UDC.

DEVIATIONS

The applicant is not requesting any deviations.

REVIEW PROCESS

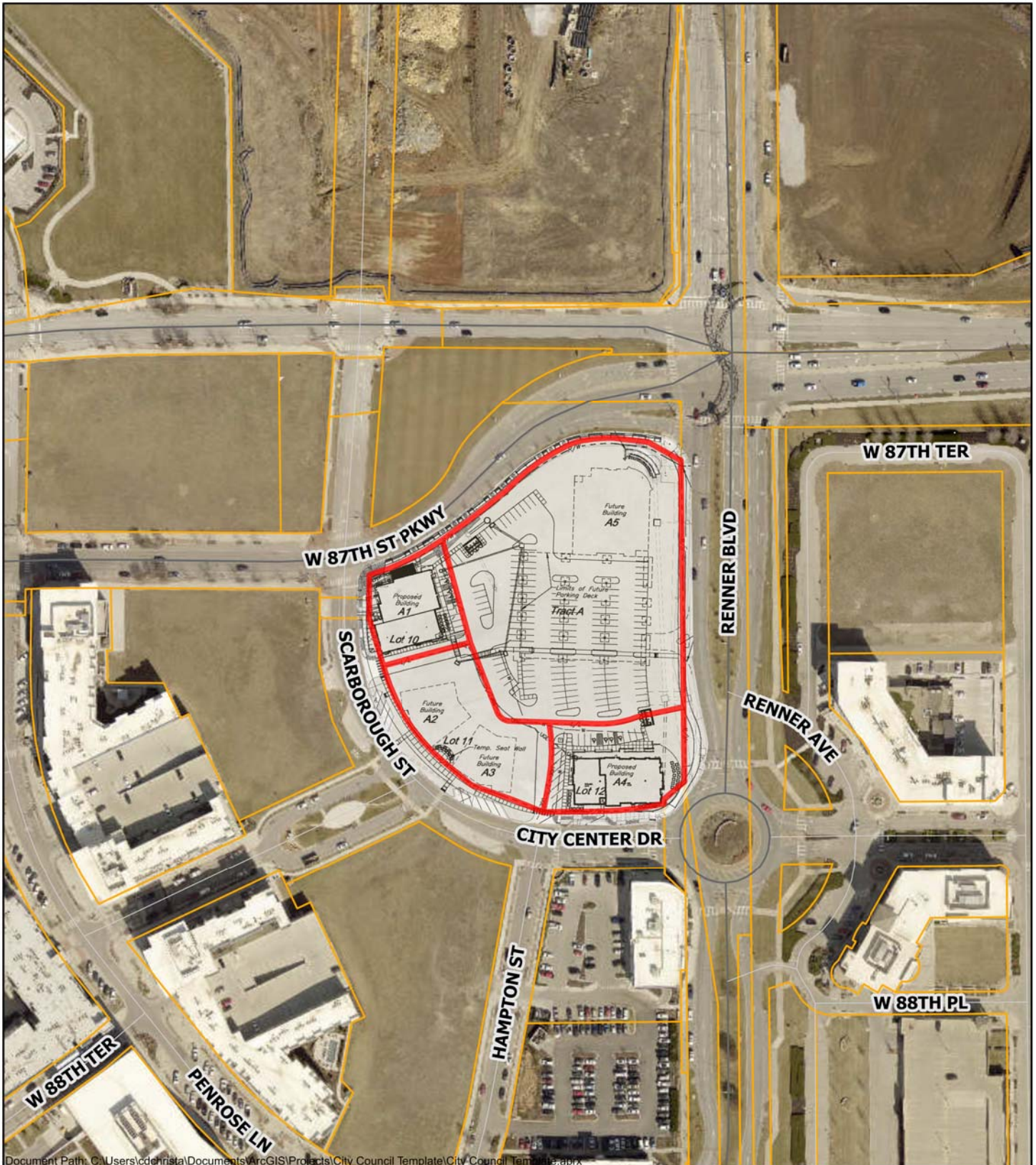
- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on February 7, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

- ★ **Staff recommends approval of the proposed Final Plan for Area A Restaurant Row – A1 & A4.**
 - This application is for two mixed-use buildings, A1 and A4 of the Restaurant Row development in City Center Area A. This project is consistent with Lenexa's goals through **Responsible Economic Development** to create **Vibrant Neighborhoods** and **Inviting Places**.

FINAL PLAN

Staff recommends **approval** of the final plan for PL22-04PR – **Area A Restaurant Row – A1 & A4** at the southwest corner of 87th Street Parkway and Renner Boulevard, for a mixed-use development.



Document Path: C:\Users\cdchris1a\Documents\ArcGIS\Projects\City Council Template\City Council Template.aprx

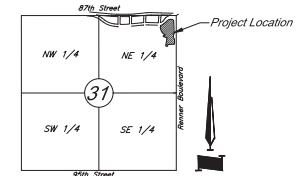
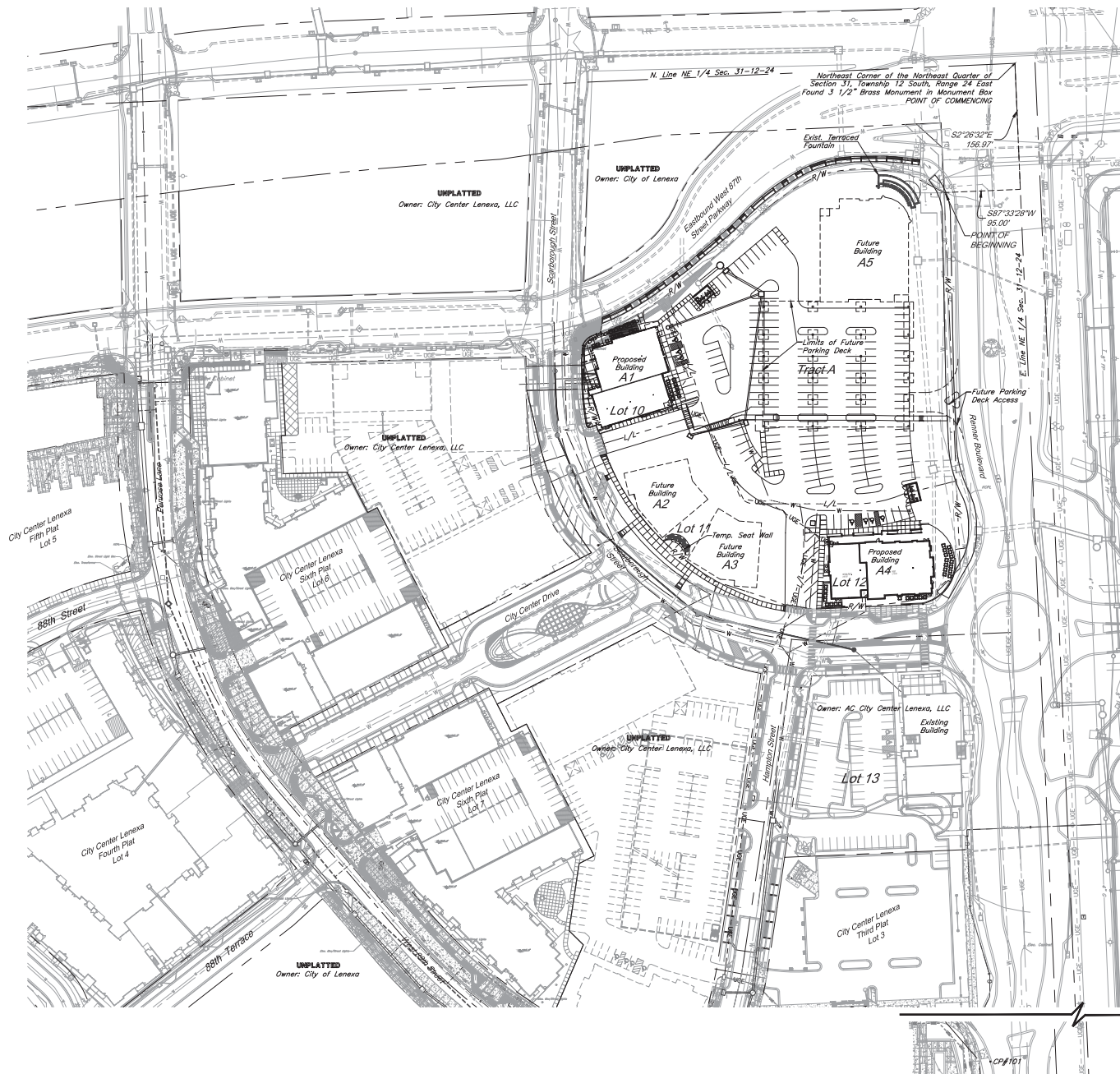
Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

City Center Lenexa Area A Restaurant Row - A1 & A4 PL23-02F



0 100 200 400 Feet





VICINITY MAP
Section 31-T12S-R24E
Scale: 1"=2000'

PROPERTY DESCRIPTION

Tract A, Lot 11, Lot 12 and Lot 13, CITY CENTER LENEXA NINTH PLAT, a subdivision in the City of Lenexa, Johnson County, Kansas.

PROJECT BENCHMARKS

CP #101 Chiseled Plus cut in top back of curb on the West side of Renner Boulevard, South of the first roundabout, North of 80th Street. Elev. 1030.15

State Plane Coordinates:
N 247113.4988
E 2227707.0099

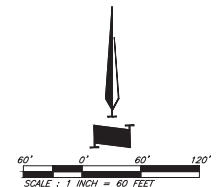
CP #100 Chiseled Plus cut in top back of curb on the West side of Renner Boulevard, North of the roundabout, at 90th Street. Elev. 1026.34

State Plane Coordinates:
N 246141.6709
E 2227745.4630

Note: Project CAF: 0.999923077

Legend

CP#101	CP#101	Barbed Wire Fence
CP#102	CP#102	Centerline
CP#103	CP#103	Fiber Optic Line
CP#104	CP#104	Gas Line
CP#105	CP#105	Guard Rail
CP#106	CP#106	Over Head Electric
CP#107	CP#107	Over Head Telephone
CP#108	CP#108	Over Head Cable TV
CP#109	CP#109	Property Line
CP#110	CP#110	Right-of-Way Line
CP#111	CP#111	SAN
CP#112	CP#112	Sanitary Sewer Line
CP#113	CP#113	Stream
CP#114	CP#114	Tree Line
CP#115	CP#115	Underground Electric
CP#116	CP#116	Underground Telephone
CP#117	CP#117	Underground Cable TV
CP#118	CP#118	Water Line
CP#119	CP#119	Proposed Grades
CP#120	CP#120	Proposed Storm Sewers
CP#121	CP#121	Existing Grades
CP#122	CP#122	Existing Storm Sewers
CP#123	CP#123	Tree Deciduous
CP#124	CP#124	Tree Deciduous
CP#125	CP#125	Tree Deciduous
CP#126	CP#126	Tree Deciduous
CP#127	CP#127	Tree Deciduous
CP#128	CP#128	Tree Deciduous
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CP#147	CP#147	Tree Deciduous
CP#148	CP#148	Tree Deciduous
CP#149	CP#149	Tree Deciduous
CP#150	CP#150	Tree Deciduous



CITY CENTER LENEXA, LOT A

FINAL DEVELOPMENT PLAN FOR:

REVISION

12/23/2022 City Comments

PROJECT NUMBER

12658_3A

DATE

12/5/2022

DESIGNED

DRAWN

REVIEWED

CBL

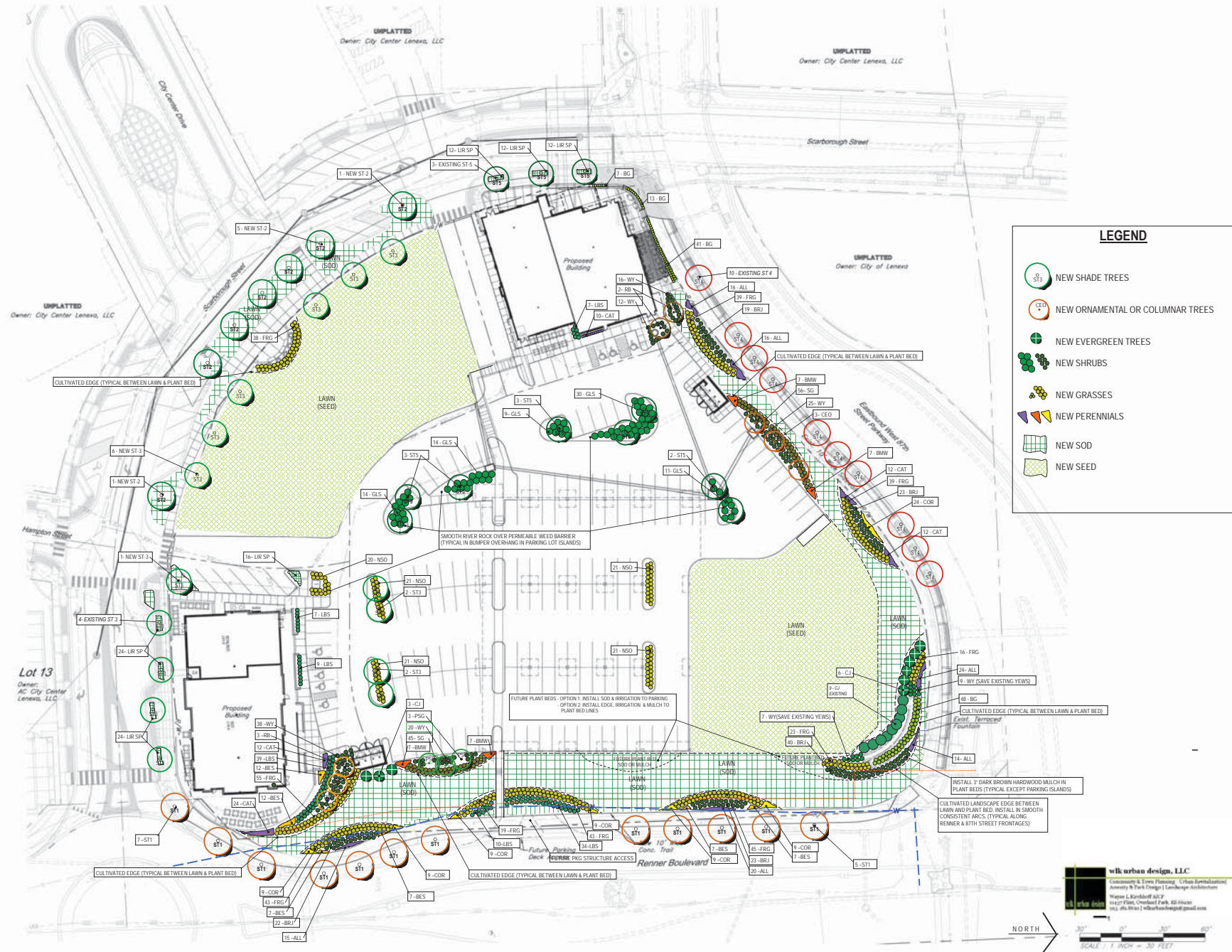
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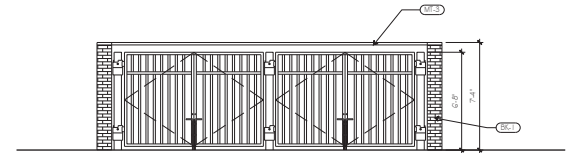
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SHEET NUMBER

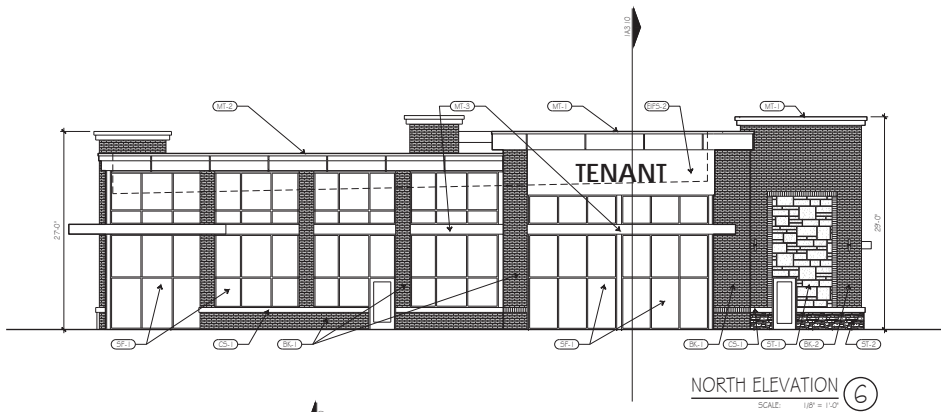
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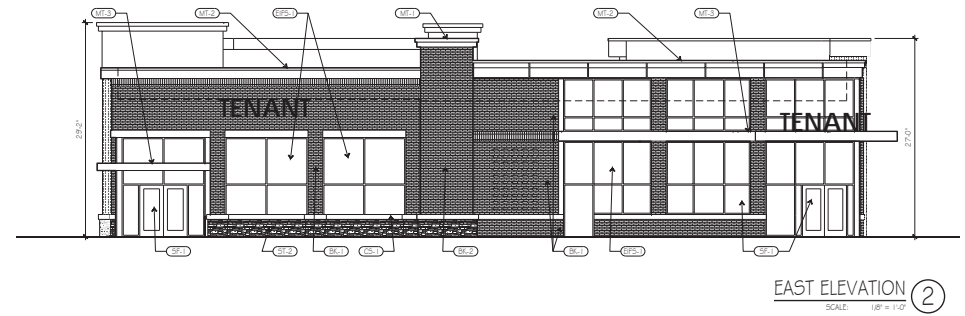




TRASH ENCLOSURE ELEVATION ③
SCALE: 1/4" = 1'-0"



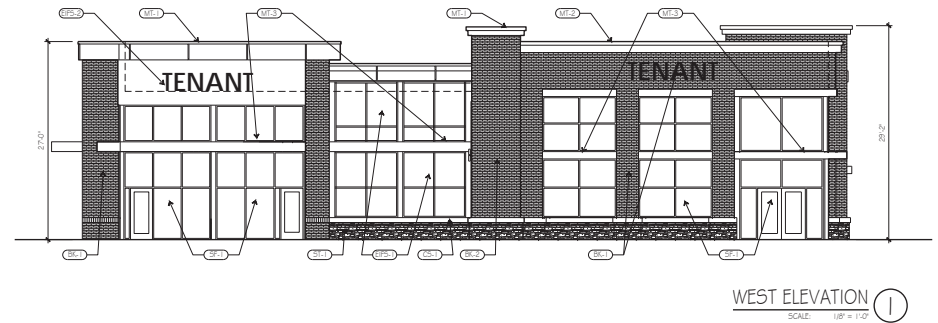
NORTH ELEVATION ⑥
SCALE: 1/8" = 1'-0"



EAST ELEVATION ②
SCALE: 1/8" = 1'-0"



SOUTH ELEVATION ⑤
SCALE: 1/8" = 1'-0"



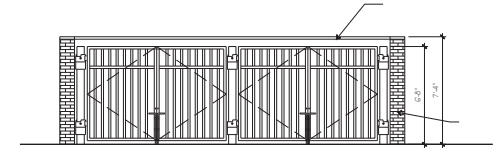
WEST ELEVATION ①
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A210 A1 ELEVATIONS

SCALE: 1/8" = 1'-0"

CITY CENTER LENEXA AREA A REDEVELOPMENT |

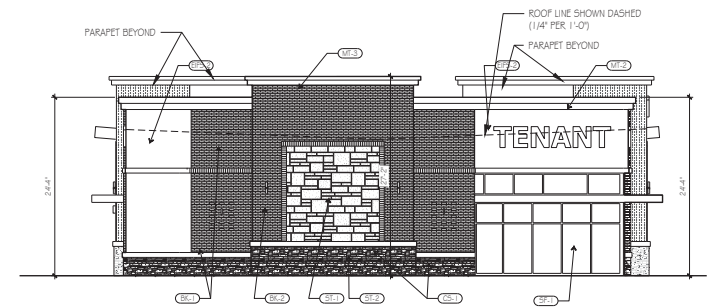
COPAKEN BROOKS | 12.05.2022



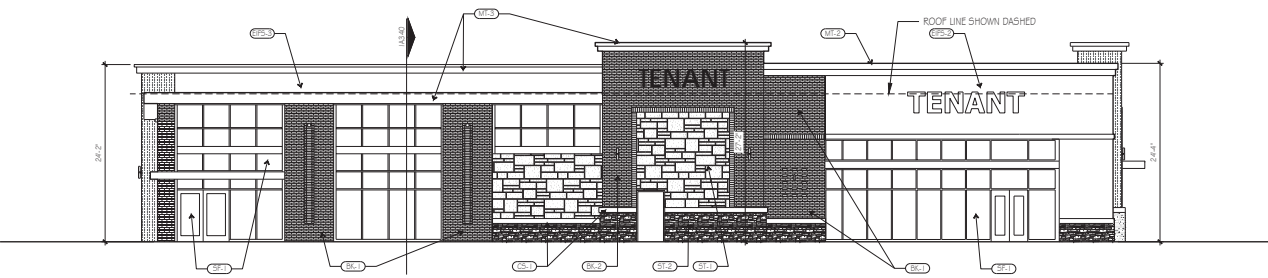
TRASH ENCLOSURE ELEVATION ③
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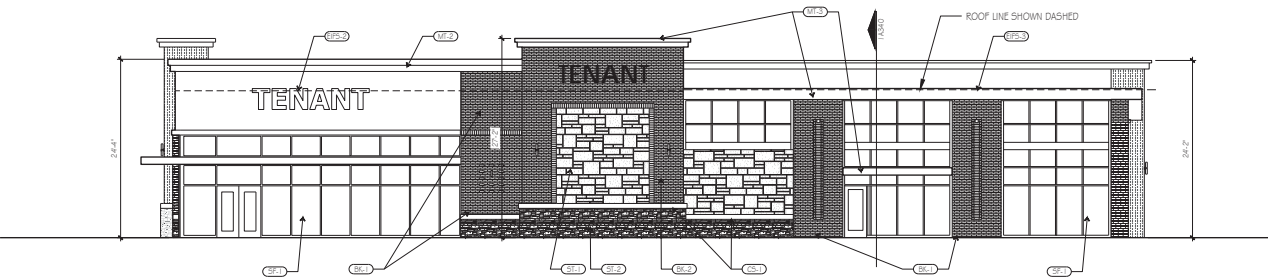
EAST ELEVATION ②
SCALE: 1/8" = 1'-0"



WEST ELEVATION ①
SCALE: 1/8" = 1'-0"



NORTH ELEVATION ⑥
SCALE: 1/8" = 1'-0"



SOUTH ELEVATION ⑤
SCALE: 1/8" = 1'-0"

A240 A4 ELEVATIONS

SCALE: 1/8" = 1'-0"

CITY CENTER LENEXA AREA A REDEVELOPMENT |

COPAKEN BROOKS | 12.05.2022

8813 PINEHURST LANE, SUITE 400 • LENEXA, KS 66151
PH: 913.586.8150 • FX: 913.586.1215 • WWW.KDOVERARCHITECTS.COM

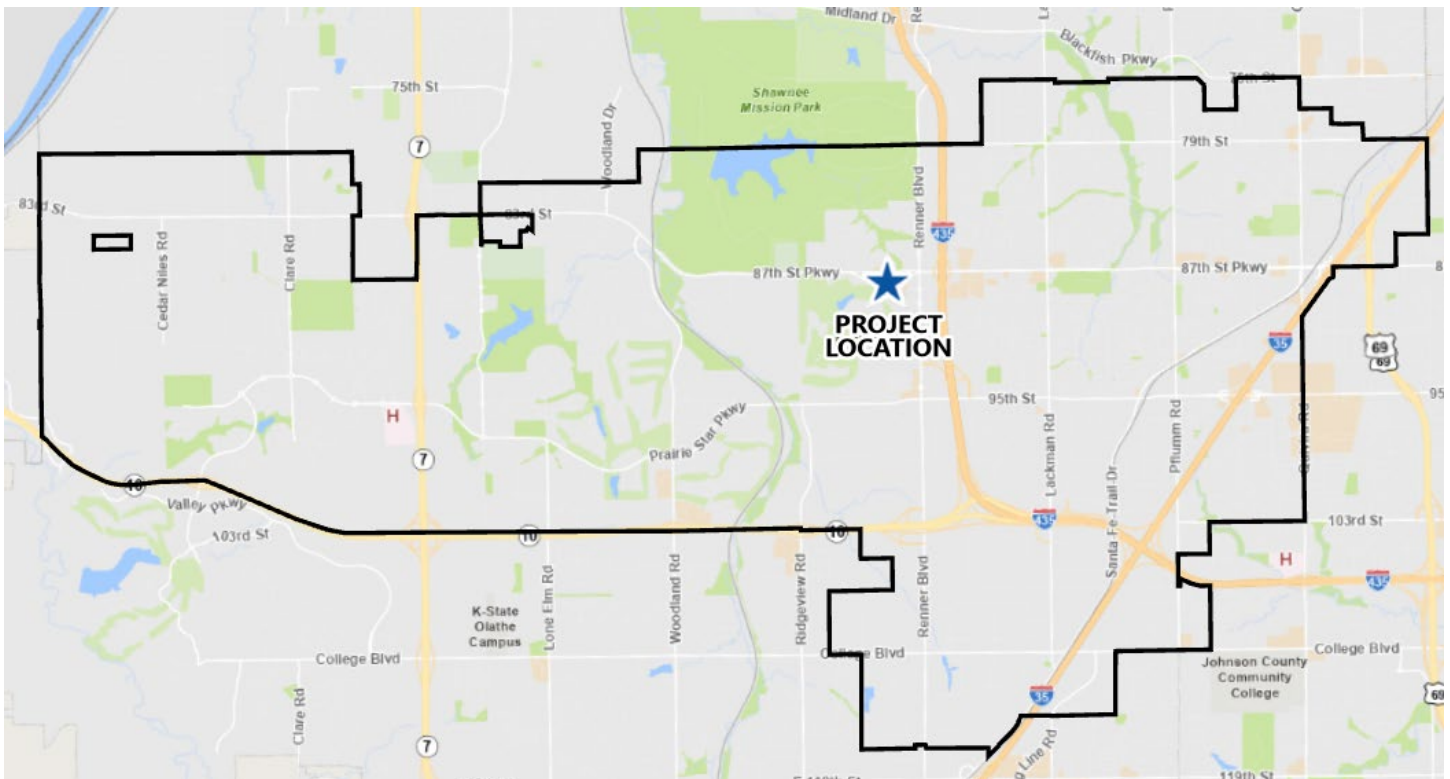


A1 VIEW LOOKING EAST FROM SCARBOROUGH



A4 VIEW LOOKING NORTH FROM SCARBOROUGH

REDEVELOPMENT PROJECT PLAN 1J FOR THE CITY CENTER TIF DISTRICT (CITY CENTER AREA A RESTAURANT ROW BUILDINGS A1 & A4)



PROJECT SUMMARY

On January 17, 2023, the City Council will consider a resolution giving notice of its intent to hold a public hearing on February 21, 2023 to consider approving Redevelopment (TIF) Project Plan 1J (“Project Plan 1J”). Project Plan 1J covers approximately 2.96 acres located on the northwest corner of Renner Blvd and the Scarborough St. (the “Project Plan 1J Area”). The Developer for Project Plan 1J is AC2CCL, LLC (the “Developer”), a related entity to City Center Lenexa, LLC, which is the Master Developer of City Center Lenexa pursuant to a separate master development agreement with the City.

Pursuant to state law, before a redevelopment project plan may be considered by the City Council at a public hearing and formally adopted, the Planning Commission must first examine the proposed project plan in light of the City’s Comprehensive Plan and make a finding that the two plans are consistent. Attached for your review is a copy of the proposed Project Plan 1J.

Project Plan 1J contemplates construction of an approximately 9,000 sq. ft. restaurant/retail building and an approximately 8,800 sq. ft. restaurant and reimbursement with TIF increment generated from the Project Plan 1J Area for Private TIF Reimbursable Costs consisting of associated surface parking, landscaping, hardscaping, sidewalks, related site amenities, signage, infrastructure improvements, land acquisition and special assessments within the Project Plan 1J Area associated with financing the 87th St. Pkwy improvement and

special assessments within the Project Plan 1J area associated with financing the improvement of Hampton St. and Scarborough St. (Renner Blvd to 87th St. Pkwy) by special benefit district (the “SBD”).

Project Plan 1J also includes reimbursement to the City with TIF increment generated from the Project Plan 1J Area for Public TIF Reimbursable Costs associated with the Hampton/Scarborough SBD. A description of all the TIF reimbursable costs, the amount, priority and duration of reimbursements are set forth in a separate Disposition & Development Agreement between the City and the Developer.

Staff believes Project Plan 1J, as proposed, is consistent with the City’s Comprehensive Plan for the reasons noted below:

- The Planning Commission recommended approval of the Preliminary Plan for the Project Plan 1J development (PL22-04PR) at its November 7, 2022 Planning Commission meeting.
- A principle articulated in the Comprehensive Plan is that the City will create neighborhoods that include varying developments that relate to each other, as well as to parks, schools and other shared amenities. These neighborhoods, larger than just subdivisions, will include or directly relate to the commercial areas that serve their daily needs. They will include these different residential and commercial uses by employing logical and attractively designed transitions. The mixing of uses both vertically and horizontally within City Center helps achieve this purpose
- Project Plan 1J is located within, but on the outer edge, of the City Center Core. The Comprehensive Plan designates the City Center Core as a unique mixed-use development in the geographic center of Lenexa that will form the future heart of the City. Development should be tailored to an urban environment, and thus be of a greater intensity or density, with public open space and pedestrian-friendly streets and include a mix of employment, entertainment, office, retail, residential, recreation and civic uses.
- The Comprehensive Plan places an emphasis on walkability in the City Center Core and discourages the focus on the automobile as with other more suburban developments.
- The construction of a large number of multi-family units and office will drive the need for retail and restaurant uses, helping incorporate the mixing of uses, which is an important component of the City Center concept. The mixing of uses may be vertical or horizontal and will provide the opportunity for reduced travel distances, thereby creating greater efficiency in the infrastructure.
- Project Plan 1J includes the construction of sidewalks, landscaping and amenity zone along the Hampton/Scarborough St. with the intent of providing a pedestrian friendly area.

STAFF RECOMMENDATION

Staff recommends **ADOPTION** of the attached resolution, making findings that Project Plan 1J for the City Center (TIF) District is consistent with the Comprehensive Plan for the City of Lenexa, Kansas.

RESOLUTION NO. 2023 - _____

A RESOLUTION FINDING CITY CENTER REDEVELOPMENT (TIF) PROJECT PLAN 1J IS CONSISTENT WITH THE CITY OF LENEXA'S COMPREHENSIVE PLAN FOR DEVELOPMENT (CITY CENTER AREA A RESTAURANT ROW BUILDINGS A1 & A4 PROJECT)

WHEREAS, the Lenexa City Council on September 11, 2001 adopted Ordinance No. 4427 establishing a Redevelopment District pursuant to K.S.A. 12-1770 *et seq.*, as amended ("Original District"); and

WHEREAS, the Original District was amended on December 20, 2005, by Ordinance No. 4824 to include a total of approximately 424 acres (the "District", also referred to as the "City Center TIF District"); and

WHEREAS, the District is generally described as an area of approximately 424 acres located on all four corners of 87th Street and Renner Blvd., as well as a tract of land located east of I-435 at 87th St Pkwy; and

WHEREAS, pursuant to K.S.A. 12-1772, as amended, the City prepared Redevelopment Project Plan 1J for the City Center TIF District ("Project Plan 1J"); and

WHEREAS, Project Plan 1J covers an area of approximately 2.96 acres located at the northwest corner of Renner Blvd. and the Scarborough St. in the area referred to as City Center Lenexa; and

WHEREAS, K.S.A. 12-1772(b), as amended, requires the Planning Commission to make a finding that Redevelopment Project Plan 1J is consistent with the City of Lenexa Comprehensive Plan before said Plan may be considered at a public hearing and officially adopted by the Lenexa City Council; and

WHEREAS, as required by law, the Planning Commission has reviewed Project Plan 1J in light of the City's Comprehensive Plan for development; and

WHEREAS, the Planning Commission finds that the proposed development for the District as a mixed use, urban development is a valuable resource for the City; and

WHEREAS, the Planning Commission finds that the development of the property within Project Plan 1J of an approximately 9,000 sq. ft. restaurant/retail building and an approximately 8,800 sq. ft. restaurant, surface parking, related site amenities, and associated infrastructure improvements is consistent with the City's Comprehensive Plan for development.

NOW THEREFORE, BE IT RESOLVED that the Planning Commission:

SECTION ONE: The Lenexa Planning Commission pursuant to K.S.A. 12-1772 hereby finds that Redevelopment Project Plan 1J is consistent with the City of Lenexa Comprehensive Plan for development for the reasons discussed at its meeting on January 9, 2023 and makes the following specific findings:

- The Planning Commission recommended approval of the Preliminary Plan for the Project Plan 1J development (PL22-04PR) at its November 7, 2022 Planning Commission meeting.
- A principle articulated in the Comprehensive Plan is that the City will create neighborhoods that include varying developments that relate to each other, as well as to parks, schools and other shared amenities. These neighborhoods, larger than just subdivisions, will include or directly relate to the commercial areas that serve their daily needs. They will include these different residential and commercial uses by employing logical and attractively designed transitions. The mixing of uses both vertically and horizontally within City Center helps achieve this purpose
- Project Plan 1J is located within, but on the outer edge, of the City Center Core. The Comprehensive Plan designates the City Center Core as a unique mixed-use development in the geographic center of Lenexa that will form the future heart of the City. Development should be tailored to an urban environment, and thus be of a greater intensity or density, with public open space and pedestrian-friendly streets and include a mix of employment, entertainment, office, retail, residential, recreation and civic uses.
- The Comprehensive Plan places an emphasis on walkability in the City Center Core and discourages the focus on the automobile as with other more suburban developments.
- The construction of a large number of multi-family units and office will drive the need for retail and restaurant uses, helping incorporate the mixing of uses, which is an important component of the City Center concept. The mixing of uses may be vertical or horizontal and will provide the opportunity for reduced travel distances, thereby creating greater efficiency in the infrastructure.
- Project Plan 1J includes the construction of sidewalks, landscaping and amenity zone along the Hampton/Scarborough St. with the intent of providing a pedestrian friendly area.

SECTION TWO: This resolution shall become effective immediately upon adoption.

ADOPTED by the City of Lenexa Planning Commission this 9th day of January, 2023.

Chris Poss, Chairman

ATTEST:

Scott McCullough, Secretary

APPROVED AS TO FORM:

Steven Shrout, Assistant City Attorney

CITY CENTER REDEVELOPMENT (TIF) PROJECT PLAN 1J
CITY CENTER REDEVELOPMENT (TIF) DISTRICT AS AMENDED
(City Center Area A Restaurant Row Buildings A1 & A4 Project)

In accordance with K.S.A. 12-1770 *et seq.*, as amended (the “**Act**”), to promote, stimulate and develop the general and economic welfare of the city of Lenexa, Kansas (“**City**”), the Lenexa City Council adopted Ordinance No. 4427 on September 11, 2001, establishing a Redevelopment (TIF) District (the “**Original District**”). The Original District was amended on December 20, 2005 by Ordinance No. 4824 to include a total of approximately 424 acres (the “**District**,” also referred to as the “**City Center TIF District**”) and is legally described in attached **Exhibit A**. The City has identified multiple City Center development projects located within the City Center TIF District.

The Act allows one or more TIF projects to be undertaken by a city within an established district and any such project plan may be implemented in separate development stages. To date, the City has approved multiple TIF project plans in the area located on the southwest corner of 87th Street Parkway and Renner Boulevard, which area is commonly referred to as the “**Lenexa City Center**” area.

The City desires to establish Project Plan 1J as set forth herein (“**Project Plan 1J**”, also referred to as the “**Project Plan**”). Project Plan 1J will incorporate approximately 2.96 acres, located south of 87th Street and west of Renner Boulevard and is legally described on **Exhibit B** (the “**Project Plan 1J Area**”). The Developer for the Project Plan is AC2CCL, LLC (the “**Developer**”). .

Anticipated within Project Plan 1J is construction of two restaurant/retail buildings, land acquisition, surface parking, landscaping, hardscaping, utilities, sidewalks, related site amenities, signage, associated infrastructure improvements, and special assessments within the Project 1J Plan Area associated with the special benefit district financing of the 87th Street Parkway improvements and Scarborough Drive and Hampton Street improvements, all of which are more specifically described in **Section 5** herein (the “**Private Project**”).

Project Plan 1J shall extend for a period of twenty (20) years from the date the Project Plan is approved by the City (the “**Project Plan Term**”). The incremental ad valorem property taxes (as defined by the Act) generated from the real property within the Project Plan 1J Area during the Project Plan Term in excess of the amount of real property taxes collected for the base year assessed valuation shall constitute the “**TIF Revenues**”. In accordance with the Act and in cooperation with the Planning Commission, the City prepared Project Plan 1J.

1. Financial Feasibility.

Staff prepared a Financial Feasibility Study (“**Feasibility Study**”) for Project Plan 1J attached hereto as **Appendix 1**. Projections on development in the Project

Plan 1J Area were provided by the Developer. The Feasibility Study incorporates a number of assumptions, including a constant mill levy of 90.733, which excludes the 20 mill school levy and the 1.5 State mill levy. The mill levy may vary each year of the TIF Term based on legislative actions and budgetary decisions made by the individual taxing jurisdictions. It also assumes property tax collection at 100%, Private Project completion by January 1, 2025 and a one percent (1%) annual increase in appraised valuation after the Private Project is fully constructed and stabilized.

The Developer will advance funds necessary to construct the Private Project and to pay the costs associated with the estimated and approved, private TIF eligible reimbursable costs set forth generally on **Exhibit C** attached hereto (the “**Private TIF Reimbursable Costs**”), and it is contemplated that Developer will subsequently be reimbursed with TIF Revenues received by the City on a “pay-as-you-go” basis. Such advances and reimbursements will be made in accordance with the terms of a Disposition & Development Agreement executed by the Developer and the City (the “**DDA**”). The Private TIF Reimbursable Costs are set forth in more detail in the DDA. The City also identified various public reimbursable costs set forth generally on **Exhibit C** which include reimbursement to the City for the City’s portion of the costs of the Public Project constructed by the City by SBD (the “**Public TIF Reimbursable Costs**”), which costs are eligible for TIF reimbursement in accordance with the City Center TIF District Plan and the terms of the DDA. Collectively, the Private TIF Reimbursable Costs and Public TIF Reimbursable Costs are referred to as the “**TIF Reimbursable Costs**”.

There is an estimated total of \$5,544,391 in TIF Reimbursable Costs identified with Project Plan 1J, but reimbursement of TIF Reimbursable Costs is dependent upon the amount of TIF Revenues generated within the Private Project 1J Area during the Project Plan Term and received by the City, and shall be paid in accordance with the amount, priority and duration set forth in the DDA. In no event will any TIF Reimbursable Costs be reimbursed in an amount that exceeds the amount of TIF Revenues available.

The Feasibility Study indicates that if projected development, assessed values and tax revenues are accurate, TIF Revenues will be sufficient to reimburse the Developer for a portion of the approved Private TIF Reimbursable Costs. Other revenue sources, including but not limited to private equity, are available to meet Private TIF Reimbursable Costs and other private development costs associated with the Project. TIF Reimbursable Costs must (1) be reasonably approved by the City in accordance with the terms of the DDA; (2) meet the definition of “redevelopment project cost” set out in K.S.A. 12-1770a(o), as amended; (3) be an eligible expense under the City’s adopted TIF Policy and/or Procedures, unless otherwise permitted in the DDA; (4) be authorized in this Project Plan 1J and in the City Center TIF District Plan; and (5) be in compliance with the terms for reimbursement and prioritization described with particularity in the DDA.

The City has identified up to \$3,737,021 in Private TIF Reimbursable Costs and anticipates reimbursing Developer for such Private TIF Reimbursable Costs incurred and paid by the Developer with available TIF Revenues generated during the twenty (20) year Project Plan Term. The Public TIF Reimbursable Costs total \$1,770,000 plus the annual TIF Administrative Fee and will be reimbursed to the City with available TIF Revenues in the time and priority set forth in the DDA. Based on the current projections and cash flow analysis contained in the Feasibility Study, it is determined that the Project benefits, TIF Revenues and other available revenues, exceed the TIF Reimbursable Costs, and that the TIF Revenues and other available revenue sources, including private revenue sources for the private costs, should be sufficient to pay for such TIF Reimbursable Costs. For any improvements constructed by Developer in Project Plan 1J, the Developer is responsible for all expenses, including but not limited to, Private TIF Reimbursable Costs, even if they exceed the amount of available TIF Revenues. The City reserves the right to amend the specific approved TIF Reimbursable Costs, and the amount, duration and prioritization thereof, to conform to the provisions of the DDA. City may also amend this Project Plan 1J in accordance with state law and the DDA.

In summary, assuming Project Plan 1J approval in the first quarter of 2023 with construction commencing immediately thereafter and complete by January 1, 2025, the City anticipates the ad valorem property tax increment will generate approximately \$2,244,024 over the Project Plan Term (the “**Estimated Total TIF Revenue Projection**”). The Developer will be responsible for all expenses of Developer, including the Private TIF Reimbursable Costs, above the TIF Revenue generated from Project Plan 1J and allocated to the Private TIF Reimbursable Costs during the Project Plan Term. If the TIF Revenue does not meet the estimated total TIF Reimbursable Costs, the City shall be under no obligation to provide financial assistance to Developer beyond the TIF Revenues actually generated from the Project Plan 1J Area in accordance with the distribution formula and term set out in the DDA. A summary of the feasibility assumptions and Estimated Total TIF Revenue Projection is included in **Exhibit E**.

2. Redevelopment District Plan and Redevelopment (TIF) Project Plan 1J.

Redevelopment District Plan (City Center TIF District Plan)

The City Center TIF District area includes the land within the City of Lenexa, Kansas as legally described on **Exhibit A**, but generally described as an area of approximately 424 acres located on all four corners of 87th Street and Renner Blvd., as well as a tract of land located east of I-435 at 87th St Pkwy. The Redevelopment (TIF) District Plan for the City Center TIF District contemplates development of a mixed use urban development project to be located on all four corners of Renner Boulevard and 87th St Pkwy and east of I-435 at 87th St Pkwy, to include office, retail, residential and public civic uses (“**City Center Project**”).

Lenexa City Center will be constructed on approximately 56.85 acres located on the southwest corner of Renner Boulevard and 87th Street Parkway and is the subject of a Development Agreement dated May 31, 2006 (as amended) between City Center Lenexa, LLC (“**CCL**”), and the City (the “**Master Development Agreement**”). Lenexa City Center is proposed to be developed in multiple phases as a mixed use project consistent with the Master Plan included in the Master Development Agreement, which is contemplated to include approximately 1,900,000 square feet of retail, residential, hotel, office, entertainment and civic uses. The Lenexa City Center project proposes to utilize on-street, structured and surface parking consistent with the Master Plan in the Master Development Agreement. Development of additional City Center projects of a smaller scale are anticipated on the other tracts of land included in the District.

In accordance with the City Center District Plan, TIF increment may be used to pay for eligible project expenses within specific project areas for such items including but not limited to public infrastructure; land acquisition; site preparation; street improvements and their appurtenances; sidewalks; storm and sanitary sewers; utility improvements as permitted in the Act; parks; parking facilities; landscaping; water mains; storm water detention; sculptures and public art; plazas; and special assessments levied pursuant to KSA 12-6a01 *et seq.* for eligible public infrastructure authorized in the District Plan.

Redevelopment (TIF) Project Plan 1J

Project Plan 1J incorporates approximately 2.96 acres of real estate and improvements. Project Plan 1J is located south of 87th Street Parkway and west of Renner Boulevard, all within the City Center TIF District. Project Plan 1J Area is legally described in **Exhibit B**. Project Plan 1J consists of two restaurant/retail buildings, surface parking, landscaping, sidewalks and other site amenities, associated infrastructure improvements all of which are more specifically described in **Section 5** herein.

3. Map of Redevelopment Project Plan 1J Area.

A map of the Project Plan 1J Area is attached as **Exhibit D**.

4. Relocation Assistance Plan.

No relocation is required with Project Plan 1J and therefore no relocation assistance plan is provided.

5. Description of the Buildings and Facilities Proposed to be Constructed or Improved.

The Private Project part of Project Plan 1J consists of an approximately 9,000 sq. ft. restaurant/retail building and an approximately 8,800 sq. ft. restaurant, surface parking, footings for a future parking deck, sidewalks, landscaping and associated infrastructure. Private TIF Reimbursable Costs incurred as a result of {LR: 00696913.5 }

Project Plan 1J include, but are not limited to, land acquisition within the Project Plan area, architectural and engineering costs associated with the site improvements (but excluding all other vertical buildings to be owned or leased by the Developer), infrastructure improvements, site development, surface parking, footings for a future parking deck, lighting, landscaping, hardscape, utilities located within the right-of-way, sidewalks, and related site amenities (insofar as landscaping, hardscaping, utilities, sidewalks and related amenities are not contained in the Public Project Improvements) and special assessments on the Project Plan 1J Area associated with the financing of the 87th St. Parkway improvements and the Scarborough Drive and Hampton Street improvements,. The Private TIF Reimbursable Costs are described in more detail in the DDA.

The Public Project part of Project Plan 1J consists of certain public improvements which include: special assessments within the Project Plan 1J Area associated with the special benefit district financing of the Scarborough Drive and Hampton Street improvements and future 87th Street Parkway and Scarborough Drive traffic signal improvements (the “Public Improvements”). Except as provided herein, the Public Improvements constitute Public TIF Reimbursable Costs. The Public TIF Reimbursable Costs and are described in more detail in the DDA. For the avoidance of doubt, the term “Public Improvements”, as used in this Project Plan 1J, is intended to only include future costs incurred by the City after the date this Project Plan 1J is approved and not costs previously incurred by the City; provided, however, the City’s share of the assessments associated with the SBD that was formed for the construction of Scarborough Drive (from Renner Boulevard to 87th Street Parkway) and Hampton Street (from its current terminus north to connect with Scarborough Drive) improvements do constitute Public Improvements and are Public TIF Reimbursable Costs.

6. Other Relevant Information.

- a. Reimbursement of TIF Reimbursable Costs shall be made from ad valorem property tax increment (as defined in the Act) actually received by the City from Project Plan 1J Area and deposited into the special fund established by the City in accordance with K.S.A. 12-1778 (the “**City Center Project Plan 1J Fund**”).
- b. If sufficient TIF Revenues are not available to pay all of the Private TIF Reimbursable Costs, the City is under no obligation to reimburse Private TIF Reimbursable Costs from any other public source. It is contemplated that TIF Reimbursable Costs also will be reimbursed by proceeds from a Community Improvement District that will be formed later.
- c. Prior to any reimbursement of Private TIF Reimbursable Costs, Developer and City shall enter into a separate, valid and enforceable DDA. A detailed description of all TIF Reimbursable Costs, and the procedure for distribution, reimbursement amount and priority of

{LR: 00696913.5 }

payment of the TIF Reimbursable Costs is set out in the DDA and consistent with this Project Plan 1J.

- d. The City does not anticipate issuing TIF Bonds, however, upon future request of Developer, the City shall reasonably consider any such request to issue TIF Bonds if the market can feasibly support such a bond issue and if the TIF Revenues and any other collateral provided for such TIF Bonds provide reasonable assurance that the principal of and interest on the TIF Bonds will be paid on a timely basis. A decision on whether or not a TIF Bond issue is feasible and adequately secured, will be the City's final decision and within the City's sole discretion. The City is under no obligation to issue TIF Bonds and makes no commitment to do so.

EXHIBIT A

LEGAL DESCRIPTION OF CITY CENTER REDEVELOPMENT DISTRICT

Beginning at the Northwest corner of the Northeast Quarter of Section 31, Township 12 South, Range 24 East; thence South along the West line of the Northeast Quarter of said Section 31 to the Southwest corner of the Northeast Quarter of said Section 31; thence East along the South line of the Northeast Quarter of said Section 31 to the Southeast corner of the Northeast Quarter of said Section 31, and continuing East along the South line of the Northwest Quarter of Section 32, Township 12 South, Range 24 East to the Easterly right-of-way line of Renner Boulevard as it now exist; thence North along the Easterly right-of-way line of said Renner Boulevard to the intersection with the South line of the Northwest Quarter of the Northwest Quarter of Said Section 32; thence East along the South line of the Northwest Quarter of the Northwest Quarter of said Section 32, to the intersection with the centerline of Interstate Route 435, as it now exists; thence South along the centerline of said Interstate Route 435 to the South line of the Northwest Quarter of said Section 32; thence East along the South line of the Northwest Quarter of said Section 32, to the Southeast corner of the Northwest Quarter of said Section 32; thence North along the East line of the Northwest Quarter of said Section 32, to the Northeast corner of the Northwest Quarter of said Section 32; thence West along the North line of the Northwest Quarter of said Section 32, to the centerline of Interstate Route 435, as it now exists; thence North along the centerline of said Interstate Route 435 to the North line of the Southwest Quarter of Section 29, Township 12 South, Range 24 East; thence West along the North line of the Southwest Quarter of said Section 29, to the Northwest corner of the Southwest Quarter of said Section 29: thence South along the West line of the Southwest Quarter of said Section 29, to the Northeast corner of the Southeast Quarter of the Southeast Quarter of Section 30, Township 12 South, Range 24 East; thence West along the South line of Stonecreek of Parkhurst 1st Plat, Stonecreek of Parkhurst 2nd Plat, and Estates of Parkhurst 1st Plat, all subdivisions of land in Johnson County, Kansas, to the Northeast corner of Horizons West Re-Plat 3rd Plat, a subdivision of land in Johnson County, Kansas; thence South along the East line of said Horizons West Replat 3rd Plat, and its extension South, to the South line of the Southeast Quarter of said Section 30; thence West along the South line of the Southeast Quarter of said Section 30 to the Point of Beginning, containing approximately 424 acres.

EXHIBIT B
LEGAL DESCRIPTION OF PROJECT PLAN 1J AREA

All Tract and Lots 10 12, City Center Lenexa Ninth Plat, subdivision in the City Lenexa, Johnson County, Kansas, EXCEPT that part of Tract A, as prepared by Michael Dean LS-1612, on December 30, 2022; and being more particularly described as follows:

BEGINNING at the Northeast corner of said Tract A, said point also being the point of intersection of the South Right-of-Way line of W. 87th Street Parkway and the West Right-of-Way line of Renner Boulevard, as both are now established; thence South 02°26'40" East, along the East line of said Tract A, and along the West Right-of-Way line of said Renner Boulevard, a distance of 143.89 feet; thence North 90°00'00" West, departing the East line of said Tract A and the West Right-of-Way line of said Renner Boulevard, a distance of 175.69 feet; thence North 45°00'00" West, a distance of 16.97 feet; thence North 00°00'00" East, a distance of 47.16 feet; thence North 40°07'25" West, a distance of 49.95 feet; thence North 12°14'39" East, a distance of 39.31 feet, to a point on the North line of said Tract A, said point also being on the South Right-of-Way line of said W. 87th Street Parkway, said point also being on a non-tangent curve; thence Northeasterly, along the North line of said Tract A, and along the South Right-of-Way line of said W. 87th Street Parkway, and along a curve to the right, whose initial tangent bearing is North 53°26'22" East, having a radius of 282.00 feet, and through a central angle of 09°58'36", an arc length of 49.10 feet, to a point of compound curvature; thence Northeasterly and Easterly, continuing along the North line of said Tract A and along the South Right-of-Way line of said W. 87th Street Parkway, and along a curve to the right, having a radius of 82.00 feet, and through a central angle of 15°06'48", an arc length of 21.63 feet, to a point of compound curvature; thence Easterly, continuing along the North line of said Tract A, and along the South Right-of-Way line of said W. 87th Street Parkway, and along a curve to the right, having a radius of 266.00 feet, and through a central angle of 09°15'33", an arc length of 42.99 feet, to a point on a non-tangent line; thence South 85°10'52" East, continuing along the North line of said Tract A, and along the South Right-of-Way line of said W. 87th Street Parkway, a distance of 52.05 feet; thence South 62°24'31" East, continuing along the North line of said Tract A, and along the South Right-of-Way line of said W. 87th Street Parkway, a distance of 54.99 feet, to the POINT OF BEGINNING of that part being excepted from said Tract A.

The hereon described TIF Plan Area contains 128,973.72 square feet, or 2.9608 acres, more or less.

EXHIBIT C
TIF Reimbursable Costs– Project Plan 1J

The following items are estimated TIF eligible private and public costs for reimbursement with TIF Revenues generated from Project Plan 1J. The priority and duration of reimbursement is set forth in the DDA.

Description of Expenditure	Reimbursement to:	Maximum Reimbursement
TIF Fee	Developer ¹	\$37,370
Itemized Private TIF Reimbursable Costs paid by Developer, excluding the TIF Fee, but including: Land acquisition within the Project Plan area; site development; A/E (excluding vertical buildings owned or leased by the Developer other than parking structures); water mains; surface and structured parking; landscaping, lighting, sidewalks, benches and similar amenities; special assessments in the Project Plan Area associated with financing the 87 th Street Parkway improvements and the Scarborough Dr. (Renner Blvd. to 87 th St. Pkwy.) and Hampton St. (current terminus north to Scarborough Dr.) improvements; temporary construction interest; and contingency.	Developer	\$3,737,021 ³
Itemized Public TIF Reimbursable Costs paid by City, excluding the Annual Administrative TIF Fee, but including: The City's share of special assessments within the Project Plan 1J Area associated with the special benefit district financing of the Scarborough Drive and Hampton Street improvements and future 87 th Street Parkway and Scarborough Drive traffic signal improvements; and public park improvements within the District	City	\$1,770,000
Annual Administrative TIF Fee: 0.5% of the annual TIF Revenues reimbursed to Developer	City	TBD
Total Maximum Aggregate of Eligible Private TIF Reimbursable Costs	Developer	\$3,774,391
Total Maximum Aggregate of Eligible Public TIF Reimbursable Costs	City	\$1,770,000 ²
Total Maximum Aggregate of TIF Reimbursable Costs		\$5,544,391²

Notwithstanding any other provision of this Plan to the contrary, reimbursable expenditures shall at all times be consistent with the Act, including judicial interpretation of the Act.

¹ The TIF Fee is based upon 1% of the Private TIF reimbursable Costs in the estimated amount of \$3,737,021 (which amount represents the Total Private TIF Reimbursable Costs less the TIF Fee). This total excludes the Annual Administrative TIF Fee as it is TBD based upon eligible TIF Revenue disbursed. This sum shall be reimbursed to Developer if it has been paid by Developer and if not, it shall be deducted from the first Private TIF Reimbursable Cost payment (and thereafter until paid in full) and paid to the City.

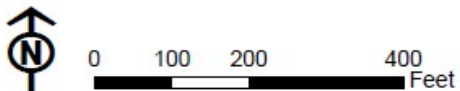
² The amount of the total Public TIF Reimbursable Costs does not include a sum for the Annual Administrative TIF Fee as this amount is to be determined as it is based upon the annual amount of TIF Revenues disbursed to Developer.

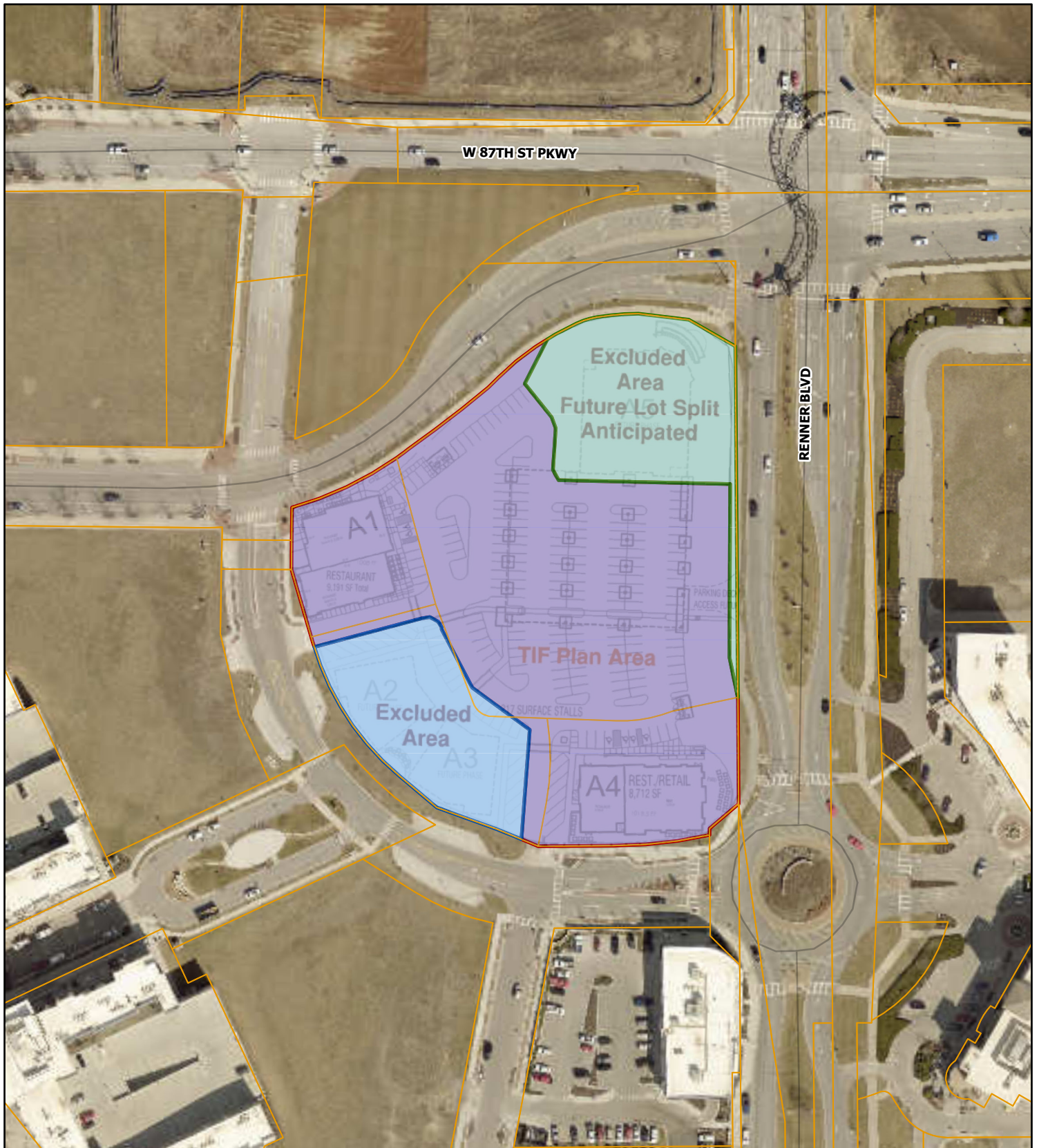
³ This amount includes sums which may be included in a subsequent Community Improvement District. At the time of certifying its costs to the City, Developer must elect the Project Plan and the incentive source for which it desires reimbursement and it shall not submit those same costs for reimbursement from any other reimbursement source. CID revenues are anticipated to reimburse tenant finish costs associated with the restaurant, which are not an eligible TIF reimbursable cost. This amount also includes costs associated with surface parking and footings for future parking structure, including site work, that may be reimbursed with TIF revenue in future phases that utilize the surface parking and parking structure, to the extent those TIF reimbursable costs are not reimbursed with TIF revenues from Project Plan 1J or CID revenues.

EXHIBIT D



Restaurant Row Phase 1 Proposed Boundaries
City of Lenexa





Restaurant Row Phase 1 Proposed Boundaries
City of Lenexa

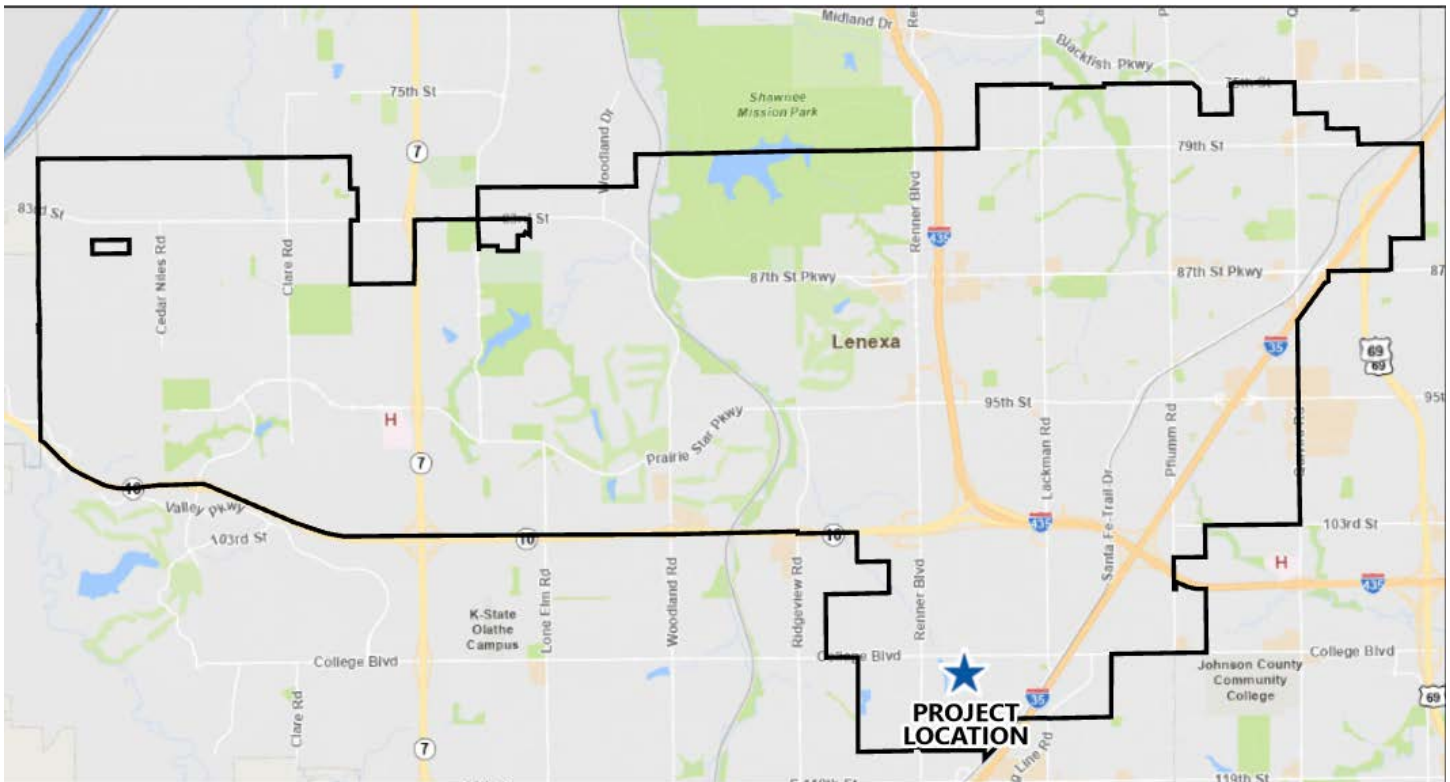


0 100 200 400 Feet



SOUTHLAKE TWENTY-FIRST PLAT

Project #:	PT23-04F	Location:	11200 Lakeview Avenue
Applicant:	Chris Crowder, KV Engineering	Project Type:	Final Plat
Staff Planner:	Kimberly Portillo, AICP	Proposed Use:	Office (data processing)



PROJECT SUMMARY

The applicant requests approval of a one-lot final plat, in conjunction with a staff-review only final plan application, for expansion of a data processing facility. The proposed final plat is a replat of two lots and one tract into one lot. The applicant proposes to construct a 60,340 SF addition to the existing 42,500 SF DataBank building at 11200 Lakeview Avenue. The lot is 7.217 acres and is part of the Southlake Business Park. The development does not include any new construction of public streets and does not include any new dedications of easements to the City. A deviation for fence height and request for deferred parking were approved with the preliminary plan. The proposed final plat is consistent with the approved preliminary plan/plat (PL22-19P), which was approved by the Governing Body on December 6, 2022. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL WITH A CONDITION

SITE INFORMATION

The site is part of the Southlake Business Park, an approximately 300-acre development, located at the northwest corner of the intersection of 113th Street and Lakeview Avenue.

The site is currently split into three parcels. On the northernmost parcel exists a 42,500 square foot building located along the Lakeview Avenue frontage with parking to the west. The existing building houses a data processing use.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
7.2	102,840	BP-2	Business Park



Exhibit 1: Aerial Image of Subject Site

LAND USE REVIEW

The proposed use for this site is a data processing facility, which falls under the office use designation in the Unified Development Code. The proposed use is permitted in the BP-2, Planned Manufacturing District. A final plan is also being reviewed for the site. The final plan is eligible for administrative review and does not require review by the Planning Commission or Governing Body.

The designated Future Land Use for the site is Business Park, which is defined as: *Developments providing space for uses that may include light assembly and manufacturing or warehousing and distribution. Settings may range from campus-like business parks to single-use buildings.* The site is part of the 300-acre Southlake Business Park, which includes surrounding uses such as medical office space, general office space, and additional data processing. The proposed use is consistent with the Comprehensive Plan, the preliminary plan, and surrounding uses.

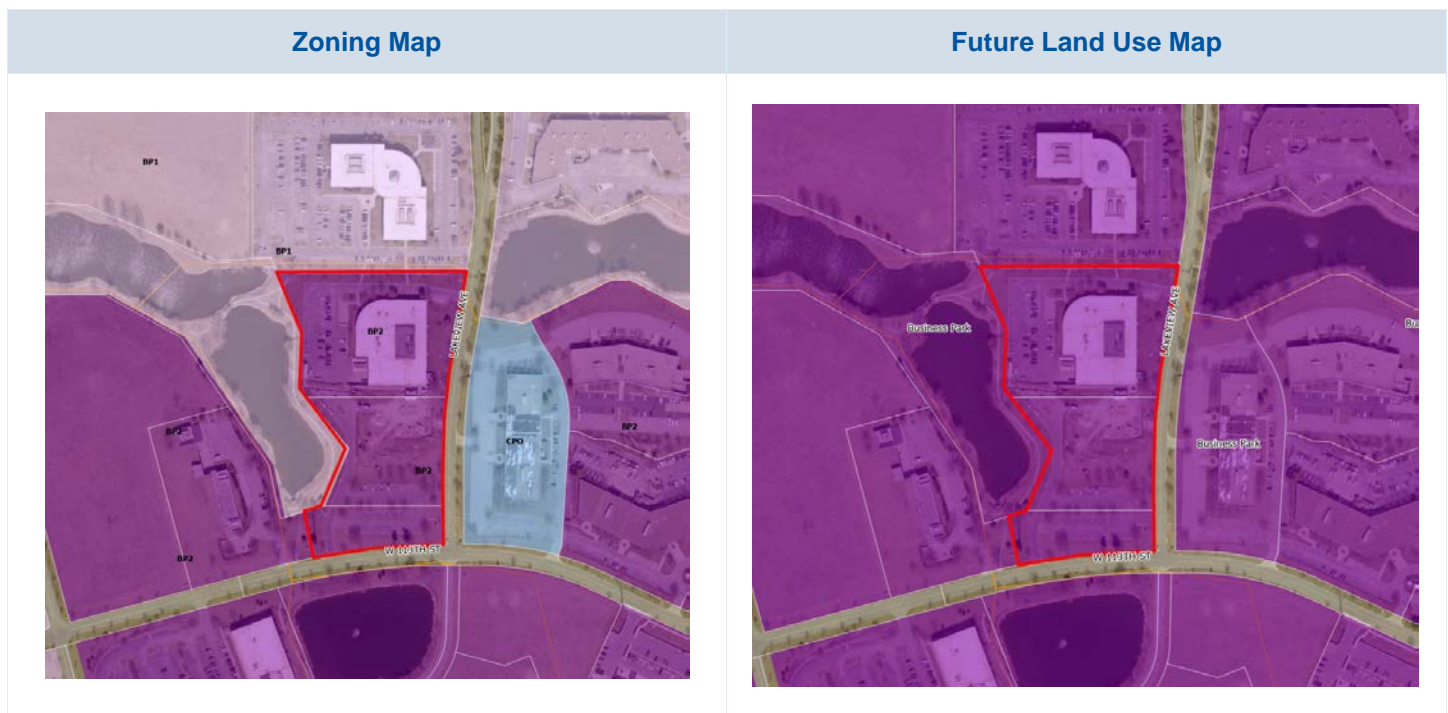


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Business Park	BP-2 Planned Manufacturing	Office (data processing)
North	Business Park	BP-1 Planned Business Park	Office
South	Business Park	BP-2 Planned Manufacturing	Pond
East	Business Park	CP-O Planned General Office	Office
West	Business Park	BP-1 Planned Business Park & BP-2 Planned Manufacturing	Pond

FINAL PLAT REVIEW

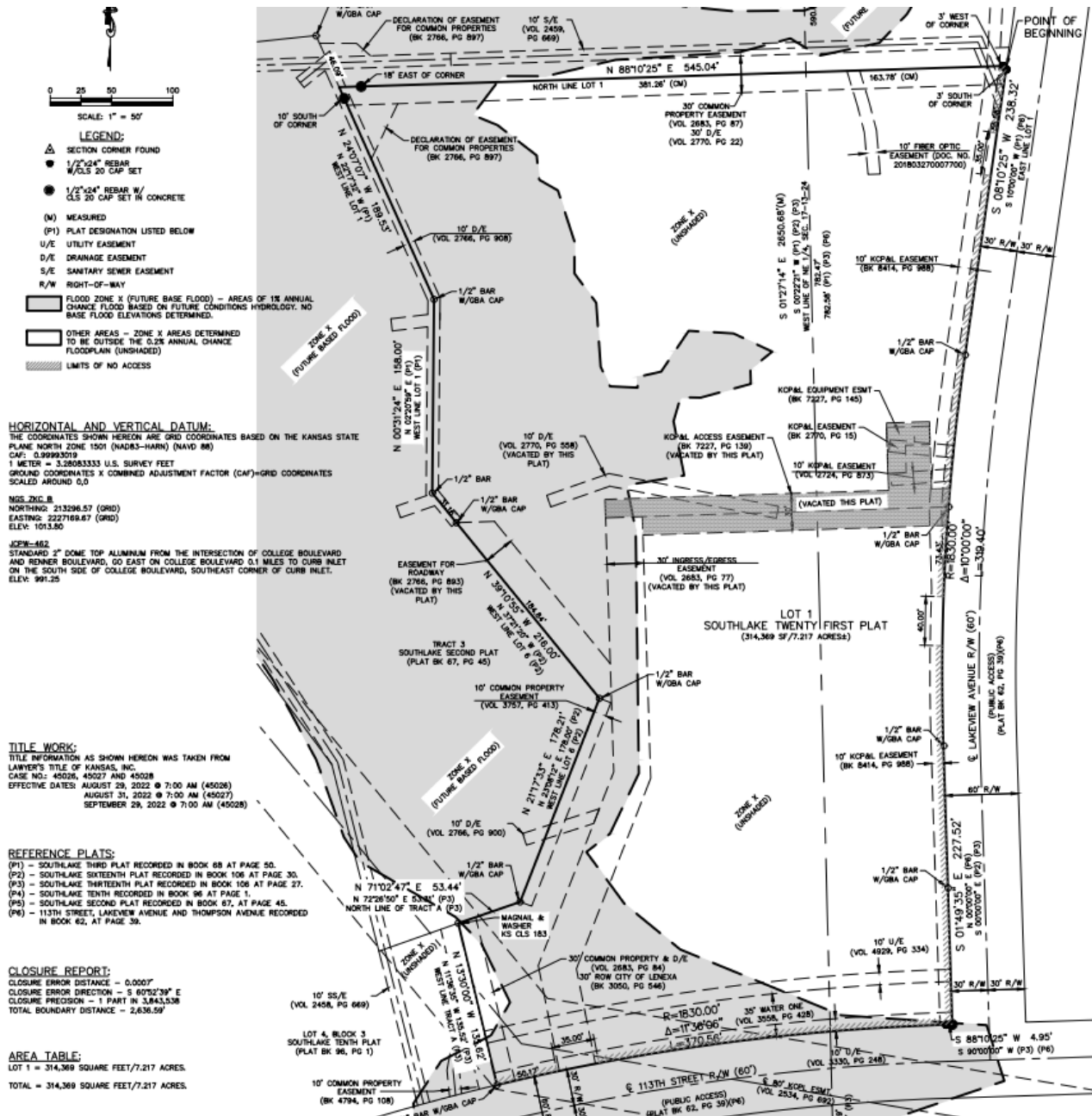


Exhibit 2: Final Plat

The final plat is a replat of Lot 1, Block 3 of Southlake Third Plat, Lot 6, Block 3 of Southlake Sixteenth Plat, and Tract A, Block 3 of Southlake Thirteenth Plat. The site is within the Southlake Business Park, which is primarily an office development, located at 11200 Lakeview Avenue at the intersection of Lakeview Avenue and W. 113th Street. The proposed lot is 7.217 acres and has frontage along Lakeview Avenue and W. 113th Street, which is designated as a local collector. There is one access point at the northeast corner of the lot along Lakeview Avenue with the remaining length of the property line marked for no access. A secondary access point is provided at the southwest corner of the lot along W. 113th Street, with the remaining length of property line marked for no access.

Just north of the site is College Boulevard, a major arterial with access to the interstate system. As the primary use of the building will be to house computer equipment it is not anticipated that a significant amount of traffic will be generated from the expansion of the data facility. The road network is sufficient to handle the proposed use. Gas, sewer, water, and power utilities are available to the site through existing infrastructure.

Several existing easements are to remain while no new easements are being dedicated to the City. The final plat shows that several easements will be vacated. Staff notes that the easements shown are private easements dedicated to other private interests and do not need any approval by the City to be vacated or released. The applicant will need to work with the private entities to release the easements and record easement releases separate from the plat. The City cannot vacate or release a private easement. The City also does not vacate public easements by plat because the City's processes do not meet the statutory standard for notice. The plat must be revised accordingly prior to obtaining the Planning Commission Chair's signature for recording.

The proposed final plat is in compliance with the approved preliminary plan and subdivision regulations.

DEVIATIONS

The applicant is not requesting any deviations with the final plat. Deviations for building setback, fence setback, and fence height were approved with the revised final plan (PL20-01FSR) in May 2020. An additional deviation and request for deferred parking were approved under the preliminary plan (PL22-19P) in November 2022.

REVIEW PROCESS

- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

★ Staff recommends approval of the proposed final plat for Southlake Twenty-First Plat.

- This is a replat of two lots and one parcel into one lot for an office (data processing) use.
- This proposal supports the following guiding principles for the City of Lenexa:
 - **Responsible Economic Development.** This plat encourages high-quality private development by allowing an existing business to expand their building footprint and operational capacity.

FINAL PLAT

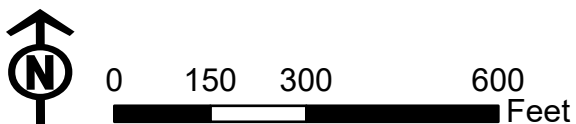
Staff recommends **approval** of the final plat for PT23-04F – **Southlake Twenty-First Plat** at 11200 Lakeview Avenue, for an office/data processing land use, with one condition:

1. The applicant must revise the final plat to remove any vacations noted on the plat prior to obtaining the Planning Commission Chair's signature for recording. The applicant must work directly with the private entities represented to release the easements and record easement releases. No easements will be vacated by approval of this final plat.



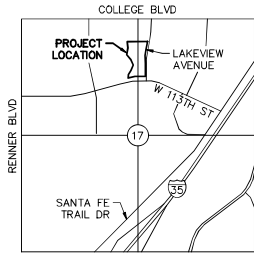
Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Southlake Twenty First Plat PT23-04F

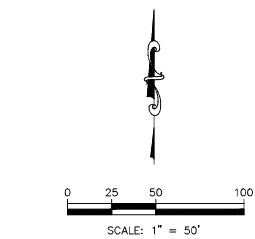


FINAL PLAT

SOUTHLAKE TWENTY FIRST PLAT
A REPLAT OF LOT 1, BLOCK 3, SOUTHLAKE THIRD PLAT,
A REPLAT OF LOT 6, BLOCK 3, SOUTHLAKE SIXTEENTH PLAT AND
A REPLAT OF TRACT A, BLOCK 3, SOUTHLAKE THIRTEENTH PLAT,
PART OF THE NORTHWEST QUARTER & NORTHEAST QUARTER OF
SECTION 17, TOWNSHIP 13 SOUTH, RANGE 24 EAST
LENEXA, JOHNSON COUNTY, KANSAS



VICINITY MAP
SEC 17 - TWP 13 S - RNG 24 E
1" = 2000'



- LEGEND:**
- SECTION CORNER FOUND
 - 1/2"x24" REBAR W/CLS 20 CAP SET
 - 1/2"x24" REBAR W/CLS 20 CAP SET IN CONCRETE
 - MEASURED
 - PLAT DESIGNATION LISTED BELOW
 - UTILITY EASEMENT
 - DRAINAGE EASEMENT
 - SANITARY SEWER EASEMENT
 - RIGHT-OF-WAY
 - FLOOD ZONE X (FUTURE BASE FLOOD) - AREAS OF 1% ANNUAL CHANCE FLOOD BASED ON FUTURE CONDITIONS HYDROLOGY. NO BASE FLOOD ELEVATIONS DETERMINED.
 - OTHER AREAS - ZONE X AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN (UNSHADED)
 - LIMITS OF NO ACCESS

HORIZONTAL AND VERTICAL DATUM:

THE COORDINATES SHOWN HEREON ARE GRID COORDINATES BASED ON THE KANSAS STATE PLANE NORTH ZONE 1501 (NAD83-HARN) (NAVD 88)
CAF: 0.99993019
1 METER = 3.28083333 U.S. SURVEY FEET
GROUND COORDINATES X COMBINED ADJUSTMENT FACTOR (CAF)=GRID COORDINATES SCALED AROUND 0,0

NGS ZKC.B
NORTHING: 213296.57 (GRID)
EASTING: 2227169.67 (GRID)
ELEV: 1013.80

JCPW-462
STANDARD 2" DOME TOP ALUMINUM FROM THE INTERSECTION OF COLLEGE BOULEVARD AND RENNER BOULEVARD, GO EAST ON COLLEGE BOULEVARD 0.1 MILES TO CURB INLET ON THE SOUTH SIDE OF COLLEGE BOULEVARD, SOUTHEAST CORNER OF CURB INLET.
ELEV: 991.25

TITLE WORK:

TITLE INFORMATION AS SHOWN HEREON WAS TAKEN FROM
LAWYER'S TITLE OF KANSAS, INC.
CASE NO.: 45026, 45027 AND 45028
EFFECTIVE DATES: AUGUST 29, 2022 @ 7:00 AM (45026)
AUGUST 31, 2022 @ 7:00 AM (45027)
SEPTEMBER 29, 2022 @ 7:00 AM (45028)

REFERENCE PLATS:

- (P1) - SOUTHLAKE THIRD PLAT RECORDED IN BOOK 68 AT PAGE 50.
- (P2) - SOUTHLAKE SIXTEENTH PLAT RECORDED IN BOOK 106 AT PAGE 30.
- (P3) - SOUTHLAKE THIRTEENTH PLAT RECORDED IN BOOK 106 AT PAGE 27.
- (P4) - SOUTHLAKE TENTH RECORDED IN BOOK 96 AT PAGE 1.
- (P5) - SOUTHLAKE SECOND PLAT RECORDED IN BOOK 67, AT PAGE 45.
- (P6) - 113TH STREET, LAKEVIEW AVENUE AND THOMPSON AVENUE RECORDED IN BOOK 62, AT PAGE 39.

CLOSURE REPORT:

CLOSURE ERROR DISTANCE - 0.0000"
CLOSURE ERROR DIRECTION - S 60°52'39" E
CLOSURE PRECISION - 1 PART IN 3,843,538
TOTAL BOUNDARY DISTANCE - 2,636.59'

AREA TABLE:

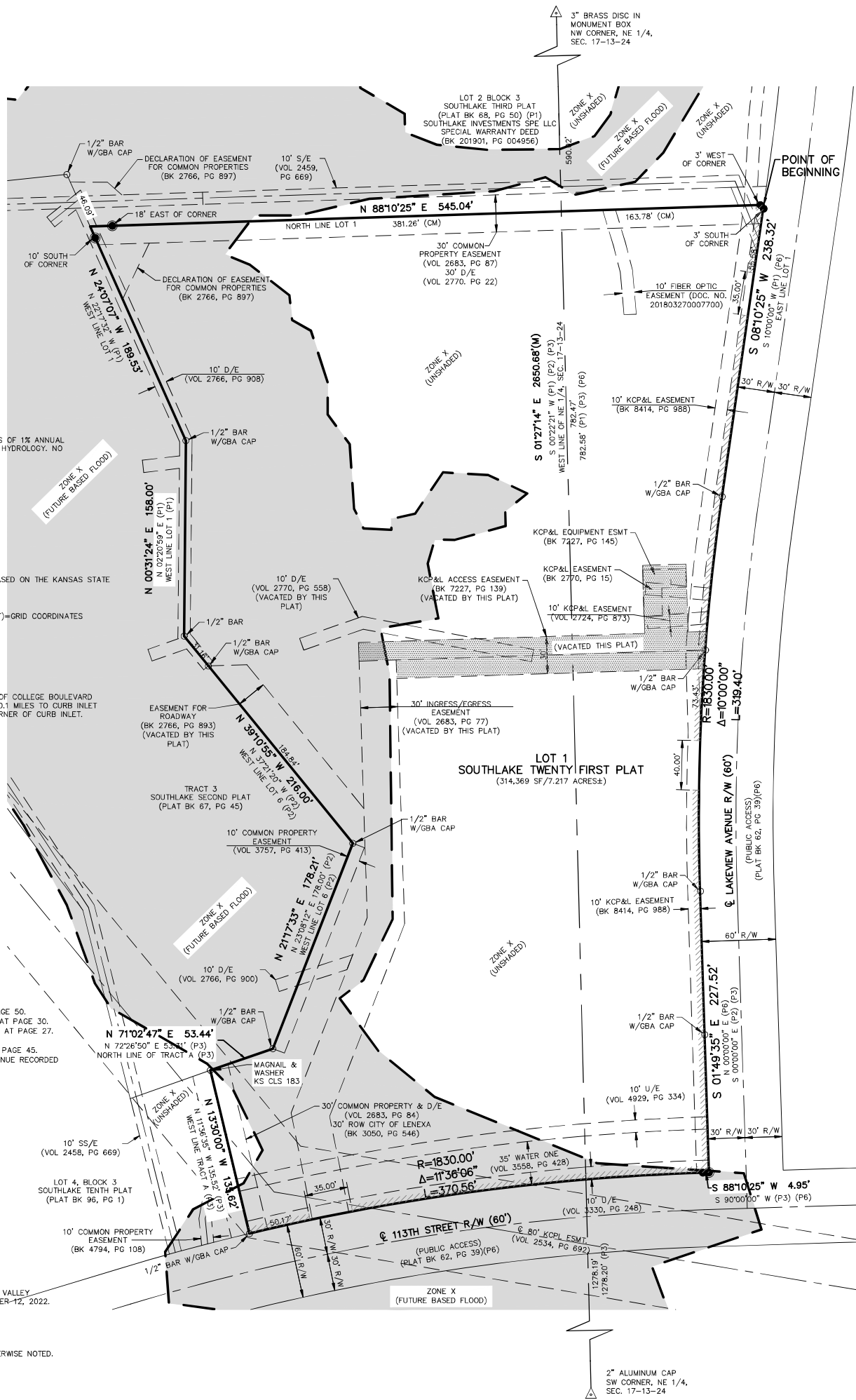
LOT 1 = 314,369 SQUARE FEET/7.217 ACRES.
TOTAL = 314,369 SQUARE FEET/7.217 ACRES.

REFERENCE SURVEYS:

- 1. TOPOGRAPHIC AND BOUNDARY SURVEY PERFORMED BY KAW VALLEY ENGINEERING PROJECT NUMBER C18S9730-1, DATED OCTOBER-12, 2022.

NOTES:

- 1. ALL COORDINATES ARE GROUND COORDINATES UNLESS OTHERWISE NOTED.



DESCRIPTION:

ALL OF LOT 1, BLOCK 3, SOUTHLAKE THIRD PLAT, A RECORDED SUBDIVISION FILED IN PLAT BOOK 68 AT PAGE 50, AND ALL OF LOT 6, BLOCK 3, SOUTHLAKE SIXTEENTH PLAT, A RECORDED SUBDIVISION FILED IN PLAT BOOK 106 AT PAGE 30, AND ALL OF TRACT A, BLOCK 3, SOUTHLAKE THIRTEENTH PLAT, A RECORDED SUBDIVISION FILED IN BOOK 106 AT PAGE 27, ALL AS FILED IN THE JOHNSON COUNTY REGISTER OF DEEDS OFFICE, LYING IN THE NORTHWEST QUARTER AND NORTHEAST QUARTER OF SECTION 17, TOWNSHIP 13 SOUTH, RANGE 24 EAST, OF THE SIXTH PRINCIPLE MERIDIAN IN THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS, DESCRIBED BY KENNETH J. DEDRICK, PS-1067, ON THIS 5TH DAY OF DECEMBER 2022 AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF SAID LOT 1, BEING ON THE WEST RIGHT OF WAY LINE OF LAKEVIEW AVENUE AS NOW ESTABLISHED; THENCE SOUTH 08°10'25" WEST (THIS AND ALL THE FOLLOWING BEARINGS ARE BASED ON THE KANSAS STATE PLANE COORDINATE SYSTEM 1983, NORTH ZONE) ON SAID WEST RIGHT OF WAY LINE, A DISTANCE OF 238.32 FEET TO A POINT OF CURVATURE MARKED BY A 1/2" REBAR WITH GBA CAP; THENCE CONTINUING ON SAID WEST LINE SOUTHWESTERLY ON A CURVE TO THE LEFT TANGENT TO THE LAST DESCRIBED COURSE, HAVING A RADIUS OF 1830.00 FEET, A DELTA ANGLE OF 10°00'00" AND AN ARC LENGTH OF 319.40 FEET TO A POINT BEING MARKED BY A 1/2" REBAR WITH GBA CAP; THENCE CONTINUING ON SAID WEST LINE SOUTH 01°49'35" EAST, A DISTANCE OF 227.52 FEET TO A POINT ON THE NORTH RIGHT OF WAY LINE OF 113TH STREET AS NOW ESTABLISHED BEING MARKED BY A KVE 1/2" REBAR WITH A CLS-20 CAP; THENCE SOUTH 88°10'25" WEST ON SAID NORTH LINE, A DISTANCE OF 4.95 FEET TO A POINT OF CURVATURE, THENCE CONTINUING SOUTHWESTERLY ON A TANGENT CURVE TO THE LEFT ON SAID NORTH LINE, HAVING A RADIUS OF 1830.00 FEET, A DELTA ANGLE OF 11°36'06" AND AN ARC LENGTH OF 370.56 FEET TO THE SOUTHWEST CORNER OF SAID TRACT A MARKED BY A 1/2" REBAR WITH GBA CAP; THENCE NORTH 13°30'00" WEST ON THE WEST LINE OF SAID TRACT A, A DISTANCE OF 135.62 FEET TO THE NORTHWEST CORNER OF SAID TRACT A MARKED BY A MAGNAIL AND WASHER KS CLS 183; THENCE NORTH 71°02'47" EAST ON THE NORTH LINE OF SAID TRACT A, A DISTANCE OF 53.44 FEET TO THE SOUTHWEST CORNER OF SAID LOT 6 MARKED BY A 1/2" REBAR WITH GBA CAP; THENCE NORTH 21°17'33" EAST ON WEST LINE OF SAID LOT 6, A DISTANCE OF 178.21 FEET TO AN ANGLE POINT IN THE WEST LINE OF SAID LOT 6 MARKED BY A 1/2" REBAR WITH GBA CAP; THENCE NORTH 39°10'55" WEST ON THE WEST LINE OF SAID LOT 6 AND WEST LINE OF SAID LOT 1, A DISTANCE OF 216.00 FEET TO AN ANGLE POINT IN THE WEST LINE OF SAID LOT 1 MARKED BY A 1/2" REBAR; THENCE NORTH 00°31'24" EAST ON THE WEST LINE OF SAID LOT 1, A DISTANCE OF 189.53 FEET TO THE NORTHWEST CORNER OF SAID LOT 1; THENCE NORTH 88°10'25" EAST ON THE NORTH LINE OF SAID LOT 1, A DISTANCE OF 545.04 FEET TO THE POINT OF BEGINNING, CONTAINING 314,369 SQUARE FEET OR 7.217 ACRES, MORE OR LESS.

OWNERS CERTIFICATION AND DEDICATION:

THE UNDERSIGNED PROPRIETOR OF THE ABOVE DESCRIBED TRACT OF LAND HAS CAUSED THE SAME TO BE SUBDIVIDED IN THE MANNER AS SHOWN ON THE ACCOMPANYING PLAT, WHICH SUBDIVISION AND PLAT SHALL HEREAFTER BE KNOWN AS "SOUTHLAKE TWENTY FIRST PLAT".

ALL DEDICATIONS FROM THE SOUTHLAKE THIRD PLAT, SOUTHLAKE SIXTEENTH PLAT, SOUTHLAKE THIRTEENTH PLAT AND ELSE WISE DEDICATED HEREBY REMAIN AS DEDICATED ON PREVIOUS PLATS. EXCEPT THOSE AS NOTED "VACATED BY THIS PLAT" AS SHOWN HEREON AND AS NOTED BELOW UNDER K.S.A. 12-512B.

IN TESTIMONY WHEREOF, THE UNDERSIGNED PROPRIETOR HAS HEREUNTO SUBSCRIBED ITS HAND.

OWNER:

(NAME OF OWNER _____)

DANIEL YAMAGISHI, GENERAL COUNSEL

STATE OF COLORADO)
COUNTY OF DENVER)SS:

ON THIS _____ DAY OF _____, 20____, BY DANIEL YAMAGISHI, GENERAL COUNSEL.

NOTARY PUBLIC

MY COMMISSION EXPIRES _____

APPROVED BY THE PLANNING COMMISSION OF THE CITY OF LENEXA, JOHNSON COUNTY, KANSAS, THIS _____ DAY OF _____, 20____

CHRIS POSS, CHAIRMAN

FLOOD STATEMENT:

A PORTION OF THIS PROPERTY LIES WITHIN FEMA FLOOD ZONE DESIGNATED ZONE X (FUTURE BASE FLOOD), DEFINED AS AREAS OF 1% ANNUAL CHANCE FLOOD BASED ON FUTURE CONDITIONS HYDROLOGY. NO BASE FLOOD ELEVATIONS DETERMINED.

THE REMAINDER OF THE PROPERTY LIES WITHIN FEMA FLOOD ZONE DESIGNATED ZONE X (UNSHADED), DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS DETERMINED BY FEMA FLOOD INSURANCE RATE MAP NUMBER 20091C0065G, MAP REVISED AUGUST 3, 2009, JOHNSON COUNTY, KANSAS. LOCATION DETERMINED BY A SCALED GRAPHICAL PLOT OF THE FLOOD INSURANCE RATE MAP.

SURVEYOR'S CERTIFICATION:

I KENNETH J. DEDRICK, BEING A DULY REGISTERED AND LICENSED LAND SURVEYOR IN THE STATE OF KANSAS, HEREBY CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT WAS BASED WAS MADE BY ME OR UNDER MY DIRECT SUPERVISION, AND MEETS OR EXCEEDS THE CURRENT KANSAS MINIMUM STANDARDS FOR BOUNDARY SURVEYS, THAT THE SUBJECT PARCEL, THAT THE LINES OF POSSESSION ARE DEPICTED HEREON, THE COURSES AND DISTANCES SHOWN HEREON ARE THOSE MEASURED ON THE DATE OF THE SURVEY AND THAT THE SURVEY WAS COMPLETED IN THE FIELD AND ON THE GROUND AND MAY BE RELIED UPON BY THE PARTIES CERTIFIED AS TO BEING CORRECT TO THE BEST OF MY BELIEF AND KNOWLEDGE.

THE FIELD WORK WAS COMPLETED ON JULY 29, 2022.

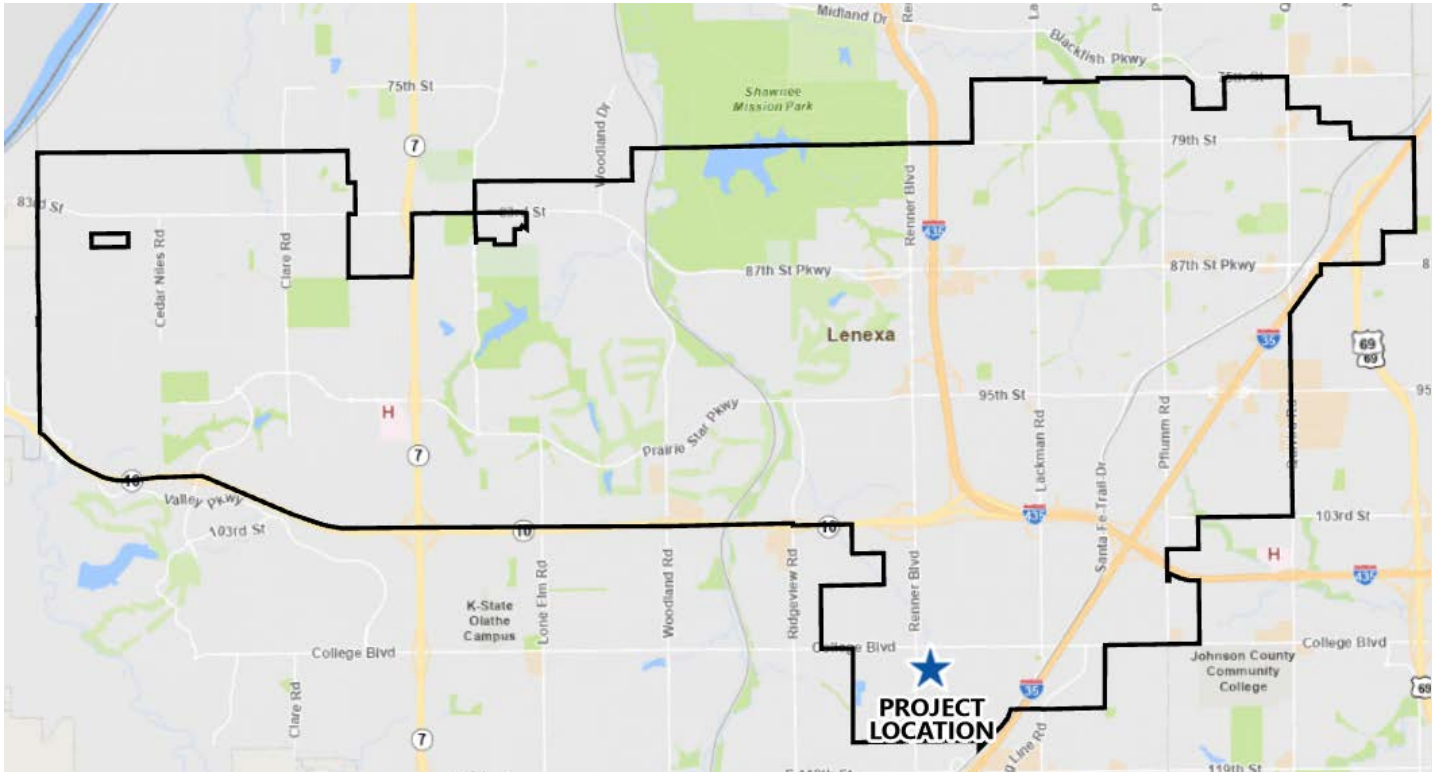
DATE OF PLAT OR MAP: DECEMBER 27, 2022.

K.S.A. 12-512B VACATION OF PLATS AND OTHER PUBLIC RESERVATIONS; RECORDING PLAT OR REPLAT; REVERSION. IN ANY AREA WHERE THERE IS A PLANNING COMMISSION WHICH HAS ADOPTED SUBDIVISION REGULATIONS GOVERNING THE PLATTING OR REPLATTING OF LAND, AND THE REGULATIONS PROVIDE FOR THE GIVING OF APPROPRIATE NOTICE TO ALL PERSONS HAVING PROPERTY RIGHTS OR INTERESTS AFFECTED BY THE PLATTING OR REPLATTING, ANY PLAT OR PART THEREOF OR STREET, ALLEY OR OTHER PUBLIC RESERVATION, INCLUDING, WITHOUT LIMITATION, EASEMENTS, DEDICATED BUILDING SETBACK LINES, AND ACCESS CONTROL, WHETHER ESTABLISHED BY INSTRUMENT, CONDEMNATION OR EARLIER PLATS, SHALL BE VACATED BOTH AS TO USE AND AS TO TITLE WITHOUT ANY FURTHER PROCEEDINGS UPON THE FILING AND RECORDING IN ACCORDANCE WITH THE PROVISIONS OF K.S.A. 12-403, AND AMENDMENTS THERETO, OF ANY PLAT OR REPLAT DULY EXECUTED IN ACCORDANCE WITH LAW AND EMBRACING THE SAME LANDS AS THOSE THERETOFORE EMBRACED BY THE EARLIER PLAT, OR PART THEREOF OR STREET, ALLEY OR OTHER PUBLIC RESERVATION, STREETS, ALLEYS OR OTHER PUBLIC RESERVATIONS WHICH MAY BE VACATED SHALL REVERT, AS PROVIDED IN K.S.A. 12-506, AND AMENDMENTS THERETO.

	14700 WEST 114TH TERRACE LENEXA, KANSAS 66215 PH. (913) 894-5150 FAX (913) 894-5977 lx@kven.com www.kven.com	PROJECT NO. C18S9730-1 DRAWN BY JDK CHECKED BY JDK SHEET 1 OF 1
	PROJECT: DATABANK 11200 LAKEVIEW AVE LENEXA, KANSAS, 66219	PREPARED FOR: BELL/KNOTT AND ASSOCIATES 12730 STATE LINE RD, SUITE 100 LEAWOOD, KANSAS 66209
KAW VALLEY ENGINEERING, INC., IS AUTHORIZED TO OFFER SURVEYING SERVICES BY KANSAS STATE CERTIFICATE OF AUTHORIZATION NO. LS-20, EXPIRES 12/31/22		

LENEXA LOGISTICS CENTRE, 8TH PLAT

Project #:	PT23-01F	Location:	NW corner of 113 th St. & Renner Blvd.
Applicant:	Kale Odom, ARCO National Holdings	Project Type:	Final Plat
Staff Planner:	Kimberly Portillo, AICP	Proposed Use:	Business Park



PROJECT SUMMARY

The proposed plat is the eighth plat for Lenexa Logistics Centre, an industrial development. This application is in conjunction with a staff review only final plan for Standard Beverage, PL23-01FS, for development of a new building with 30,000 SF of office space and 50,000 SF of warehouse space for a beverage manufacturing and distribution company in the BP-2, Planned Manufacturing District. The applicant proposes a one-lot final plat for an office and warehouse/distribution use at the northwest corner of 113th Street and Renner Boulevard. The lot is 5.96 acres and has access from a private drive off 113th Street. The development does not include any new construction of public streets and does not include any new dedications of easements to the City. Right-of-way for 113th Street and sidewalks on 113th and Renner Boulevard have been previously dedicated. The proposed final plat is consistent with the approved preliminary plan (PL16-03PR), which was approved by the Governing Body on February 16, 2016. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The subject property was part of a larger rezoning and preliminary plan for Lenexa Logistics Centre in 2013. At that time, RZ13-02 rezoned the property from AG, Agricultural Zoning District, to BP-2, Planned Business Park District, and PL13-02P provided a companion preliminary plan for the development of ten buildings on a 120-acre site. Several of the sites within Lenexa Logistics Centre have since had approved final plats and plans and have been developed.

In 2016, the subject property was included as part of a revised preliminary plan application, PL16-03PR, to grant setback deviations for a different lot in the overall development. The subject property was not modified from the 2013 preliminary plan approval with the revised preliminary plan.

A one-lot final plat, called Lenexa Logistics Centre, Seventh Plat, was approved for this site by the Planning Commission on October 5, 2020 (PT20-21F). The proposed use at that time was for a warehousing development. The final plat was never recorded and has since expired. Although there have been no significant changes, due to the expiration of the plat a re-approval by the Planning Commission is required. This plat was renamed as the eighth plat because a different lot in the development has already been platted as the seventh plat.

Access to the lot is established via a 15-foot shared access easement off 113th Street on the west property line and an access point along 113th Street. Right-of-way for 113th Street and sidewalk easements on 113th Street and Renner Boulevard were previously dedicated.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
5.96	N/A	BP-2	Business Park



Exhibit 1: Aerial Image of Subject Site

LAND USE REVIEW

The proposed use for this site is a headquarters office and warehousing/distribution, all of which are permitted uses in the BP-2, Planned Manufacturing District. A final plan is also being reviewed for the site. The final plan is eligible for administrative review and does not require review by the Planning Commission or Governing Body.

The designated Future Land Use for the site is Business Park, which is defined as: *Developments providing space for uses that may include light assembly and manufacturing or warehousing and distribution. Settings may range from campus-like business parks to single-use buildings.* The site is part of the larger warehousing and distribution development known as Lenexa Logistics Centre. Surrounding uses include both warehouses and offices. The proposed use is consistent with the Comprehensive Plan, the preliminary plat, and surrounding uses.

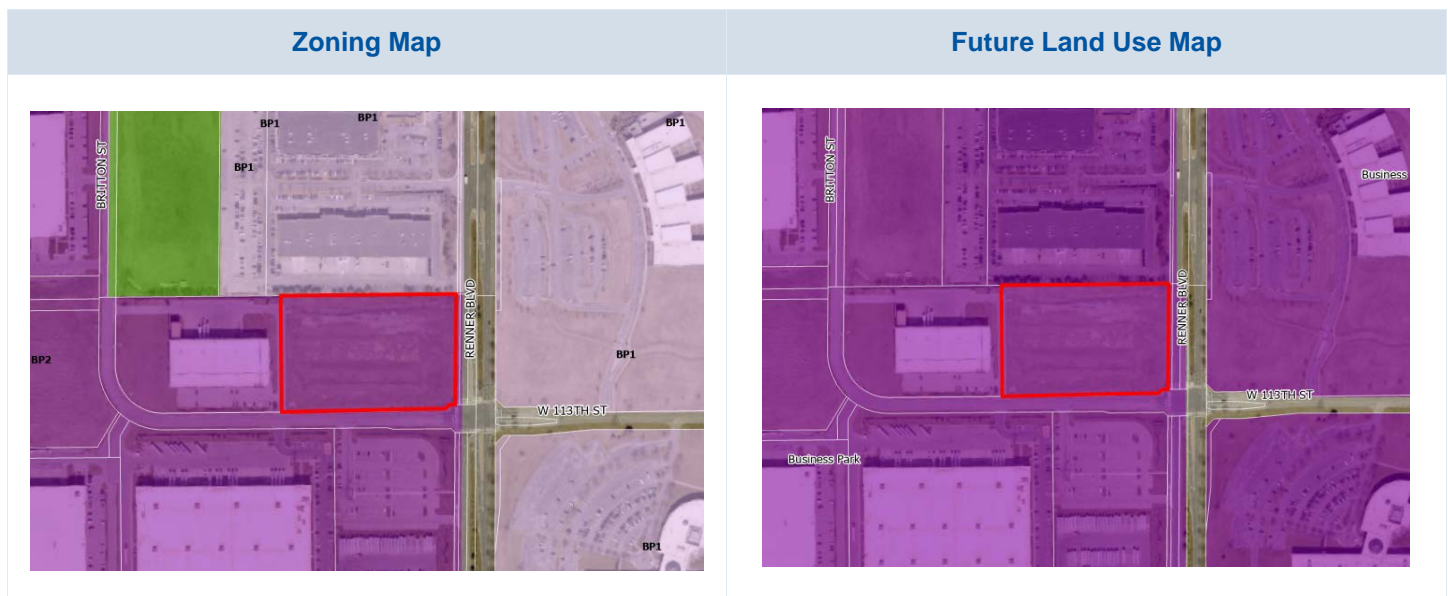


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Zoning	Land Use Designation ^[SK1]	Current Use
Subject Property	BP-2 Planned Industrial District	Business Park	Undeveloped
North	BP-1 Planned Business Park	Business Park	Warehouse/ Office
South	BP-2 Planned Industrial District	Business Park	Warehouse/ Distribution & Parking Lot (uncovered)
East	BP-1 Planned Business Park	Business Park	General Office Buildings
West	BP-2 Planned Industrial District	Business Park	Warehouse/ Distribution

FINAL PLAT REVIEW

The final plat is for one lot on 5.96 acres located in the Lenexa Logistics Centre warehousing development at the northwest corner of Renner Boulevard and W. 113th Street. Renner Boulevard is designated as a major arterial with close access to the interstate system via College Boulevard, another major arterial. The road network is sufficient to handle the proposed use. There is an existing sidewalk network along Renner Boulevard and W. 113th Street and the plat shows existing sidewalk easements. Gas, sewer, water, and power utilities are available to the site through existing infrastructure. The proposed final plat is in compliance with the approved preliminary plat and subdivision regulations.

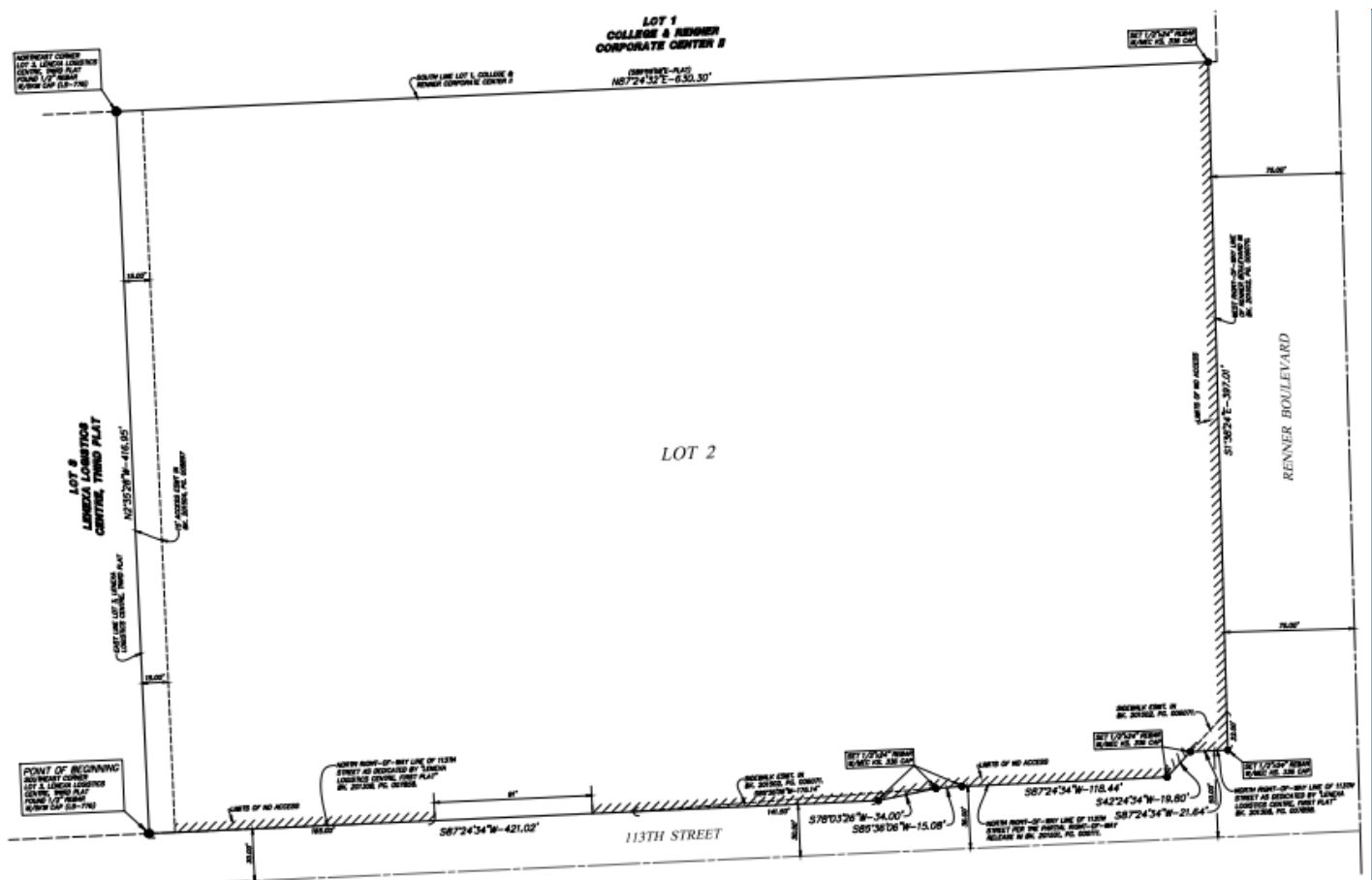


Exhibit 2: Proposed Final Plat PT23-01F.

There are no new easements or rights-of-way are proposed to be dedicated to the City with the plat. The plat shows an existing 15-foot access easement on the westernmost portion of the plat, which represents the shared drive with Lenexa Logistics Centre, Third Plat. The shared drive will serve as one of two access points for future development, with the second access being a 91-foot wide entrance point from W. 113th Street. The remainder of frontage along W. 113th Street and Renner Boulevard is access-restricted.

DEVIATIONS

The applicant is not requesting any deviations.

REVIEW PROCESS

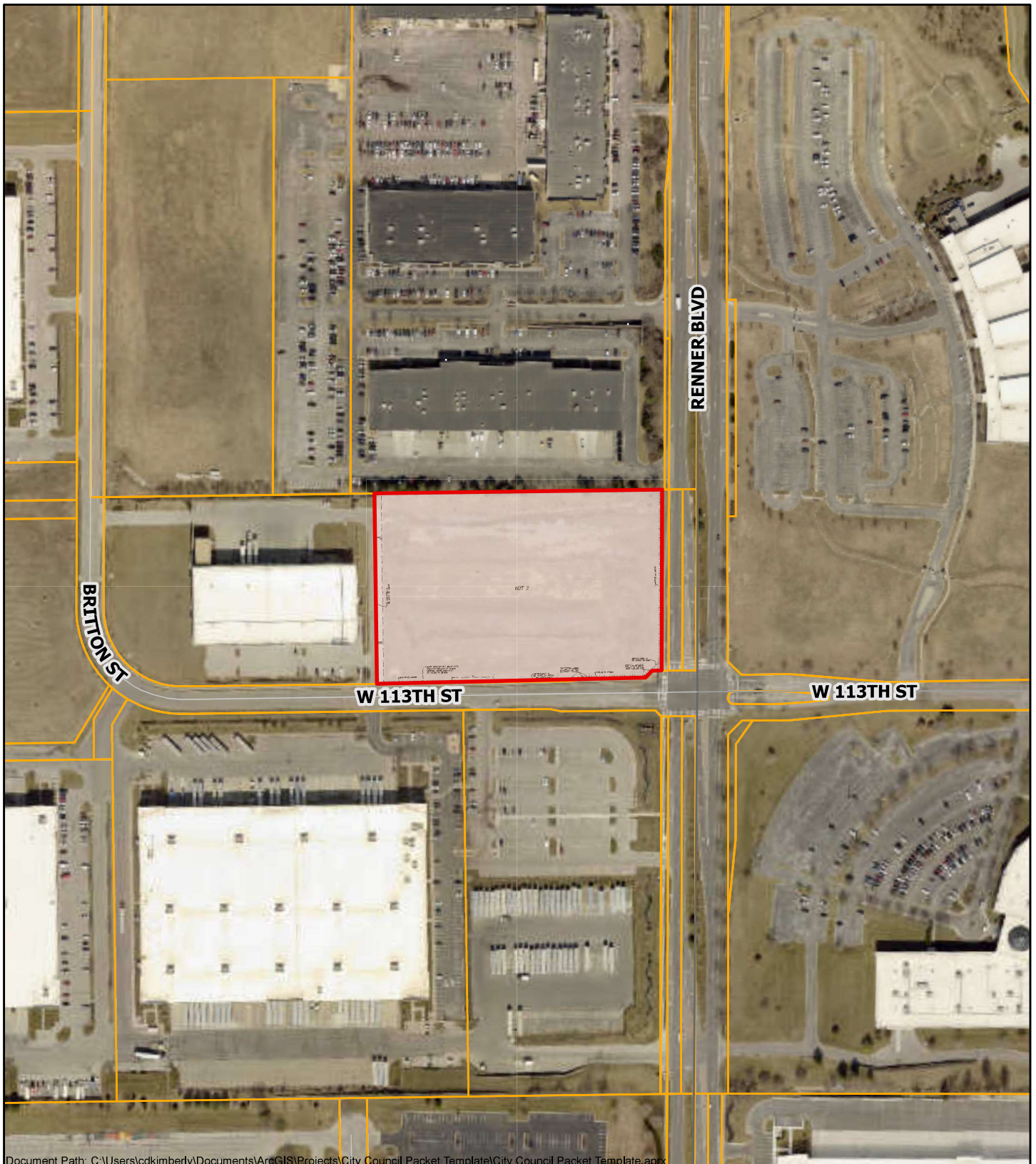
- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

- ★ **Staff recommends approval of the proposed Final Plat for Lenexa Logistics Centre, Eighth Plat.**
 - This is a final plat for one lot with an office and warehouse/distribution use.
 - This proposal supports the following guiding principles for the City of Lenexa:
 - Responsible Economic Development: This plat encourages high-quality private development by supporting development of a headquarters office.

FINAL PLAT

Staff recommends **approval** of the final plat for PT23-01F – **Lenexa Logistics Centre, Eighth Plat** at the northwest corner of 113th Street and Renner Boulevard, for an office and warehouse/distribution land use.



Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

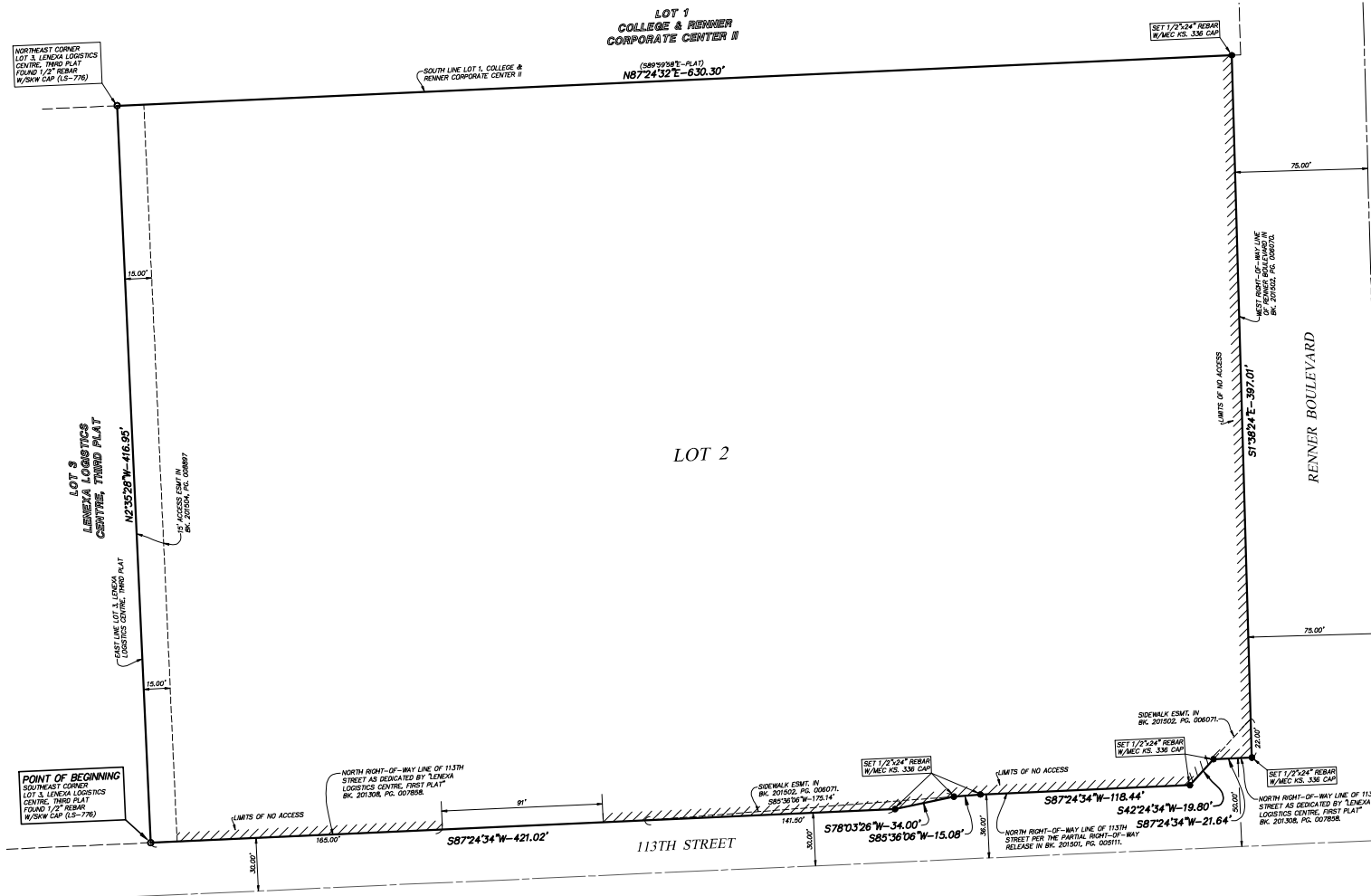
Lenexa Logistics Centre, Eighth Plat PT23-01F



0 150 300 600
Feet



PLAT OF
LENEXA LOGISTICS CENTRE, EIGHTH PLAT
A PART OF THE NE 1/4 OF SECTION 18, TOWNSHIP 13 SOUTH, RANGE 24 EAST
CITY OF LENEXA, JOHNSON COUNTY, KANSAS

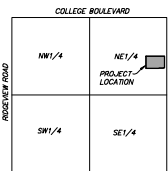


General Notes:

Bearings shown hereon are in accordance with the State Plane Coordinate Systems, Kansas North Zone, NAD83, (GCS=KANSAS2011) and based on the recorded plat of "LENEXA LOGISTICS CENTRE, FIRST PLAT".

Error of closure exceeds 1 in 50,000

This property lies within Flood Zone X, defined as areas determined to be outside the limits of the 0.2% annual chance floodplain, as shown on the Flood Insurance Rate Map prepared by the Federal Emergency Management Agency for the City of Lenexa, Johnson County, Kansas, Community No. 200168, Panel No. 00640 & 00650 and dated August 3, 2009.



VICINITY MAP
SEC. 18-T13S-R24E



SCALE: 1" = 50'

- = SET MONUMENT AS DESCRIBED
- = FOUND MONUMENT AS DESCRIBED

LOT 2	259,861± SQ. FT.	5.966± ACRES
Total	259,861± SQ. FT.	5.966± ACRES

McCLURE

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11031 Strang Line Road
Lenexa, Kansas 66215

(913) 888-7800 www.mcclurevision.com

I hereby certify that the information shown hereon is based on a field survey performed under my supervision in December, 2022.

By: Craig E. Chaney KS PS-1141

Description:

This is a survey and subdivision of all that part of the NE 1/4 of Section 18, Township 13, Range 24, in the City of Lenexa, Johnson County, Kansas, more particularly described as follows:

Beginning at the Southeast corner of Lot 3, LENEXA LOGISTICS CENTRE, THIRD PLAT, a subdivision of land in the City of Lenexa, Johnson County, Kansas; thence N 2° 35' 28" W along the East line of said Lot 3, a distance of 416.95 feet to the Northeast corner of said Lot 3, a solid point also being on the South line of Lot 1, COLLEGE & RENNER CORPORATE CENTER II, a subdivision of land in the City of Lenexa, Johnson County, Kansas; thence N 87° 24' 32" E along the South line of said Lot 1, a distance of 630.30 feet to a point on the West right-of-way line of Renner Boulevard, as now established in Book 201502 at Page 006070 as filed in the Office of the Register of Deeds, Johnson County, Kansas; thence S 1° 38' 24" E along said West right-of-way line, a distance of 307.07 feet to a point on the North right-of-way line of 113th Street, as now established by the dedication of LENEXA LOGISTICS CENTRE, FIRST PLAT, a subdivision of land in the City of Lenexa, Johnson County, Kansas; thence S 87° 24' 34" W along said North right-of-way line, a distance of 21.64 feet to a point on the North right-of-way line of said 113th Street, as now established by the Partial Right-of-Way Release as filed in Book 201501 at Page 005111 in the Office of the Register of Deeds, Johnson County, Kansas; thence Westerly, along said North right-of-way line the following courses: S 42° 24' 34" W, a distance of 19.80 feet; thence S 87° 24' 34" W, a distance of 118.44 feet; thence S 85° 36' 06" W, a distance of 15.08 feet; thence S 78° 03' 26" W, a distance of 34.00 feet to a point on the North right-of-way line of said 113th Street as established by said LENEXA LOGISTICS CENTRE, FIRST PLAT; thence S 87° 24' 34" W along said North right-of-way line, a distance of 421.02 feet to the Point of Beginning, containing 259,861 square feet, more or less, equal to 5.966 acres, more or less.

OWNERS CERTIFICATION AND DEDICATION:

The undersigned proprietor of the above described tract of land has caused the same to be subdivided in the manner as shown on the accompanying plat, which subdivision and plat shall hereafter be known as "LENEXA LOGISTICS CENTRE, EIGHTH PLAT".

IN TESTIMONY WHEREOF, the undersigned proprietor has hereunto subscribed his hand.

Owner: Lenexa Logistics S-2 Holdings, LLC,
a Kansas limited liability company.

By: LRico Services, LLC, its Manager

By: Darrell D. Swank
President, LRico Services, LLC

ACKNOWLEDGMENT

STATE OF)
) SS.
COUNTY OF)

This instrument was acknowledged before me on _____ by Darrell D. Swank, President of LRico Services, LLC, Manager of Lenexa Logistics S-2 Holdings, LLC, a Kansas limited liability company.

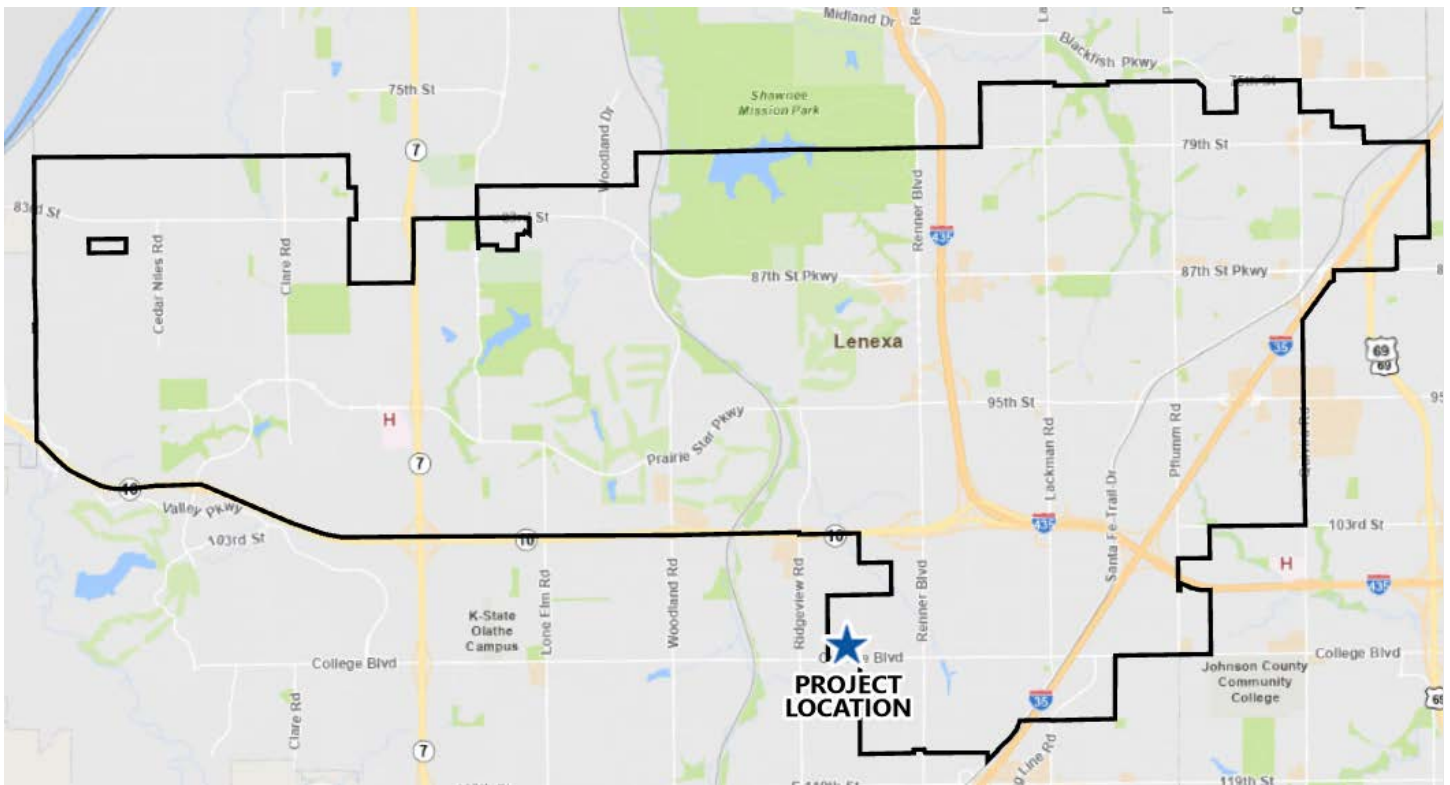
My Appointment expires: _____

APPROVED by the Planning Commission of the City of Lenexa, Johnson County, Kansas, this _____ day of _____, 2023.

Chris Pass, Chairman

LENEXA LOGISTICS CENTRE NORTH, SIXTH PLAT

Project #:	PT23-03F	Location:	The northwest corner of College Boulevard and Mill Creek Road
Applicant:	Curtis Holland, Polsinelli	Project Type:	Final Plat
Staff Planner:	Kimberly Portillo, AICP	Proposed Use:	Industrial



PROJECT SUMMARY

The proposed plat is the sixth plat for Lenexa Logistics Centre North, an industrial development. The applicant proposes a one-lot final plat for future warehousing and distribution use in the BP-2, Planned Manufacturing District. The project site is located at the northwest corner of Mill Creek Road and College Boulevard. The lot is 6.048 acres and has access from a private drive off College Boulevard. The development does not include any new construction of public streets and does not include any new dedications of easements to the City. Right-of-way for College Boulevard has been previously dedicated by Lenexa Logistics Centre North, First Plat. The proposed final plat is consistent with the approved preliminary plan (PL04-02P), which served as a preliminary plat and was approved by the Governing Body on September 2, 2014. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The subject property was part of a larger rezoning and preliminary plan for Lenexa Logistics Centre North in 2014. At that time, RZ14-03 rezoned the property from AG, Agricultural and CP-O, Planned General Office Zoning Districts to CP-2, Planned Manufacturing District and PL14-02P provided a companion preliminary plan for the development of four buildings on an 80-acre site. Two of the four sites within Lenexa Logistics Centre North have since had approved final plats and plans and have been developed.

Access to the lot is established via a shared access private drive with right-in/right-out access to College Boulevard to the west of the lot. A second, shared full access point will be provided from College Boulevard between the subject site and adjacent unplatted parcel to the east.

Stormwater management for the site is consistent with the previously approved business park stormwater report for the overall Lenexa Logistics North business park, within which this site was included.

LAND AREA (AC) 6.048	BUILDING AREA (SF) N/A	CURRENT ZONING BP-2	COMP. PLAN Office & Research Development
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Exhibit 1: Aerial Image of Subject Site

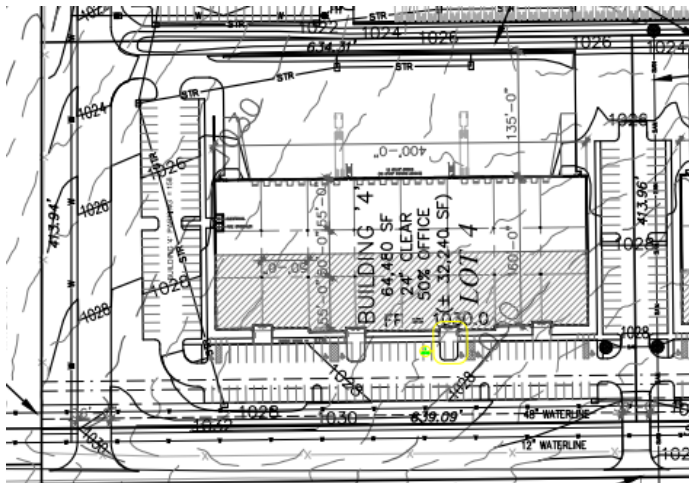


Exhibit 2: View of the subject site from the approved preliminary plan, PL14-02P.

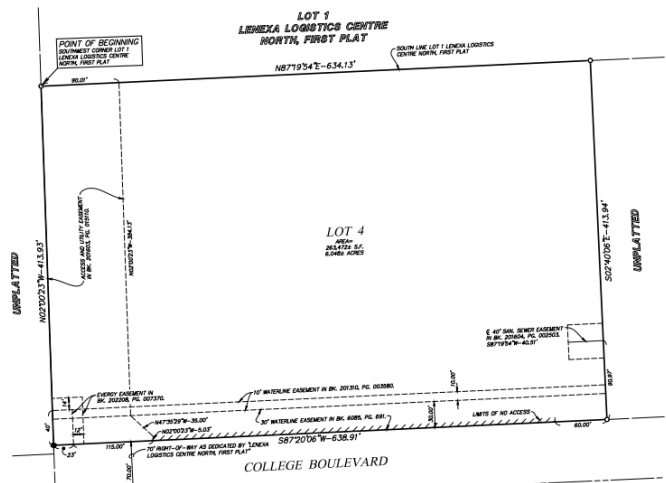


Exhibit 3: Currently submitted final plat PT23-03F.

LAND USE REVIEW

The proposed lot will be part of the Lenexa Logistics Centre North Development, which includes uses such as office, warehousing, and distribution, all of which are allowed uses in the BP-2, Planned Manufacturing District. Review of a final development plan will be required prior to development on the site, at which time further land use review will be conducted.

The designated Future Land Use for the site is Office & Research Development, which is defined as: *The combination of office with research and related manufacturing or assembly with little or no sales of product. Examples include medical research spaces, bioscience, technology, or product development testing. Research uses may vary widely.* A specific use has not been identified for the site. At such time as a final plan is presented, staff will ensure the proposed use is compatible with the approved preliminary plan and the underlying zoning.

Zoning Map



Future Land Use Map



TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Office, Research & Development	BP-2 Planned Manufacturing	Undeveloped
	Office, Research & Development	BP-2 Planned Manufacturing	Warehouse distribution/ storage facility
	City of Olathe	R-2 Two-Family District & R-3 Low-Density Multifamily District, City of Olathe	Residential Duplex
	Office, Research & Development	BP-2 Planned Manufacturing	Undeveloped
	City of Olathe	Planned Mixed-Use, City of Olathe	Undeveloped

FINAL PLAT REVIEW

The final plat is for one lot on 6.048 acres located in the Lenexa Logistics Centre North industrial development. College Boulevard is designated as a major arterial with direct access to the interstate system. The road network is sufficient to handle future development. There is an existing sidewalk network along College Boulevard and Mill Creek Road in the public right-of-way. Gas, sewer, water, and power utilities are available to the site with existing infrastructure.

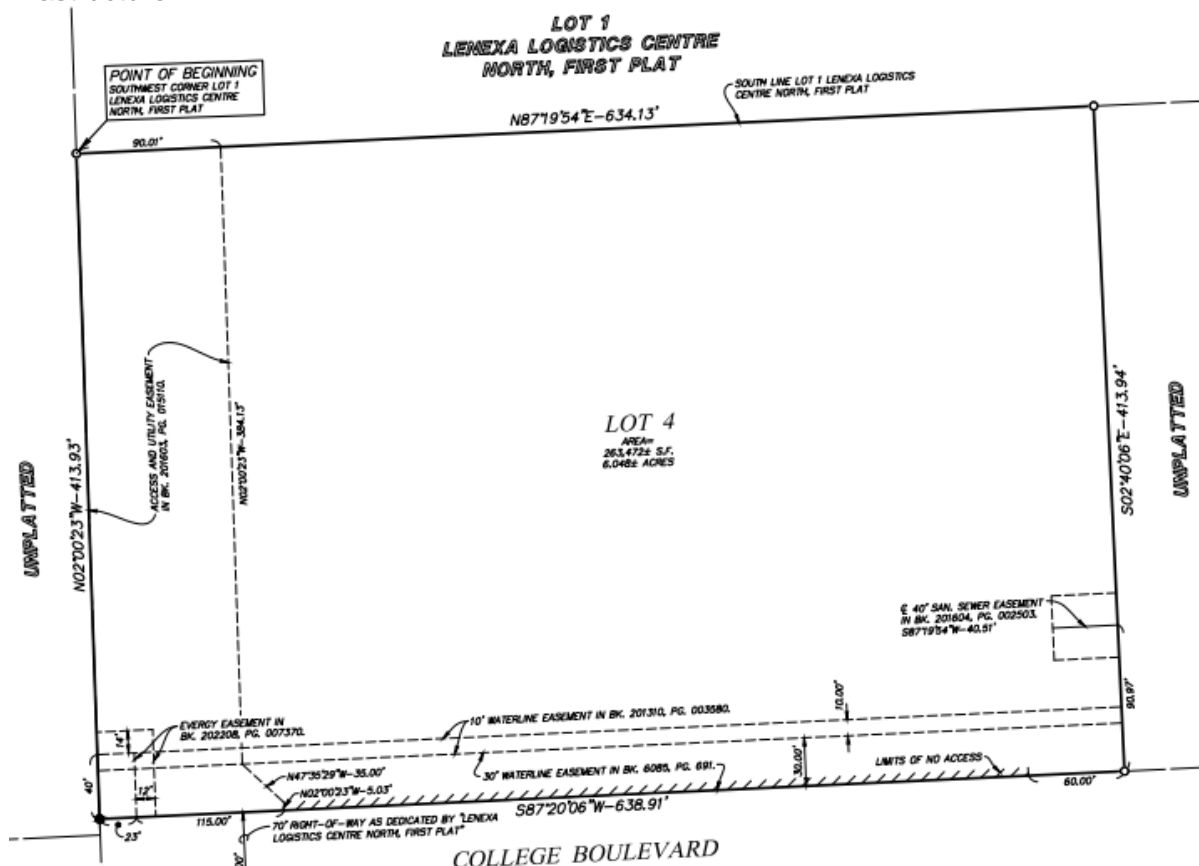


Exhibit 4: Proposed Final Plat PT23-03F

There are no new easements or rights-of-way are proposed to be dedicated to the City with the plat. The plat shows an existing access and utility easement on the west side of the plat, which represents the private drive shared with Lot 1 of Lenexa Logistics Centre North, First Plat. The private drive will serve as one of two access points for future development, with the second access being an entrance at the southeast corner of the plat onto College Boulevard. The remainder of frontage along College Boulevard is access restricted. The access points are consistent with the preliminary plan. Other existing easements shown on the plat include a 10-foot water line easement and 30-foot water line easement on the southernmost portion of the site, a 12-to-14-foot power, easement on the southwest corner of the site, and a 40-foot sanitary sewer easement near the southeast corner of the site.

DEVIATIONS

The applicant is not requesting any deviations.

REVIEW PROCESS

- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

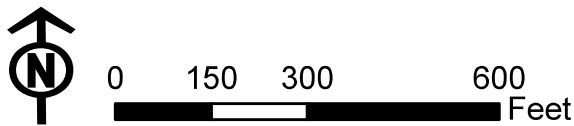
- ★ **Staff recommends approval of the proposed final plat for Lenexa Logistics Centre North, Sixth Plat.**
 - This is a final plat for one lot zoned BP-2, Planned Manufacturing District.
 - This proposal supports the follow guiding principles for the City of Lenexa:
 - Responsible Economic Development: This plat supports future development of the site into a productive use.

FINAL PLAT

Staff recommends **approval** of the final plat for PT23-03F – **Lenexa Logistics Centre North, Sixth Plat** at the northwest corner of College Boulevard and Mill Creek Road in the BP-2, Planned Manufacturing District.



Lenexa Logistics Centre North, 6th Plat
PT23-03F



PLAT OF
LENEXA LOGISTICS CENTRE NORTH 6TH PLAT
A PART OF THE SW 1/4 OF SECTION 07, TOWNSHIP 13 SOUTH, RANGE 24 EAST
CITY OF LENEXA, JOHNSON COUNTY, KANSAS

DESCRIPTION:

All that part of the Southwest Quarter of Section 7, Township 13 South, Range 24 East in the City of Lenexa, Johnson County, Kansas, with such part being originally described on this 9th day of December 2nd, 2022 by me, Craig E. Chaney, Professional Surveyor, Kansas License No. 1141, and with said part being more particularly described as follows:
BEGINNING at the Southwest Corner of Lot 1, LENEXA LOGISTICS CENTRE NORTH, FIRST PLAT, a subdivision of land in the City of Lenexa, Johnson County, Kansas as recorded in Book 201602 at Page 007083 in the Office of the Register of Deeds, Johnson County, Kansas; thence N 87° 19' 54" E, along the South Line of said Lot 1, a distance of 634.13 feet; thence S 02° 40' 06" E, departing said South Line, a distance of 413.94 feet to a point on the North Right-of-Way Line of College Boulevard, as now established by said LENEXA LOGISTICS CENTRE NORTH, FIRST PLAT; thence S 87° 20' 06" W along said North Right-of-Way Line, a distance of 638.91 feet to a point on the Southerly Extension of the West Line of said Lot 1, thence N 2° 00' 23" W, along said Southerly Extension of the West Line of said Lot 1, a distance of 413.94 feet, to the POINT OF BEGINNING, containing 263,472 square feet, more or less, equal to 6.048 acres, more or less.

OWNERS CERTIFICATION AND DEDICATION:

The undersigned proprietor of the above described tract of land has caused the same to be subdivided in the manner as shown on the accompanying plat, which subdivision and plat shall hereafter be known as "LENEXA LOGISTICS CENTRE NORTH 6TH PLAT".

IN TESTIMONY WHEREOF, the undersigned proprietor has hereunto subscribed its hand.

OWNER:

JRD Real Estate VII, LLC, a Delaware Limited Liability Company

By:

Brian Emmert, Treasurer

STATE OF

) SS.

COUNTY OF

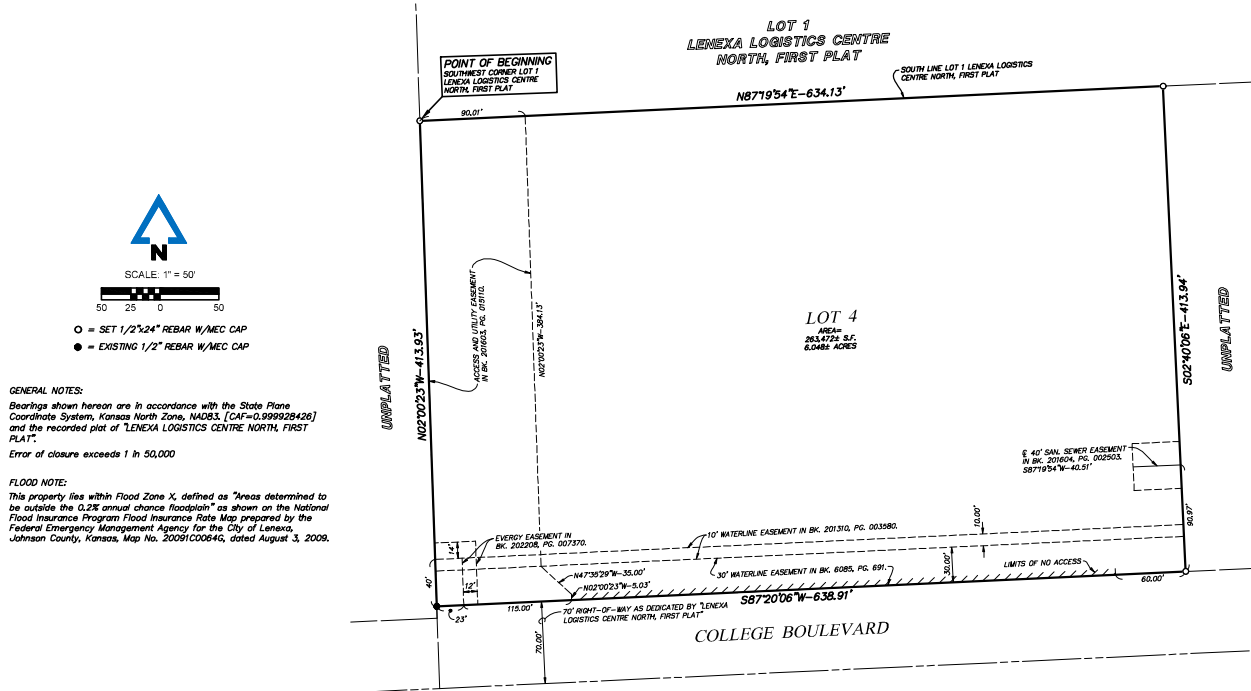
This record was acknowledged before me on _____ by Brian Emmert as Treasurer of JRD Real Estate VII, LLC, a Delaware Limited Liability Company.

Notary Public

My Commission Expires: _____

APPROVED by the Planning Commission of the City of Lenexa, Johnson County, Kansas, this ____ day of _____, 2023.

Chris Poss, Chairman



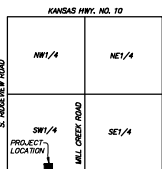
GENERAL NOTES:

Bearings shown hereon are in accordance with the State Plane Coordinate System, Kansas North Zone, NAD83, [CAF=0.999928426] and the recorded plat of "LENEXA LOGISTICS CENTRE NORTH, FIRST PLAT".

Error of closure exceeds 1 in 50,000

FLOOD NOTE:

This property lies within Flood Zone X, defined as "Areas determined to be outside the 0.2% annual chance floodplain" as shown on the National Flood Insurance Program Flood Insurance Rate Map prepared by the Federal Emergency Management Agency for the City of Lenexa, Johnson County, Kansas, Map No. 20091C0064G, dated August 3, 2009.



VICINITY MAP
SEC. 07-T13S-R24E

I hereby certify that the information shown hereon is based on a field survey performed under my supervision in November 2022.

By: Craig E. Chaney KS PS-1141

McCLURE

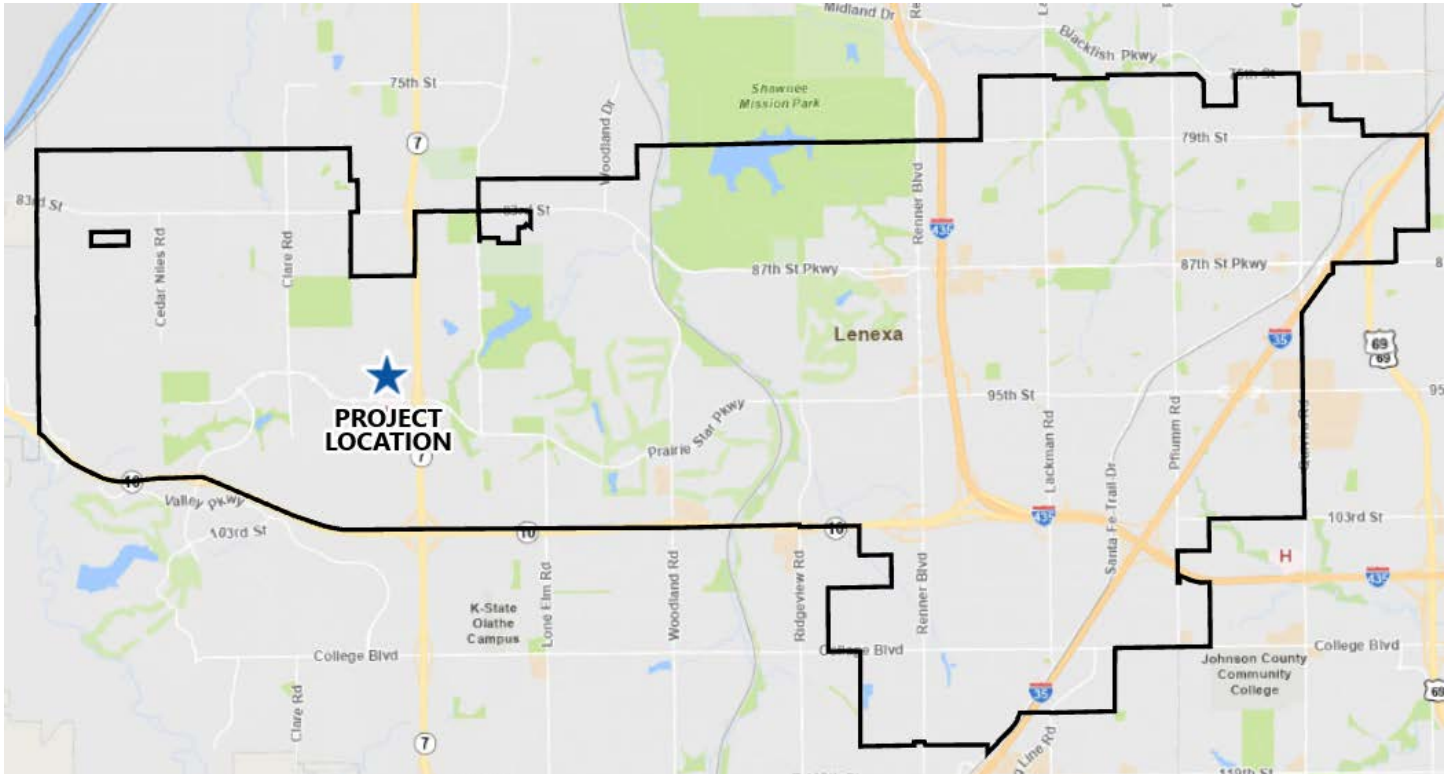
"Your vision. Engineered here."

11031 Strang Line Road
Lenexa, Kansas 66215

(913) 888-7800 www.mcclurevision.com

PRAIRIE STAR MEDICAL OFFICE BUILDING

Project #:	PL23-03F	Location:	9471 Meadow View Drive
Applicant:	Andrea Lemken, PIC	Project Type:	Final Plan
Staff Planner:	Christa McGaha, AICP	Proposed Use:	Medical Services



PROJECT SUMMARY

The applicant proposes to construct a medical office building at the northeast corner of Prairie Star Parkway and Meadow View Drive. The building includes three separate tenant spaces. The structure is one-story measuring 29' 9" tall and totals 11,641 square feet. The applicant requests approval of a final plan. The proposed final plan is consistent with the approved preliminary plan (PL22-07PR), which was approved by the Governing Body on December 5, 2022. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The subject property is located at the northeast corner of Prairie Star Parkway and Meadow View Drive west of the K-7 Highway interchange on Prairie Star Parkway. Last month, in December 2022, the Planning Commission recommended approval of a Revised Preliminary Plan (PL22-07PR) for the development of an 11,641 square foot medical office building in this location.

Previous approvals included a preliminary plan (PL06-19P) for a two-story bank/restaurant in 2006 as a part of the overall preliminary plan for Reserve Plaza. The preliminary plan for Reserve Plaza consisted of eight lots and 10 buildings with a mix of retail, restaurant, gas station, and office land uses, with a proposed detention pond. The subject property was then platted as Lot 8 of Reserve Plaza in 2006.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
1.35	11,641	CP-1	Neighborhood Retail



Exhibit 1: Vicinity Aerial Image of Subject Site



Exhibit 2: Aerial Image with Site Plan.

LAND USE REVIEW

The proposed development is a medical office building, which falls within the “medical services” land use category in the Unified Development Code (UDC). The building will include three tenant spaces for medical office uses. The future tenants have not been identified and are anticipated to be a variety of medical uses such as medical clinics, dentists, and optometrist offices. Medical services are a permitted land use within the CP-1, Planned Neighborhood Commercial Zoning District.

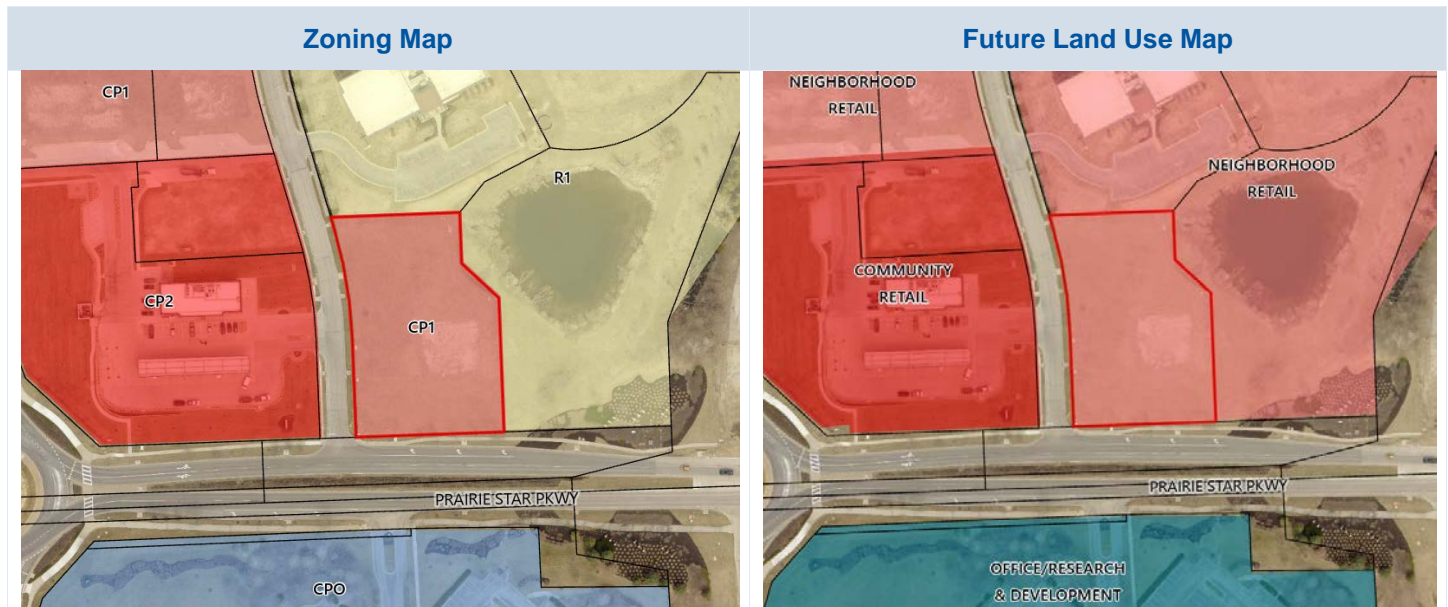


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Neighborhood Retail	CP-1, Planned Neighborhood Commercial	Undeveloped
North	Neighborhood Retail	R-1, Residential Single-Family District	Church or place of worship, Kaw Prairie Community Church
South	Office/Research & Development	CP-O, Planned General Office District	Hospital, Advent Health Lenexa
East	Neighborhood Retail	R-1, Residential Single-Family District	Stormwater detention pond
West	Community Retail	CP-2, Planned Community Commercial District	Gasoline sales, general & Convenience store – Casey's gas station & general store

The proposed medical office is compatible with existing medical uses in the area. The Advent Health Lenexa hospital is located across Prairie Star Parkway to the south and a medical office building (PL11-04) was constructed to the northwest of the site adjacent to Dunraven Road in 2012. Kaw Prairie Community Church is located adjacent to the north, a stormwater detention pond (Tract A of Reserve Plaza) is located to the east. A Casey's gas station and general store (PL20-12F) was recently completed in 2021 to the west of the subject site.

FINAL PLAN REVIEW

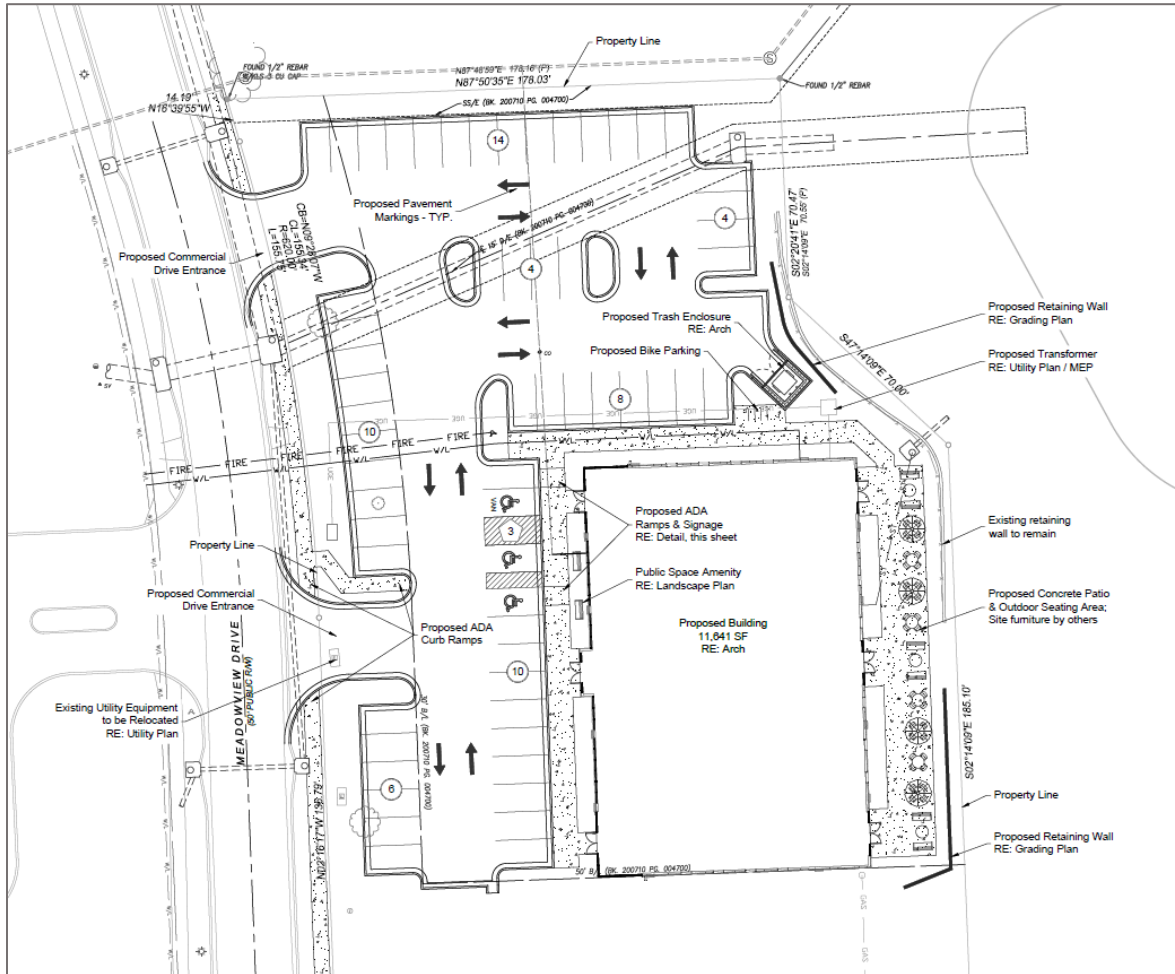


Exhibit 3: Site Plan.

★ Request for Exception to Required Parking Area Setback

[Section 4-1-B-26-B-7-a](#) of the UDC allows encroachments into parking setbacks are allowed as long the parking area complies with the other site plan requirements.

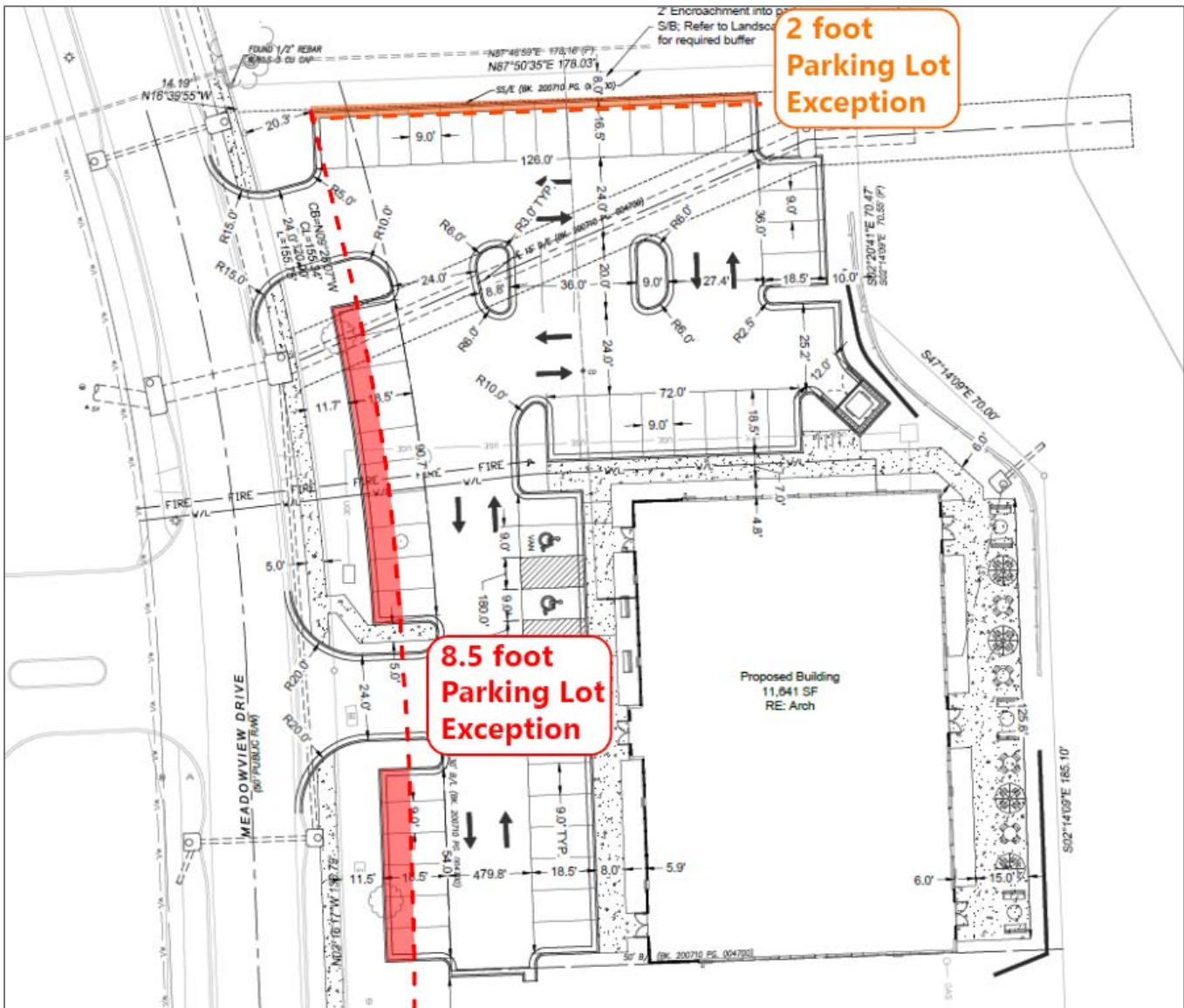


Exhibit 4: Graphic of Parking Lot Encroachment and Dimensions.

DIMENSIONAL STANDARDS

The applicant is requesting an exception to [Section 4-1-D-1-O-4-g](#) of the UDC to allow the parking lot to encroach two feet into the 10-foot required parking lot pavement setback along the north property line and 8.5 feet into the 20 feet required parking lot setback along Meadow View Drive.

TABLE 2: SETBACK ANALYSIS			
Property Line	Required Minimum Setback	Proposed Setback	Difference
North	10 feet for parking area	8 feet	- 2
West	20 feet for parking area	11.5 feet	- 8.5

PUBLIC IMPROVEMENTS

Public improvements include streetlights along Meadow View Drive.

TRAFFIC, ACCESS, AND PARKING

- Prairie Star Parkway is classified as a Major Arterial roadway.
- Meadow View Drive is classified as a Local Road.
- There are two points of access to the site from Meadow View Drive.
- The site plan meets parking requirements.

TABLE 3: PARKING ANALYSIS

Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
Medical Service	1 space per 200 square feet	59	59	0

STORMWATER

Stormwater management facilities for this site were approved with, and previously constructed by, the Reserve Plaza project. This development is appropriately tying into the existing stormwater facilities.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

LIGHTING

Proposed lighting types include street and internal parking lot lighting. Poles shall be Square Straight Steel Poles, 25 feet in height measured from top of foundation to fixture, with the base of the pole not to exceed 2 feet in height. The photometric plan is in compliance with the performance standards of [Section 4-1-C-4-I](#) of the UDC.

LANDSCAPING

The proposed perimeter plantings along Prairie Star Parkway, Meadow View Drive, and the east and north lot lines meet the perimeter planting requirements of [Section 4-1-D-2-L](#) of the UDC. The proposed landscape plan also meets the standards for street trees, internal parking lot landscaping, and conforms to the internal parking lot requirements. Final landscape design will be submitted and reviewed with the final development plan for this lot to ensure all applicable landscaping code standards landscaping standards have been met.

ARCHITECTURE

The proposed one-story medical office building is 11,641 square feet and 29' 9" tall. The building has three separate tenant areas with entrances on the east and west facades. The northernmost tenant space is a proposed 4,000 square foot dentist office, the middle space is a 3,600 square foot medical clinic, and the southernmost tenant space is 4,000 square feet proposed for an optometrist/eye care clinic.

Exterior finish materials include:



The materials and design are consistent in character with surrounding buildings in the area. The main entrances to the tenant areas are located on the west building elevation. Wood walls delineate the north and south tenant entrances. These walls continue in a curved shape to form an eave that accents the individual entrances. The middle tenant entrance is identified with articulated stone and a parapet varying the roofline.



Exhibit 5: Northwest Perspective Rendering.

The building has several windows on each façade, with storefront windows spanning the west elevation. A band of clerestory windows spans both the east and west elevations, wrapping around the corners of the building. Wood accent bands are located above each tenant entrance and will serve as the location for the proposed façade signs for the tenants. The wood accent bands for the north and south tenants wrap around the corners of the buildings to add definition to the entrance areas.



Exhibit 6: Southwest Perspective Rendering.

Exterior site elements include a trash enclosure and two pedestrian patio areas.

The proposed building meets the architectural standards required in [Section 4-1-C-5](#) of the UDC. Staff is supportive of the proposed architecture.

DEVIATIONS

The applicant is not requesting any deviations.

REVIEW PROCESS

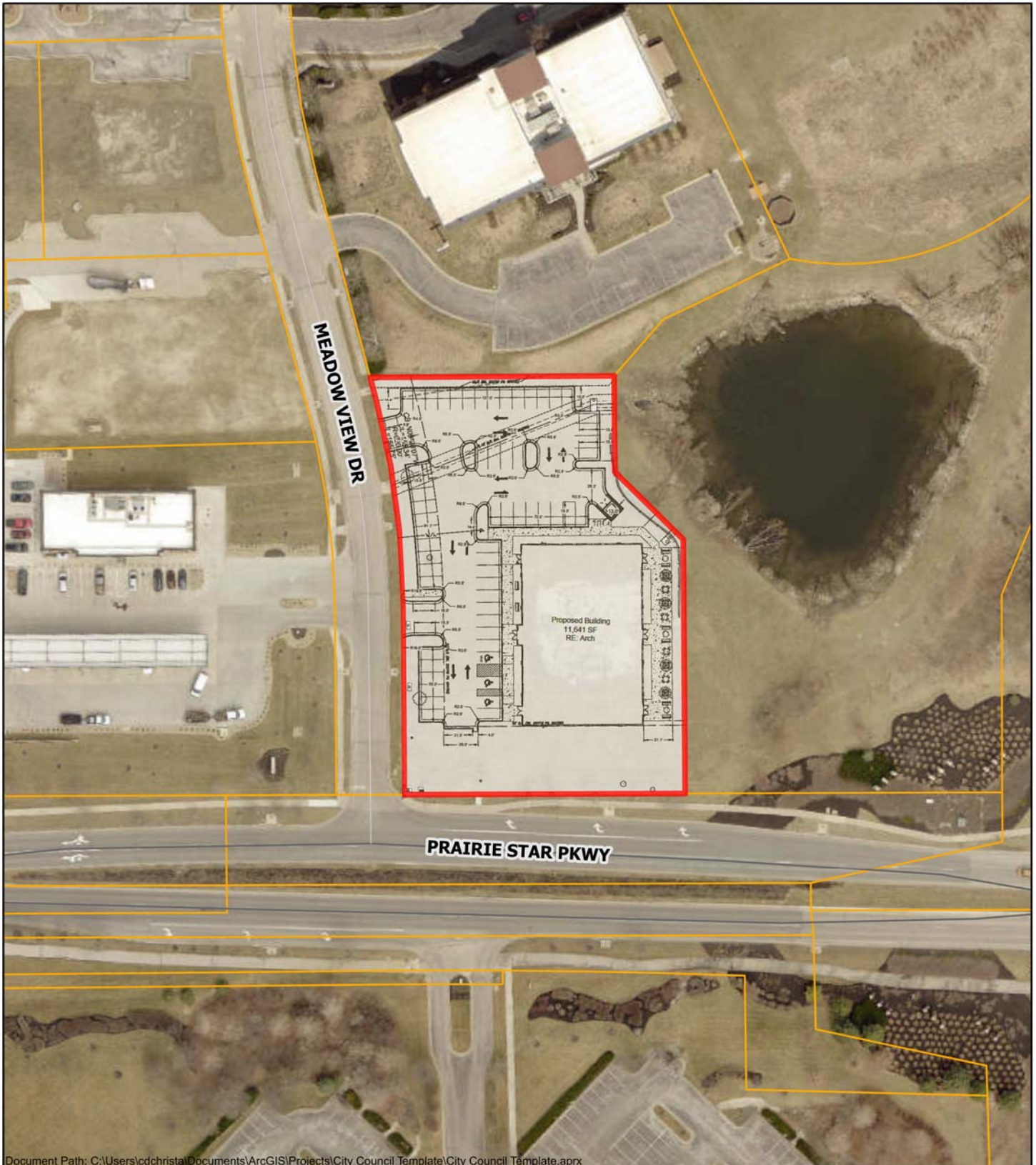
- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

- ★ **Staff recommends approval of the proposed Final Plan for Prairie Star Medical Office Building.**
 - This project is consistent with Lenexa's goals through **Responsible Economic Development** to promote **Vibrant Neighborhoods** and **Healthy People**.

FINAL PLAN

Staff recommends **approval** of the final plan for PL23-03F – **Prairie Star Medical Office Building** at 9471 Meadow View Drive, for medical services with an exception to allow the parking lot area to encroach 2-feet into the required 10-foot setback along the north property line and to encroach 8.5-feet into the required 20-foot setback from Meadow View Drive to the west.



Document Path: C:\Users\cdchrista\Documents\ArcGIS\Projects\City Council Template\City Council Template.aprx

Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

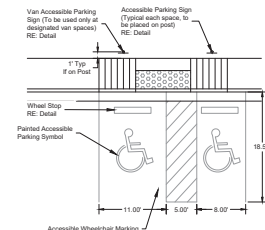
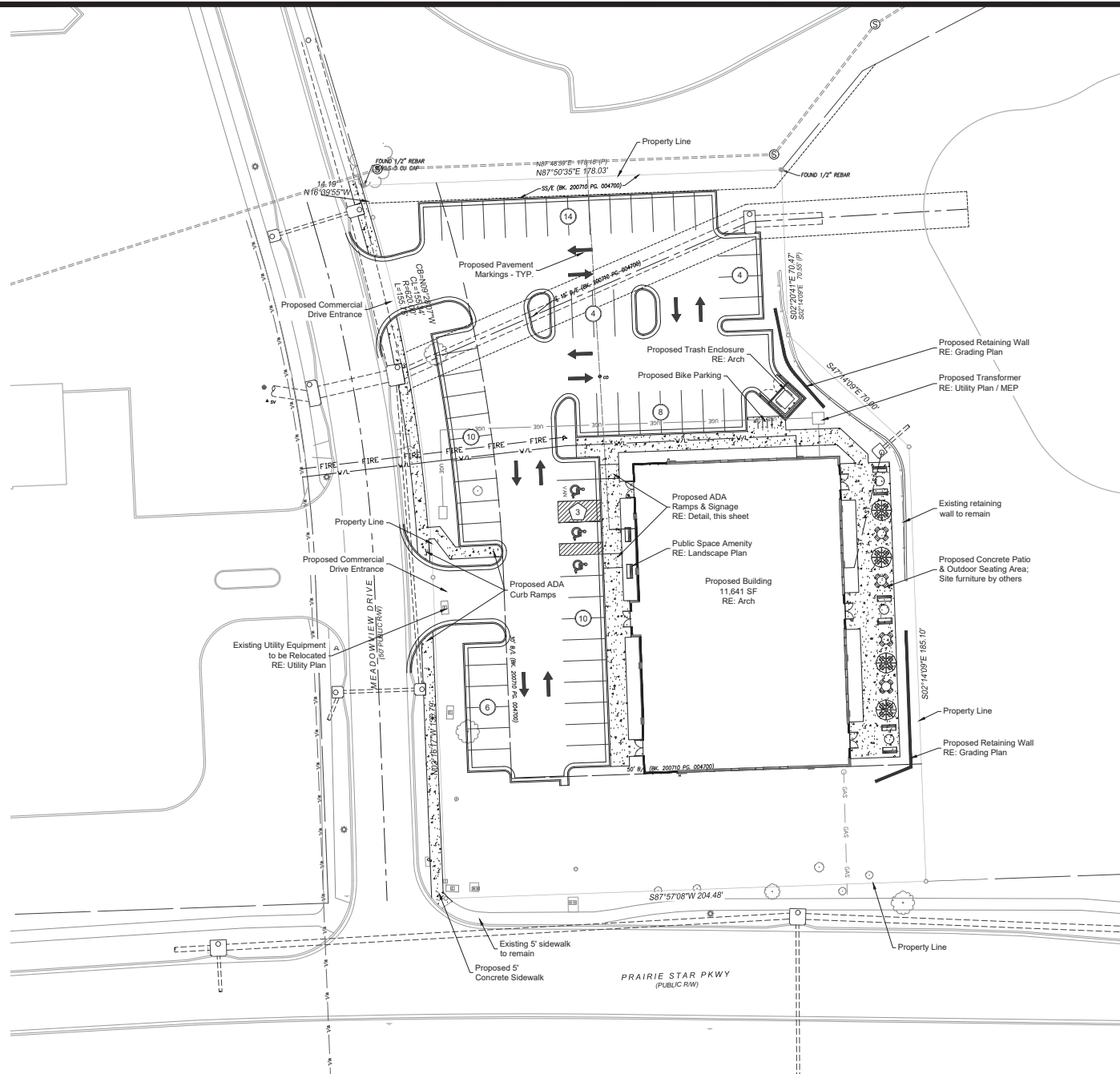
Prairie Star Medical Office Building PL23-03F



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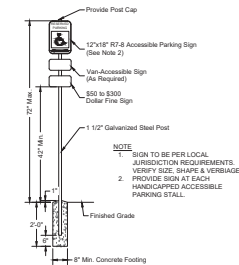


Revision
Date 22-01-2022 2:46pm
Drawn by: [Signature]
Checked by: [Signature]



Note: Accessible Parking Symbol and Signage shall comply with the applicable recommendations of the Manual of Uniform Traffic Control Devices (MUTCD)

ACCESSIBLE PARKING DETAIL
Not to Scale



HANDICAP SIGNAGE
Not to Scale

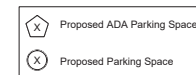
SITE SUMMARY		
Area	Square Feet	% of Site
Building	11,641	19.76%
Paved Surface	28,917	49.08%
Unpaved Surface	18,365	31.16%
Total Lot Area	58,923 SF (1.35 AC)	

LAND USE SCHEDULE


Zoning
Existing: CP-1
Proposed: CP-1
Proposed Use: Commercial - Medical Office

Parking
Required: 1 Space / 200 SF = 58 Spaces
Provided: 59 Spaces (Incl. 3 ADA)

Total Floor Area: 11,641 SF
Land Area: 58,923 SF (1.35 AC.)
Impervious Coverage (Building & Pavement): 40,558 SF



TREES	CODE	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	GA	Ginkgo biloba 'Autumn Gold' TM / Autumn Gold Maidenhair Tree	B&B	2" Cal.		5
	LF	Liriodendron tulipifera 'Fastigiata' / Columnar Tulip Poplar	B&B	2" Cal.		6
	NH	Nyssa sylvatica 'Haymanred' TM / Red Rage Tupelo	B&B	2" Cal.		4
	TM	Taxodium distichum 'Mickelson' TM / Shawnee Brave Bald Cypress	B&B	2" Cal.		2
ORNAMENTAL	TC	Tilia cordata 'Greenspire' / Littleleaf Linden	B&B	2" Cal.		3
	CODE	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
	CR	Cercis canadensis 'JN2' TM / The Rising Sun Eastern Redbud	B&B		6' Ht. Min.	5
	CK	Cornus kousa / Kousa Dogwood	B&B		6' Ht. Min.	5
	CODE	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
SHRUBS	CAK	Calamagrostis x acutiflora 'Karl Foerster' / Feather Reed Grass	3 Gal.		18"-24" Ht. Min.	21
	FB2	Fothergilla gardenii 'Blue Mist' / Blue Mist Dwarf Fothergilla	3 Gal.		18"-24" Ht. Min.	42
	HB	Hydrangea paniculata 'ILVOBO' TM / Bobo Panicked Hydrangea	3 Gal.		18"-24" Ht. Min.	37
	MM	Miscanthus sinensis 'Morning Light' / Morning Light Eulalia Grass	3 Gal.		18"-24" Ht. Min.	51
	CODE	BOTANICAL / COMMON NAME	CONT	CAL	SIZE	QTY
EVERGREEN SHRUBS	JF	Juniperus chinensis 'Sea Green' / Sea Green Juniper	5 Gal.		18"-24" Ht. Min.	27
	JH2	Juniperus horizontalis 'Hughes' / Hughes Juniper	5 Gal.			3
	JS2	Juniperus scopulorum 'Skyrocket' / Skyrocket Juniper	5 Gal.		18"-24" Ht. Min.	33

<u>GROUND COVERS</u>	<u>BOTANICAL / COMMON NAME</u>	<u>CONT</u>	<u>QTY</u>
	Turfgrass Sod Fescue Mix; RE: Notes / Fescue Sod	SOD	14,622 sf

1. LOCATE UTILITIES PRIOR TO COMMENCING LANDSCAPE OPERATIONS. ALL TREES SHALL BE FIELD POSITIONED AS TO AVOID CONFLICTS WITH EXISTING AND PROPOSED UTILITIES. NOTIFY LANDSCAPE ARCHITECT OF ANY CONFLICTS OR OBSTRUCTIONS.
2. CONTRACTOR SHALL STAKE ALL PLANTING AREAS IN THE FIELD PRIOR TO PLANTING FOR APPROVAL OF THE OWNER OR THEIR REPRESENTATIVE.
3. QUANTITIES SHOWN ARE FOR REFERENCE ONLY. CONTRACTOR SHALL VERIFY ALL PLANT QUANTITIES PRIOR TO BIDDING AND SHALL BE RESPONSIBLE FOR ALL QUANTITIES FOR THEIR BID. ANY DISCREPANCIES WITH THE PLAN SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT. THE PLAN QUANTITIES SHALL SUPERCEDE SCHEDULED QUANTITIES.
4. ALL PLANT MATERIAL SHALL BE SPECIMEN QUALITY AND SHALL COMPLY WITH RECOMMENDATIONS AND REQUIREMENTS OF ANSI Z60.1 'THE AMERICAN STANDARD FOR NURSERY STOCK'.
5. ALL PLANTING BEDS & NATIVE GRASS STANDS SHALL BE EDGED AS SHOWN IN PLAN.
6. PREPARE PLANTING BEDS AND INCORPORATE AMENDMENTS ACCORDING TO PLANS.
7. SHREDDED HARDWOOD MULCH, PER SPECIFICATIONS SHALL BE USED AS A THREE INCH (3") TOP DRESSING IN ALL PLANTING BEDS AND AROUND ALL TREES. SINGLE TREES AND SHRUBS SHALL BE MULCHED TO THE OUTSIDE EDGE OF THE SAUCER OR LANDSCAPE ISLAND.
8. AGGREGATE MULCH, PER SPECIFICATIONS SHALL BE USED AS A THREE INCH (3") TOP DRESSING ONLY IN PLANTING BEDS ADJACENT TO THE BUILDING FOUNDATION.
9. ALL TREES SHALL BE STAKED PER DETAIL.
10. ALL PLANT MATERIAL SHALL BE INSTALLED TO ALLOW A ONE FOOT (1') CLEARANCE BETWEEN PLANT AND ADJACENT PAVEMENT.
11. THE LANDSCAPE CONTRACTOR SHALL NOT COMMENCE WORK UNTIL THE SITE IS FREE OF DEBRIS CAUSED BY ON-GOING CONSTRUCTION OPERATIONS. REMOVAL OF DEBRIS SHALL BE THE RESPONSIBILITY OF THE GENERAL CONTRACTOR. LANDSCAPE WORK SHALL NOT BEGIN UNTIL THE LANDSCAPE ARCHITECT AND OWNER HAVE GIVEN WRITTEN APPROVAL FOR SUCH. THERE SHALL BE NO DELAYS DUE TO LACK OF COORDINATION FOR THIS ACTIVITY.
12. THE LANDSCAPE ARCHITECT AND OWNER SHALL APPROVE GRADES AND CONDITION OF SITE PRIOR TO SODDING/SEEDING OPERATIONS.
13. ALL AREAS DISTURBED DURING CONSTRUCTION AND NOT DESIGNATED FOR OTHER PLANTINGS OR HARDSCAPE SHALL BE SODDED WITH TURF TYPE FESCUE.
14. ALL LANDSCAPE AREAS SHALL BE IRRIGATED. TURF AREAS SHALL BE IRRIGATED BY SPRAY OR ROTOR. PLANT BEDS SHALL BE IRRIGATED BY DRIP IRRIGATION. IRRIGATION SYSTEM SHALL INCLUDE AUTOMATIC RAIN SENSOR DEVICE. IRRIGATION SHOP DRAWINGS SHALL BE PROVIDED BY THE CONTRACTOR FOR APPROVAL PRIOR TO CONSTRUCTION.

Street Trees
Required: 1 Tree / 40'
Provided:
Prairie Star Pkwy (204') = 5 trees (incl. 3 existing)
Meadow View Dr (238') = 6 trees

Perimeter Plantings Along Parkway (Prairie Star Pkwy)
Required:
Adj. to Building or Open Area: 2 shade, 3 orn., 25 shrubs, 200 sf bed / 100'
Adj. to Parking Lot: As above + 3' ht. shrub screen 75% length of parking

Provided:
204' = 4 trees (incl. 4 existing)*, 6 orn., 51 shrubs, 408 sf bed, shrub screen adj. to parking
**Preserved Tree Credits: Trees that measure from 2 to 10 inch caliper shall be credited on a 1-for-1 basis. Trees that measure greater than 10 inches shall be credited on a 2-for-1 basis.*

Perimeter Plantings Along Arterial & Collector (Meadow View Dr)
Required:
Adj. to Building or Open Area: 2 trees, 1 orn., 12 shrubs, 75 sf bed / 100'
Adj. to Parking Lot: As above + 3' ht. shrub screen 75% length of parking

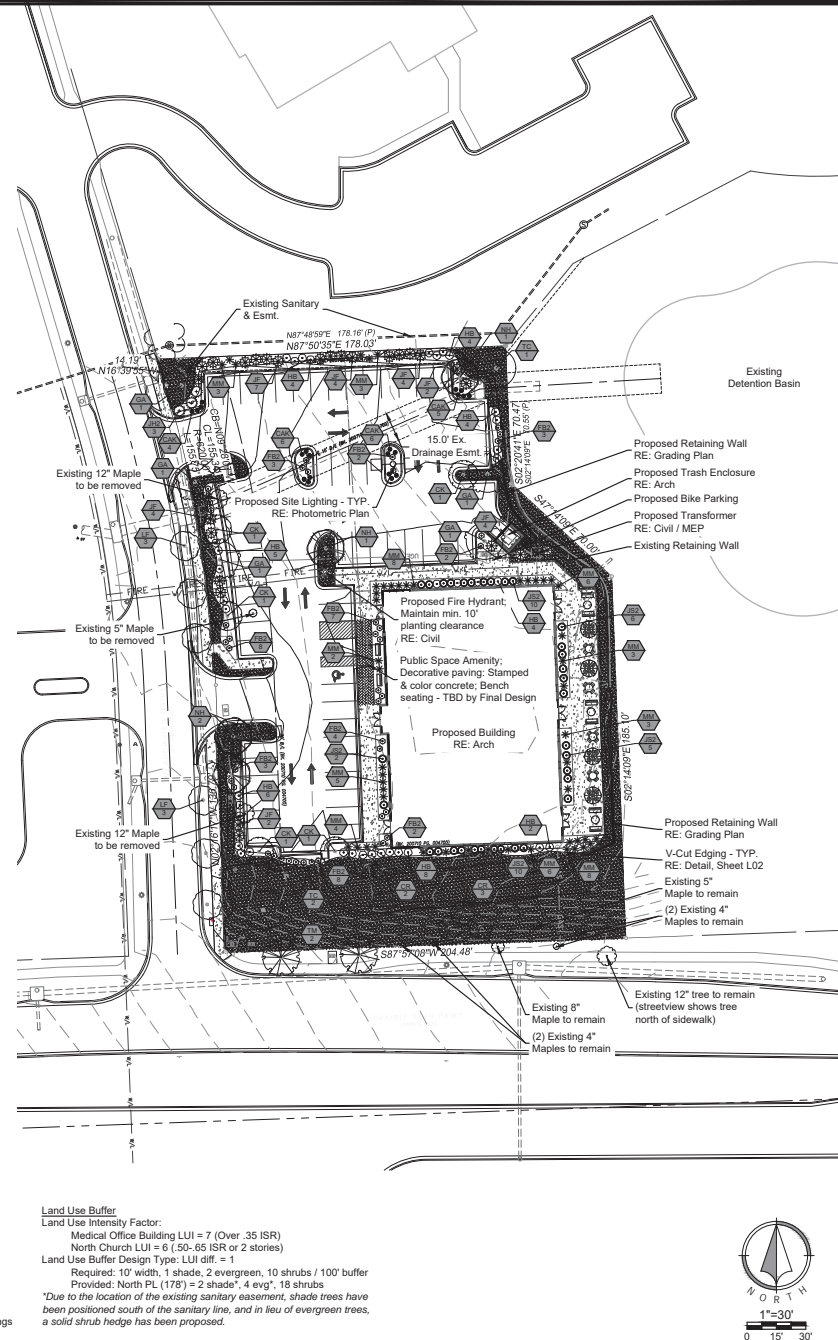
Provided:
238' = 5 trees, 3 orn., 29 shrubs, 179 sf bed, shrub screen adj. to parking

Perimeter Plantings Along Lot Lines (East & North)
Required:
Adj. to Building: 1 shade, 1 shade / 100'
Adj. to Parking Lot: 1 shade, 9 shrubs / 100'

Provided:
East PL (103') = 1 shade, 10 shrubs
North PL (145') = 2 shade, 13 shrubs

Internal Parking Lot
Required: 7.5% landscaped area, 1 tree / island
Provided: 21,206 sf parking lot x 7.5% = 1,590 sf, 5 islands = 5 trees

Public Space Amenity
Required: 10 sf / 1 parking space
Provided: 59 parking spaces x 10 sf = 590 sf: Decorative paving, benches, plantings



Prairie Star Medical Office Building
Lenexa, Johnson County, Kansas
22-0185

Landscape Plan

12/22/22	Per City Comments
12/05/22	Original Submittal
DATE	REVISION

DRAWN BY	CHECKED BY
----------	------------

DRAWN BY _____ CHECKED BY _____

**Renaissance
Infrastructure
Consulting**



400 E 17TH STREET
KANSAS CITY, MISSOURI

T. Andrew Gabbert - KS#714

Sheet
L01



1 WEST ELEVATION
1/8" = 1'-0"



2 EAST ELEVATION
1/8" = 1'-0"

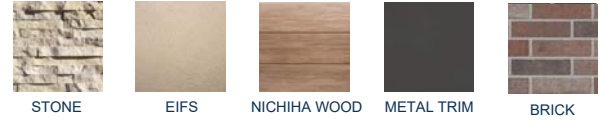


3 SOUTH ELEVATION
1/8" = 1'-0"



4 NORTH ELEVATION
1/8" = 1'-0"

PROPOSED EXTERIOR MATERIAL SCHEDULE



GENERAL SIGNAGE CRITERIA

OVERALL BUILDING HEIGHT: 25'-9"
BUILDING FACADE: 3387 SQFT
ALLOWABLE SIGNAGE: 170 SQFT PER SIGN
TEXT HEIGHT: 18"
LOGO HEIGHT: 6'-0"



5 3D View 1

NORTHWEST PERSPECTIVE CONCEPT



6 3D View 2

SOUTHWEST PERSPECTIVE CONCEPT



ARCHITECTURE

15225 Broadmoor Street
Overland Park, KS 66223
h|d Architecture, LLC

Certificate of Authority Number:



Name: Gregory H. Hightberger
License: A6656
State: Kansas
Expiration: 02/24/2020

CIVIL CONSULTANT

Renaissance Infrastructure Consulting
8653 Penrose Ln
Lenexa, KS, 66219

Phone Number: 913-317-9500

STRUCTURAL CONSULTANT

Bob D Campbell & Co
4338 Bellevue Ave
Kansas City, MO 64111

Phone Number: 816-778-7147

MEP CONSULTANT

SBYS Engineers, LLC
1100 Main Street, Floor 4
Kansas City, MO 64105

Phone Number: 913-689-9449

PRAIRIE STAR MEDICAL
OFFICE BUILDING

9471 Meadow View Dr
Lenexa, KS 66227

Date 12/22/2022
Job Number 22-038
Drawn By TWG
Checked By JD

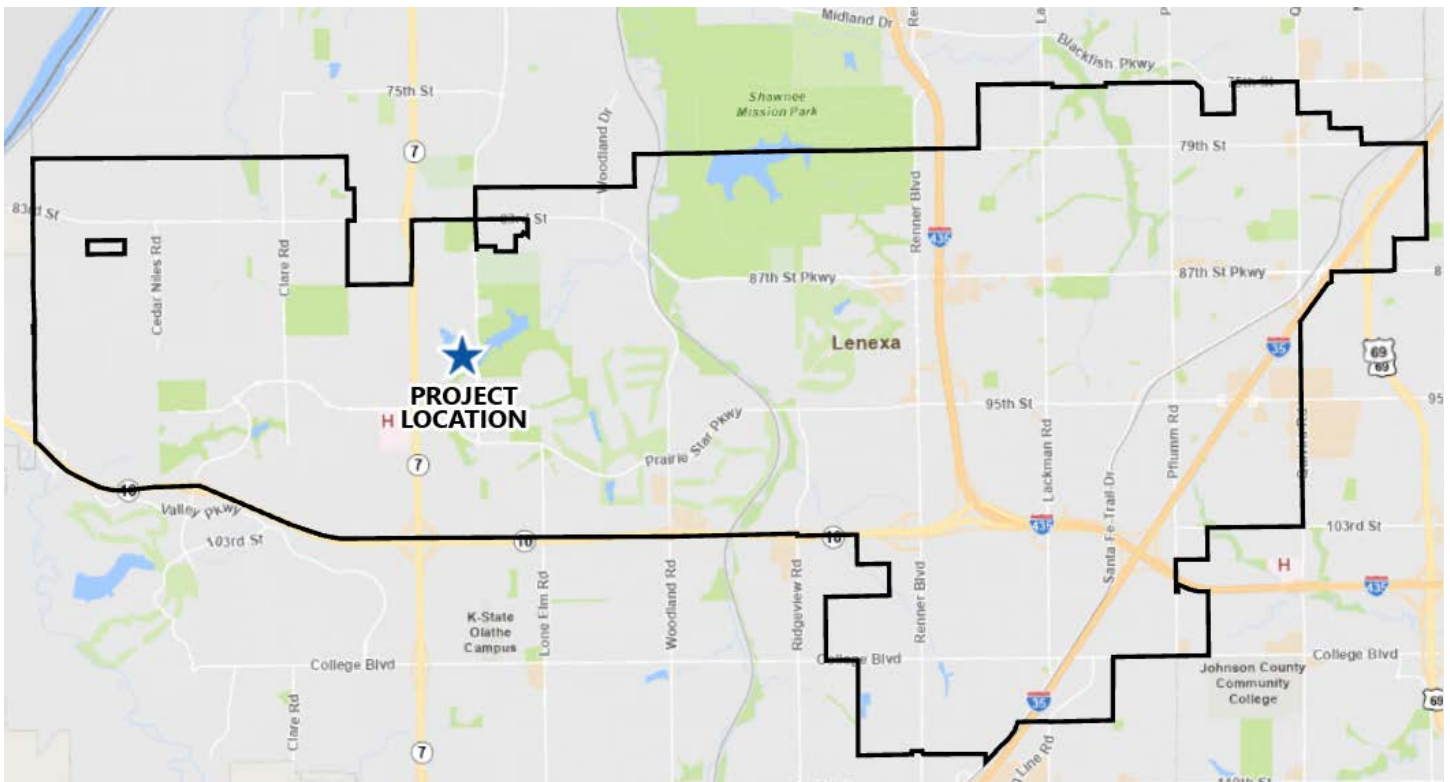
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© h|d Architecture, LLC

ELEVATIONS

WATERCREST LANDING AMENITY AREA

Project #:	PL23-01F	Location:	9096 Monticello Road
Applicant:	Doug Ubben, Phelps Engineering	Project Type:	Final Plan
Staff Planner:	Kimberly Portillo, AICP	Proposed Use:	Subdivision Amenity Area



PROJECT SUMMARY

This application is for an amenity area as part of the Watercrest Landing residential subdivision and is located at 9096 Monticello Road in the RP-1 Planned Residential Low-Density Zoning District. The applicant proposes to construct a private residential amenity area with a clubhouse and additional accessory uses. The amenity area will serve the single-family lots in the Watercrest Landing subdivision. The clubhouse is one story tall and totals 1,600 square feet. The development includes construction of extended sidewalk on the tract to connect with the proposed trail system. Tract B, which houses the site, and the amenities will be owned and maintained by the Watercrest Landing Homes Association. The applicant requests approval of a final plan for the development. The proposed final plan is consistent with the approved preliminary plat (PT21-02P) and final plat (PT21-13F), which was approved by the Governing Body on August 17, 2021. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

The subject property was annexed into the city limits in 1993. In 2006, the area was part of a master development plan known as Bristol Ridge Farms, which included rezoning of 233 acres from agricultural uses to the RP-1, RP-2, NP-O, and CP-1 Zoning Districts. The development did not move forward.

In 2020, applications for rezoning, a preliminary development plan, a concept plan, and a preliminary plat were submitted for a 188-acre portion of the former Bristol Ridge Farms site. The associated applications were RZ20-05, PL20-06P, and PL20-02CP. The proposed development at that time was to be a master-planned development consisting of apartments, townhomes, and single-family. This development was known as Copper Creek Apartments and Townhomes and Watercrest Landing South. The applicant withdrew the application following the Governing Body remanding the applications back to the Planning Commission and subsequently continuing the project in December of 2020.

In January of 2021, a modified application for rezoning and a preliminary plan/plat was resubmitted for Watercrest Landing South. The updated rezoning (RZ21-01) and preliminary plat (PT21-02P) included rezoning from the AG, RP-1, and NP-O Zoning Districts to the RP-1 Zoning District for 94.73 acres. The rezoning and preliminary plan/plat were approved by the Planning Commission on 03/01/2021 and Governing Body on 3/16/2021.

A final plat, Watercrest Landing, Third Plat (PT21-13F) was approved by the Planning Commission on 08/02/2021 with dedications accepted by the Governing Body on 08/17/2021.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
2.28	1,600	RP-1	Suburban Residential & Low Density Residential

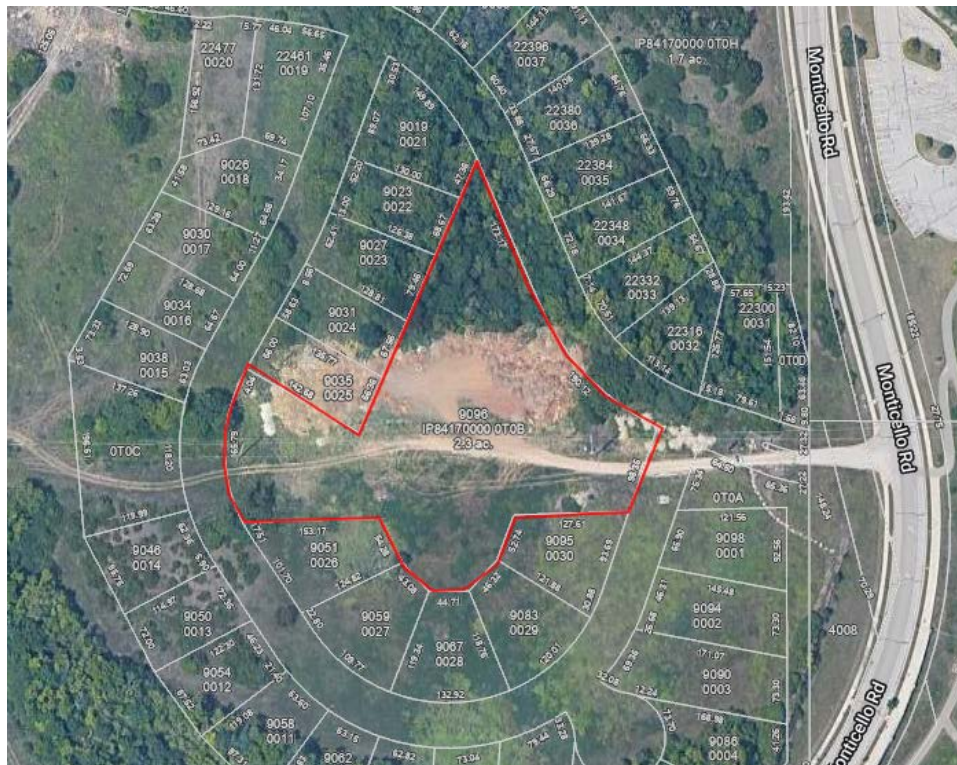


Exhibit 1: Aerial Image of Subject Site

LAND USE REVIEW

The proposed use is a private subdivision amenity area. The general use and layout have been approved as part of the overall subdivision plan for Watercrest Landing. The amenity area will only be available for the use of the residents of Watercrest Landing subdivision and their limited guests. The clubhouse building is intended to serve as a pool house with seating, dressing rooms, and other uses complimentary to the pool. There will be no employees present and no available space for private rentals or meetings. The amenities will be open for guests during daylight hours in the summer pool season. The clubhouse will be open to guests by appointment only through the homeowner's association manager during the off-season.

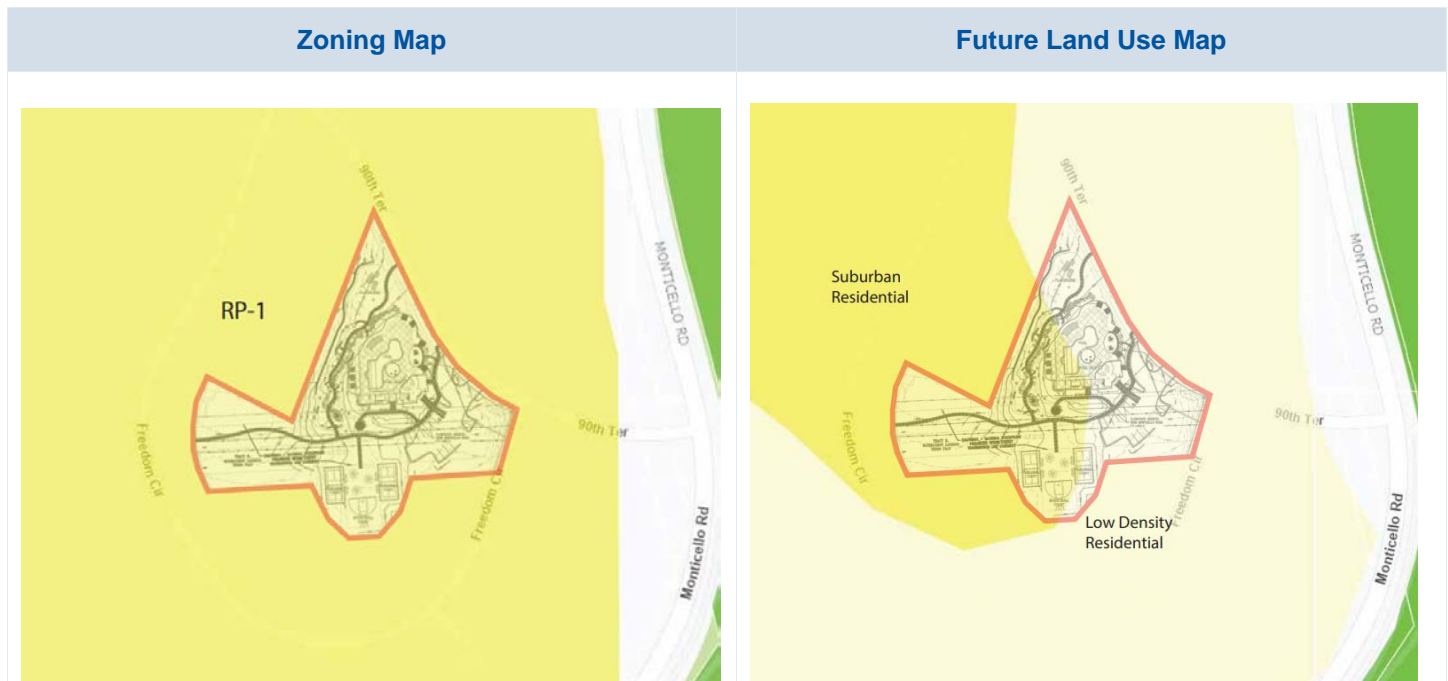


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Low Density Residential & Suburban Residential	RP-1 Planned Residential Low Density	Undeveloped
	Low Density Residential & Suburban Residential	RP-1 Planned Residential Low Density	Single-family lots
	Low Density Residential	RP-1 Planned Residential Low Density	Single-family lots
	Low Density Residential	RP-1 Planned Residential Low Density	Single-family lots
	Suburban Residential	RP-1 Planned Residential Low Density	Single-family lots

The location of the proposed amenity area was approved as part of the overall subdivision plan for Watercrest Landing. The proposed use is compatible with the surrounding residential uses.

FINAL PLAN REVIEW

The applicant proposes to construct a clubhouse building, pool and pool deck, mini-golf course, playground, basketball court, pickleball courts, and an associated parking area on Tract B of Watercrest Landing, Third Plat. On-site landscaping, buffering and screening are to be provided as well as street tree plantings. The site is located at the intersection of planned Freedom Circle and 91st Terrace and has access from 91st Terrace.

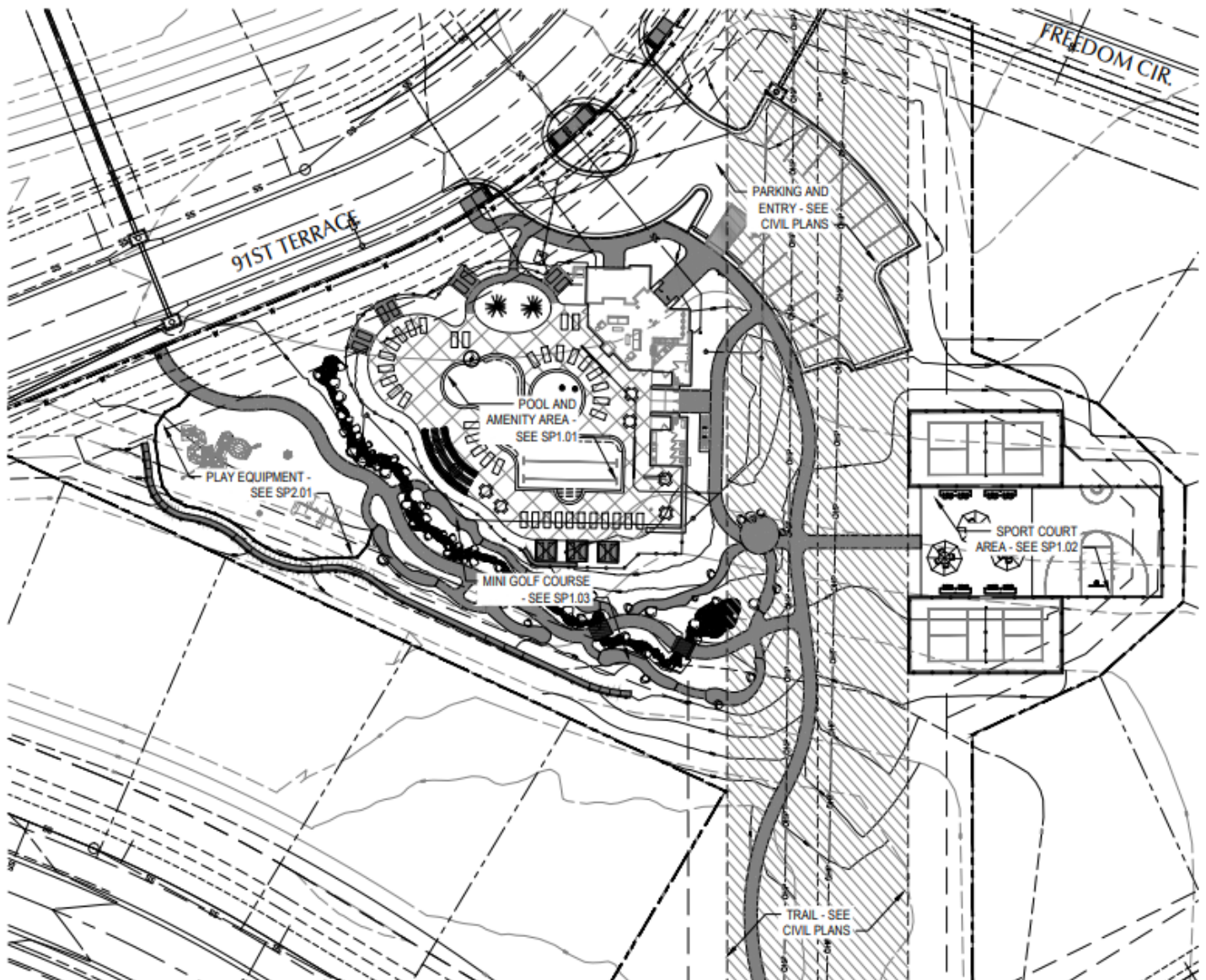


Exhibit 2: Proposed Site Plan

DIMENSIONAL STANDARDS

Section 4-1-B-26-D of the Unified Development Code (UDC) states that all permitted or approved non-residential uses (excluding permitted accessory structures) shall be subject to the minimum property development regulations as listed for the CP-O Planned General Office Zoning District and additionally 4-1-B-26-E of the UDC

states that any building, structure, or appurtenance not subject to other setback requirements, except fences and walls, shall maintain a setback of at least 2 feet from all property lines.

TABLE 2: SETBACK ANALYSIS (CLUBHOUSE)			
Type	Required Minimum Setback	Proposed Setback	Difference
Streets	30 ft	42.34 ft at closest point	+ 12.34 ft
Other	30 ft	75.86 ft at closest point	+ 45.86 ft

The clubhouse is the primary structure and is in compliance with the dimensional standards as noted above. The closest amenities to property lines are the pickleball courts at 7.75 feet and 7.32 feet from the nearest property line at the nearest corner. The pickleball courts consist of flatwork with a four-foot-tall fence. This meets the required setback for flatwork; however, the applicant will provide extra shrubbery in this area to serve as screening. Additionally, the applicant plans to exclude court lighting to discourage night-time use of the courts.

PUBLIC IMPROVEMENTS

Plans for W. 90th Terrace are approved as part of Watercrest Landing, Third Plat and are currently under construction. These improvements will provide access for this project as well as for the single-family lots.

TRAFFIC, ACCESS, AND PARKING

Two vehicular entrances designed per the City's commercial design standards will provide access from W. 90th Terrace in a U-shape drive for clubhouse drop-off and access to the parking lot. An internal 5' sidewalk network will connect to the sidewalk network approved along W. 90th Terrace. The internal sidewalk will connect the various amenities and continue south to link into the proposed 10' asphalt trail network within the subdivision.

This development is formally classified as a "recreation facility, private" land use and does not have a designated parking requirement in the UDC but rather allows flexibility for City Staff to determine the required amount of parking. The applicant proposes 16 parking stalls with one accessible space. With a clubhouse floor area of 1,600 SF, this provides 1 stall per 100 SF of floor area. Similar uses in the UDC with designated parking requirements include "club/lodge" and "fitness center", both of which have a requirement of 1 stall per 200 SF. This is a private facility only intended to serve the nearby residents of the subdivision, most of whom are within reasonable walking distance to the facility. Staff concludes that with these considerations, the access to the existing trail network, and addition of bike racks, the parking provided is sufficient.

TABLE 3: PARKING ANALYSIS				
Land Use	Parking Formula	Required Parking	Proposed Parking	Difference
Recreation facility, private	To be determined by City	To be determined by City	16	n/a

STORMWATER

Stormwater management for this project was included within, and is currently being constructed, with the Watercrest Landing, Third Plat improvements.

FIRE PREVENTION

The Fire Department reviewed the plans based on the current adopted fire codes and local amendments. All general planning review comments have been acknowledged or satisfied and there are no outstanding Fire Department planning review items that need to be addressed for this project to move forward. A more detailed fire code review will be conducted based on the adopted codes at the time of the building permit documentation submittal.

LIGHTING

The applicant elected to not provide exterior lighting for the amenity areas because use will be restricted to daylight hours. Wall sconces on the clubhouse building are to be in a fixed downcast position.

LANDSCAPING



Exhibit 3: Landscape Plan

The landscape plan includes street trees as shown in the previously approved final plat. No trees are to be placed within the Evergy easement.

Staff requested additional screening along the southernmost tip of the site to serve as a buffer between the sport court area and adjacent rear yards. The applicant agreed to provide the screening.

Additional landscaping will be provided as a feature of the amenity area, including buffer plantings between the playground and adjacent properties, parking lot screening, and plantings along the foundation of the building. Large deciduous trees are also proposed throughout the site. Turf and walking paths will be provided as part of the mini-golf course.



Exhibit 6: Playground Structure

A playground is proposed with typical metal and plastic design. The playground and pool will both be enclosed with a 5 ft steel fence with self-closing and self-latching gate.

Two pickleball and one basketball court are proposed at the south end of the site. The sport courts will be flatwork with a 4' high black vinyl coated chain-link fence enclosure.

DEVIATIONS

The applicant is not requesting any deviations.

REVIEW PROCESS

- The Planning Commission is the final authority for approval of this project.
- The applicant should inquire about additional City requirements, such as permits and development fees.

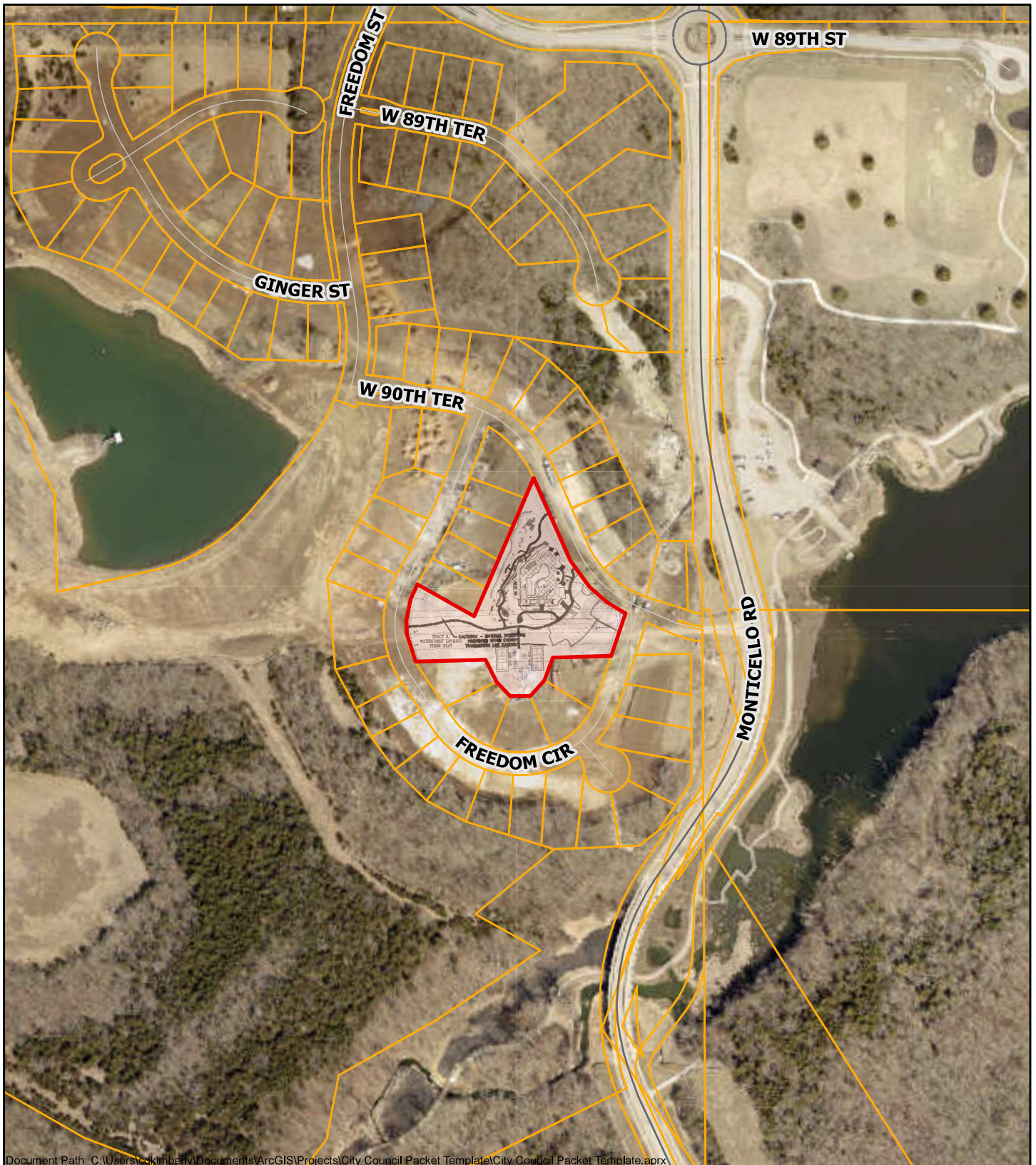
RECOMMENDATION FROM PROFESSIONAL STAFF

- ★ **Staff recommends approval of the proposed Final Plan for Watercrest Landing Amenity Area.**
 - This project promotes the Vision 2040 goal of Vibrant Neighborhoods.

FINAL PLAN

Staff recommends **approval** of the final plan for PL23-01F – **Watercrest Landing Amenity Area** at 9096 Monticello Road, for a residential amenity area, with the following conditions.

1. Prior to building permit approval, the applicant shall provide to the City for review a copy of the encroachment agreement from Evergy for work occurring within the designated easement.

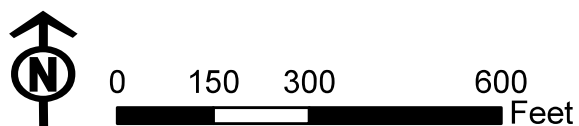


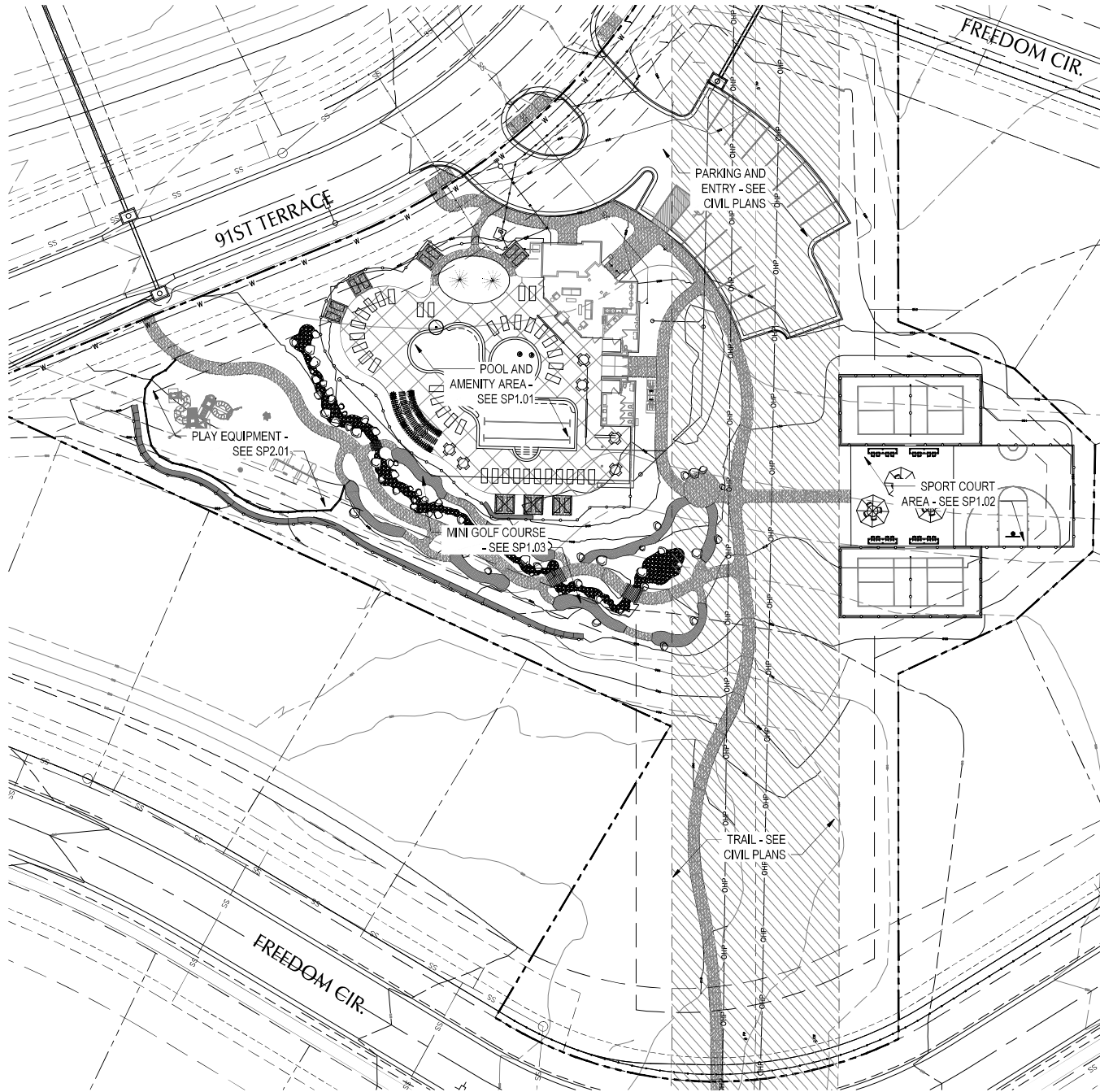
Document Path: C:\Users\kimberly\Documents\ArcGIS\Projects\City Council Packet Template\City Council Packet Template.aprx

Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Watercrest Landing Amenity Area Final Plan

PL23-01F





ARCHITECTURE
LANDSCAPE
ARCHITECTURE
ENERGY SERVICES

NSPJ

ARCHITECTS^{PC}

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F. 913.831.1563
NSPJARCH.COM
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3515 W. 25TH ST., SUITE 201
PRAIRIE VILLAGE, KS 66208



A NEW AMENITY CENTER FOR:

WATERCREST LANDING SOUTH AMENITY CENTER

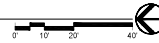
LENEXA, KANSAS

DRAWING RELEASE LOG:

- 12/06/2022 POP SUBMITTAL
- 12/27/2022 POP REVIEW/ITAL

REVISIONS

PRELIMINARY
OVERALL SITE PLAN



FINAL DEVELOPMENT PLAN **SP1.00**

DATE:
12/27/2022

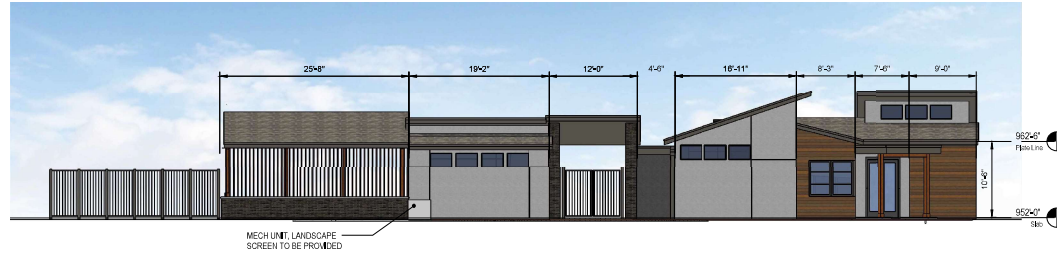
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BNH

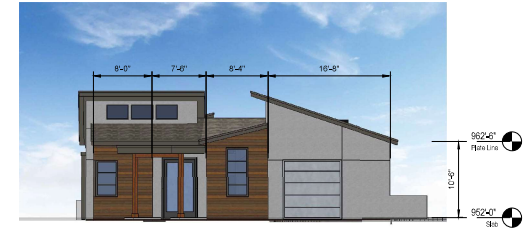
SHEET NO.

MATERIAL LEGEND

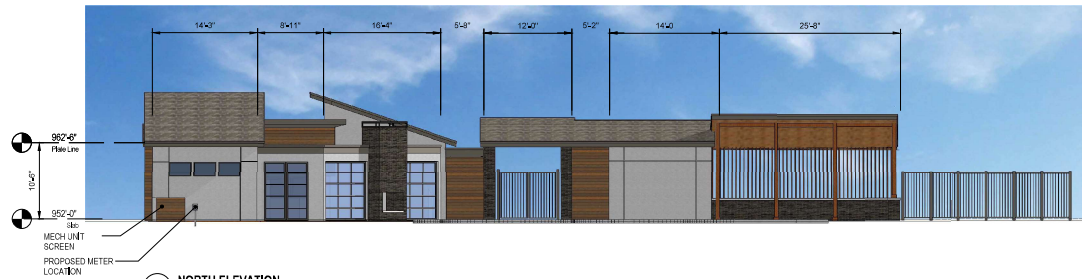
	ASPHALT SHINGLE
	DARK BRICK
	DARK GRAY STUCCO
	OFF WHITE STUCCO
	STEEL SIDING
	TREATED TIMBER
	STEEL SOFFIT



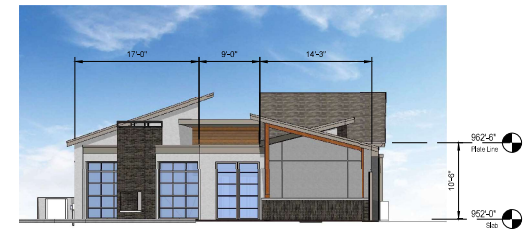
5 SOUTH ELEVATION
Scale: 1/8" = 1'-0"



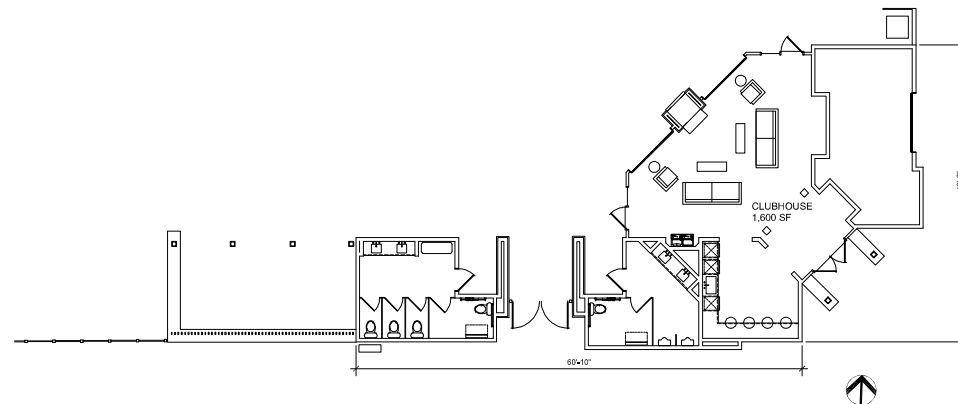
4 EAST ELEVATION
Scale: 1/8" = 1'-0"



3 NORTH ELEVATION
Scale: 1/8" = 1'-0"



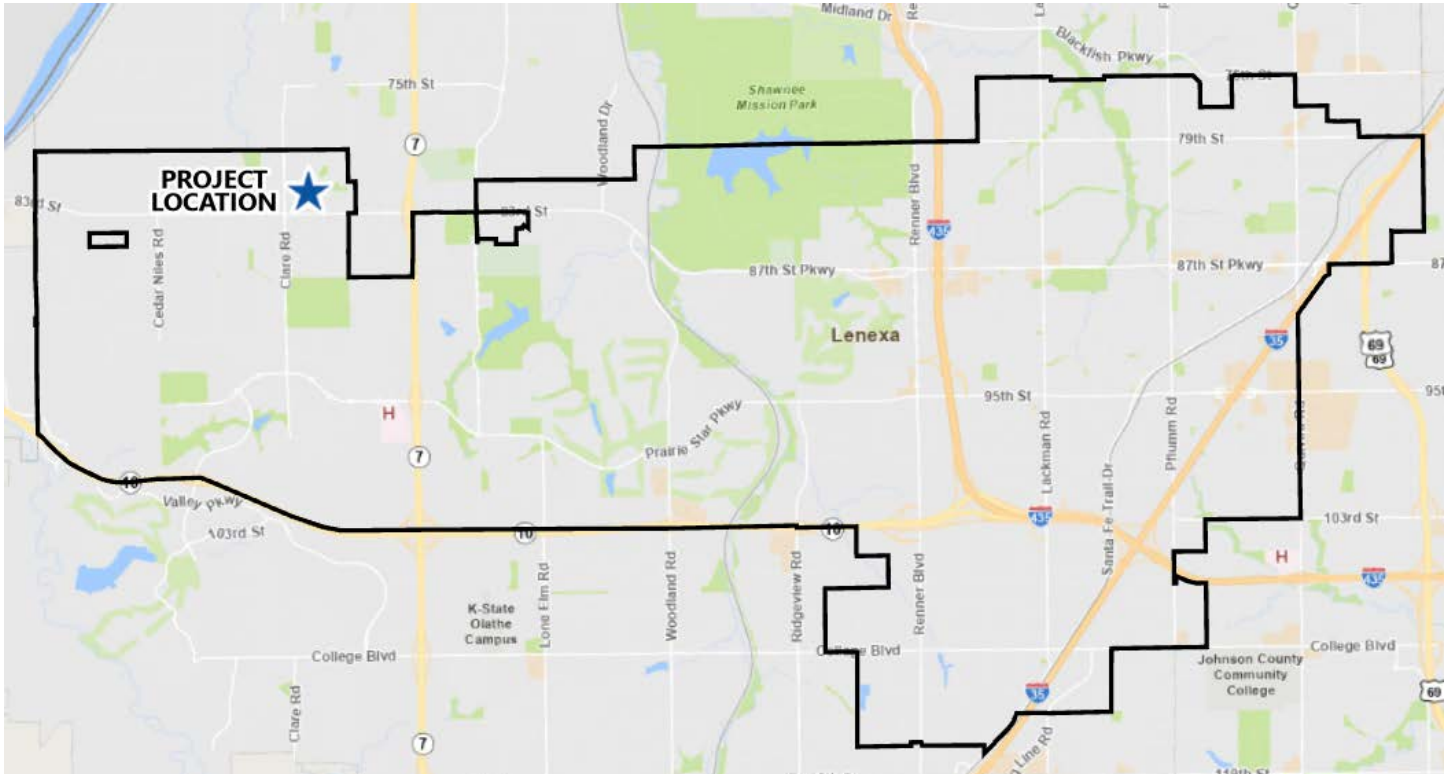
2 WEST ELEVATION
Scale: 1/8" = 1'-0"



1 BUILDING PLAN
Scale: 1/8" = 1'-0"

BELMONT ESTATES, SECOND PLAT

Project #:	PT23-01PR, PT23-02F	Location:	8110 Belmont Drive
Applicant:	Jake Hattock, Schlagel Associates	Project Type:	Preliminary & Final Plat
Staff Planner:	Christa McGaha, AICP	Proposed Use:	Single-Family Residential



PROJECT SUMMARY

The applicant proposes a preliminary plat and final plat to be considered simultaneously for a replat of Lots 8-13 of Belmont Estates to reconfigure the parcels into two single-family residential lots. The development includes the vacation of the existing right-of-way and the dedication of necessary right-of-way to move the planned cul-de-sac farther south, the construction of a new public street, and related infrastructure improvements. The applicant requests approval of both a preliminary and final plat for the development. This project does not require a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

Belmont Estates is located north of 83rd Street between Gleason Road and Clare Road. The original 20-lot plat for Belmont Estates was approved in 1966. The subdivision was annexed into the City of Lenexa in 1986.

LAND AREA (AC) 3.56	BUILDING AREA (SF) N/A	CURRENT ZONING R1	COMP. PLAN Low Density Residential
-------------------------------	----------------------------------	-----------------------------	--



Exhibit 1: Aerial Image of Subject Site.



LAND USE REVIEW

The proposed application is for a replat of a single-family subdivision to combine six existing single-family lots into two larger single-family lots and relocate the planned cul-de-sac further south.

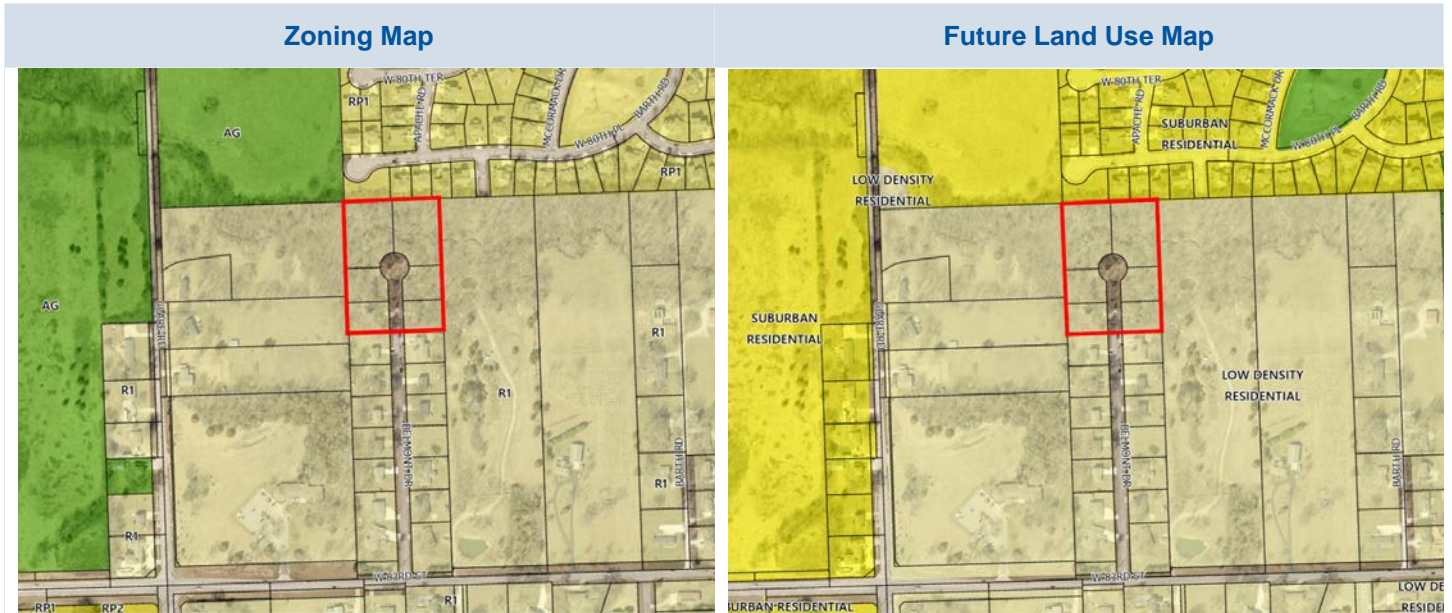


TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Low Density Residential	R-1, Residential Single-Family	Undeveloped
	Suburban Density Residential	R-1, Residential Single-Family RP-1, Planned Residential Single-Family	Single-Family Subdivision, Gleason Glen, Sixth Plat
	Low Density Residential	R1, Residential Single-Family	Single-Family, Belmont Estates
	Low Density Residential	R1, Residential Single-Family	Undeveloped
	Low Density Residential	R1, Residential Single-Family	Undeveloped

The proposed use is compatible in context of the existing land uses. The proposed use of the site is changing from six possible single-family homes to two single-family homes. The existing lots on the northernmost portion of Belmont Drive are within the Flood Zone X and include stream buffer, which makes these existing lots undevelopable in their current form. The applicant requests to combine the six current lots into two developable lots at the end of Belmont Drive.

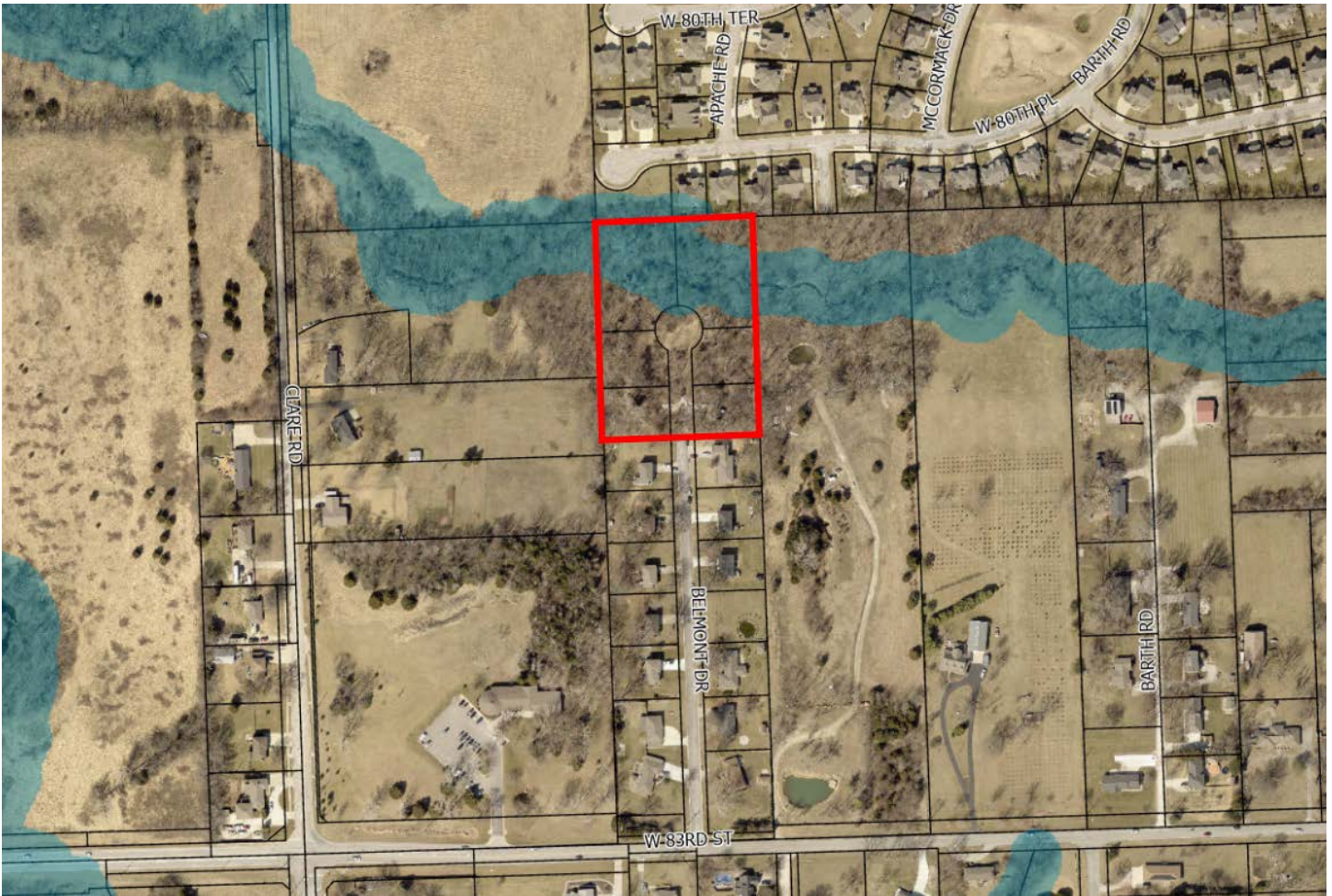


Exhibit 3: Stream Buffer on Subject Area and Surrounding Properties.

PRELIMINARY PLAT REVIEW

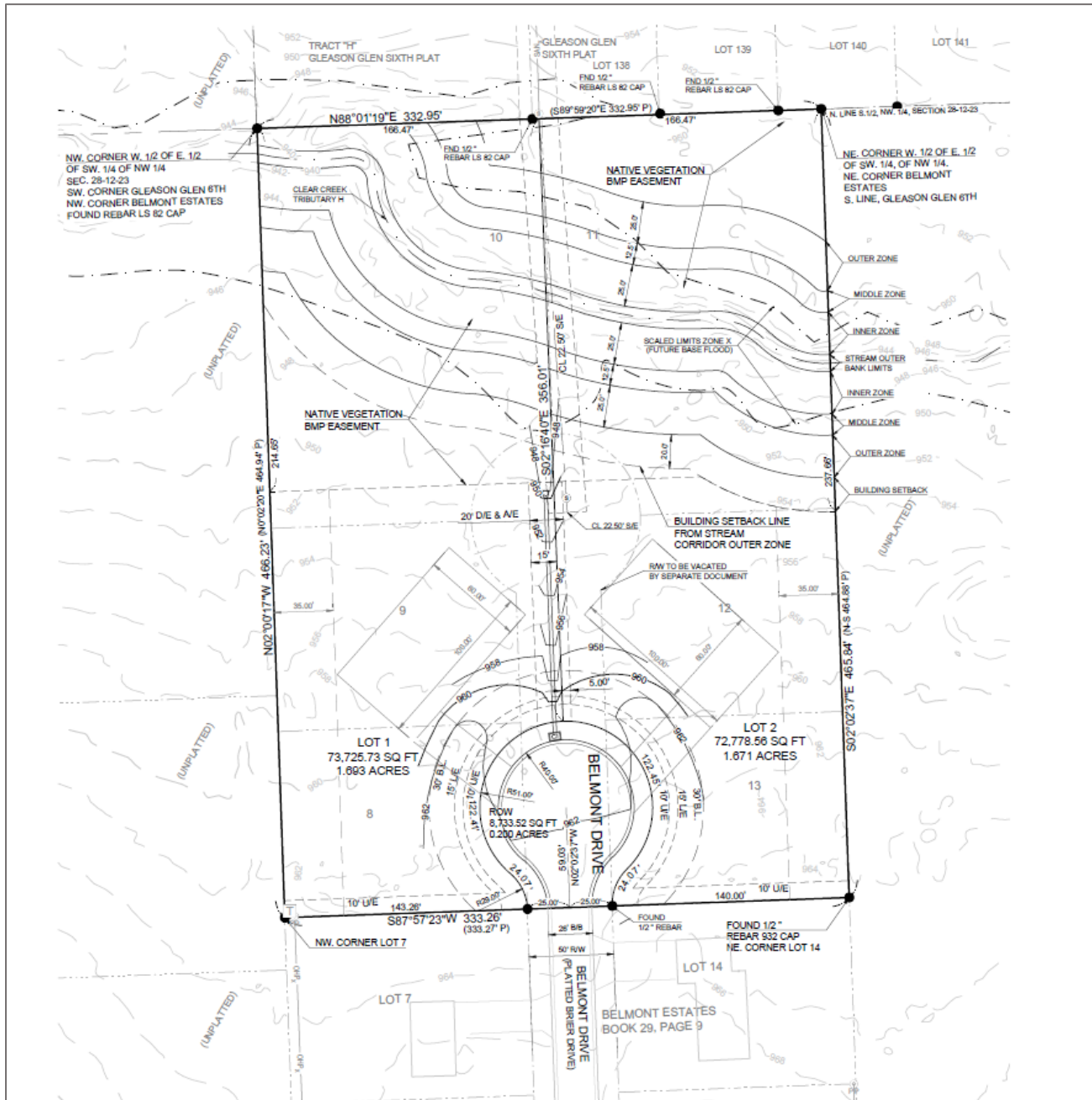


Exhibit 4: Preliminary Plat.

The two-lot preliminary plat is on 3.56 acres at the north end of Belmont Drive. This is the replat and subdivision of Lots 8, 9, 10, 11, 12, and 13 of Belmont Estates for two single-family lots and the relocation of the cul-de-sac bulb south on Belmont Drive. Lot 1 is 1.69 acres on the west side of the cul-de-sac on Belmont Drive. Lot 2 is 1.67 acres on the east side of the drive. The preliminary plat shows proposed building footprints for potential single-family homes on these lots. The Clear Creek tributary and stream setback lines are shown on the preliminary plat.

FINAL PLAT REVIEW

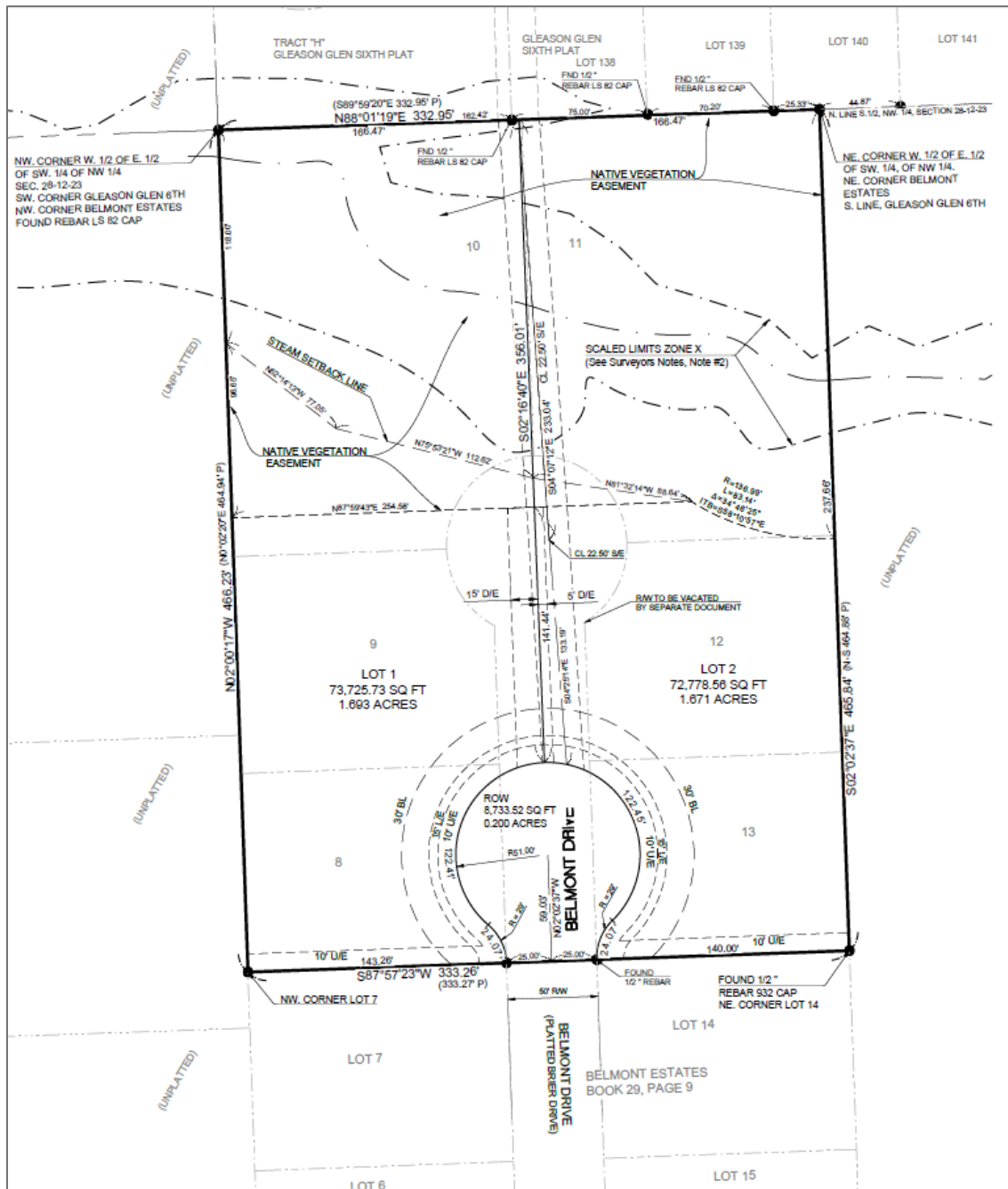


Exhibit 5: Final Plat.

The two-lot preliminary plat is on 3.56 acres at the north end of Belmont Drive. The existing right-of-way is to be vacated separately by separate document. Dedications to the City of Lenexa with the final plat include right-of-way and easements for drainage, utility, and landscaping. Additionally, a private easement is designated on this plat as "Native Vegetation Easement" will be dedicated for the property owners. Trees within the Native Vegetation Easement with a caliper dimension of 6" or larger shall not be removed unless such trees are dead, diseased, or pose a threat to the public or adjacent property. Maintenance of Native Vegetation Easement shall be the responsibility of the owners of the lots within which they are located. Maintenance shall include, but not be limited to removal of dead or diseased trees or trees posing a threat to the public or adjacent property. Development is prohibited within the Native Vegetation Easement.

DEVIATIONS

The applicant is not requesting any deviations.

REVIEW PROCESS

- *The Planning Commission is the final authority for approval of the preliminary and final plat.*
- *This project requires acceptance of right-of-way and easements by the City Council.*
- *Pending an approval from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on February 7, 2023.*
- *The applicant should inquire about additional City requirements, such as permits and development fees.*

RECOMMENDATION FROM PROFESSIONAL STAFF

★ **Staff recommends approval of the Preliminary Plat and Final Plat for Belmont Estates, Second Plat.**

This application is for a replat of Lots 8-13 of Belmont Estates into two new single-family residential lots. The development includes the vacation of the existing right-of-way and the dedication of right-of-way for a new cul-de-sac farther south, the construction of a new public street, and related infrastructure improvements. This project is consistent with Lenexa's goals through **Responsible Economic Development** to create **Vibrant Neighborhoods** and **Inviting Places**.

PRELIMINARY PLAT

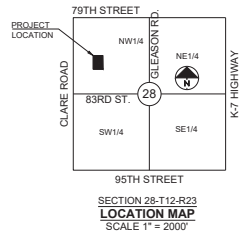
Staff recommends **approval** of the preliminary plat for PT23-01PR – **Belmont Estates, Second Plat** at 8110 Belmont Drive, for a two-lot single-family residential plat.

FINAL PLAT

Staff recommends **approval** of the final plat for PT23-02F - **Belmont Estates, Second Plat** at 8110 Belmont Drive, for a two-lot single-family residential plat.

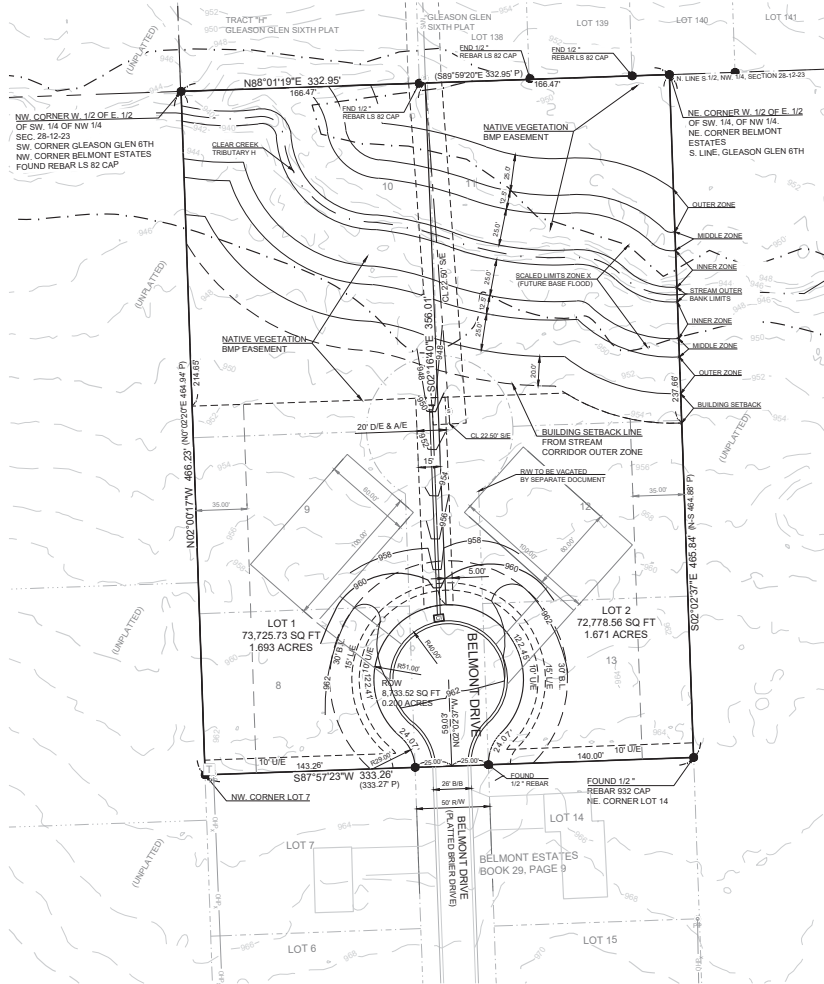
PRELIMINARY PLAT OF BELMONT ESTATES SECOND PLAT

A REPLAT AND SUBDIVISION OF ALL OF LOTS 8, 9, 10, 11, 12 and
13, AND PART OF BRIER DRIVE, BELMONT ESTATES
IN THE NW 1/4 OF SEC. 28-12-23 IN THE CITY OF LENEXA,
JOHNSON COUNTY, KANSAS



LEGEND:

- FOUND 1/2" REBAR WITH KSL S 54 CAP UNLESS OTHERWISE NOTED
- FOUND MONUMENT AS NOTED
- SET 1/2" REBAR WLS-54 CAP UNLESS OTHERWISE NOTED
- BL - BUILDING LINE
- R/W - RIGHT-OF-WAY
- L/E - LANDSCAPE EASEMENT
- S/E - STREET EASEMENT
- U/E - UTILITY EASEMENT
- (P) - PLATTED
- DR - DRAINAGE EASEMENT
- A/E - ACCESS EASEMENT
- (T) - TELEPHONE PEDESTAL
- UT - UTILITY POLE
- OP - OVERHEAD POWER LINE
- CG - CURB & GUTTER
- EXISTING LOT AND PROPERTY LINES



100-YEAR OVERFLOW SWALE CHANNEL									
SECTION	DRAINAGE AREA (AC.)	Q ₁₀₀ (CFS)	BED SLOPE (%)	BASE WIDTH (FT.)	SIDE SLOPE	TOP WIDTH (FT.)	NORMAL DEPTH (FT.)	VELOCITY (FPS)	VELOCITY HEAD (FT.)
A	1.89	12.47	6.66	5	4:1	7.82	0.35	5.52	0.47

DESCRIPTION:

A replat of all of Lots 8, 9, 10, 11, 12 and 13, and a part of Briar Drive adjacent to 8, 9, 10, 11, 12 and 13, BELMONT ESTATES, a subdivision in the City of Lenexa, Johnson County, Kansas, according to the recorded plat thereof. And containing 3.5638 acres more or less.

DEDICATION:

The undersigned proprietor of the above described tract of land has caused the same to be subdivided in the manner shown on the accompanying plat which hereafter shall be known as "BELMONT ESTATES SECOND PLAT".

The streets and roads shown on this plat and not heretofore dedicated as thoroughfares are hereby dedicated to the City of Lenexa, Johnson County, Kansas.

An easement or license to enter upon, locate, construct and maintain or authorize, the location, construction or maintenance and use of conduits, pipes, inlets, manholes, surface drainage facilities and other similar facilities, upon, over, and under those areas outlined and designated on this plat as "Drainage Easements" or "D/E," is hereby granted to the City of Lenexa, Kansas. Drainage Easements shall remain free of fences, shrubs, trees and other obstacles that would restrict the flow of drainage.

The City of Lenexa is under no duty or obligation to maintain or repair the stormwater drainage facilities placed within the limits of this easement, and in no event shall this easement be construed to impose any such obligation on the City of Lenexa.

An easement or license to lay, construct, maintain, alter, repair, replace and operate one or more sewer lines and all appurtenances convenient for the collection of sanitary sewage, together with the right of ingress and egress, over and through those areas designated as "Sanitary Sewer Easement" or "S/E" on this plat, together with the right of ingress and egress over and through adjoining land as may be reasonably necessary to access said easement and is hereby dedicated to the Consolidated Main Sewer District of Johnson County, Kansas or their assigns. Alteration of land contours will be permitted only with the express written approval of JCM. Any placing of improvements or planting of trees on said permanent right-of-way will be done at the risk of subsequent damage thereto without compensation therefor.

An easement or license to enter upon, locate, construct and maintain or authorize, the location, construction or maintenance and use of conduits, pipes, poles, wires, drainage facilities, sanitary sewers, ducts and cables, and similar facilities, upon, over and under those areas outlined and designated on this plat as "Utility Easement" or "U/E" is hereby granted to the City of Lenexa, Kansas. No above ground facilities associated with the uses herein permitted shall be constructed, located, or maintained in any area of the "U/E" that crosses or intersects with a "Drainage Easement" or "D/E".

A 10 foot wide Utility Easement or "U/E" is hereby dedicated to the City of Lenexa, Johnson County, Kansas, or its designees, off of lots or tracts adjacent to and parallel with all proposed interior Street Right-of-Way lines.

A 15 foot wide Landscape Easement or "L/E" is hereby dedicated to the City of Lenexa, Johnson County, Kansas, off of lots adjacent to and parallel with all Street lines. Maintenance of the "L/E" within Lots shall be the responsibility of the Homeowner. Maintenance of the "L/E" within Tracts shall be the responsibility of the development home owner association. In the event the HOA no longer lawfully exists or fails to maintain these tracts, then maintenance shall be the responsibility, shared equally, of the owners of property that are part of this subdivision, whether herein platted or by joining the subdivision in a later development phase.

An easement or license is hereby granted to the City of Lenexa, Johnson County, Kansas, to enter upon, construct and maintain pipes, inlets, manholes, surface drainage facilities, drainage ditches, drainage channels or water courses, other drainage facility tributary connections and appurtenant work relative to storm water drainage upon, over, or under the areas outlined and designated on this plat as "Stormwater Quality / Quantity Easement" or "BMPE".

RESTRICTIONS:

The use of all Lots and Tracts of land in this subdivision shall hereafter be subject to the covenants and restrictions which will be filed at the office of the Register of Deeds of Johnson County, Kansas and shall hereby become a part of the dedication of this plat as though fully set herein.

The "Stormwater Quality / Quantity Easement" or "BMPE" is to be maintained by the property owners and their successors and assigns.

This plat shall not be filed by the Register of Deeds unless filing is within 730 calendar days after the City of Lenexa Governing Body approval date, inclusive.

The ground floor area classification for lots in this plat is Class "E".

SURVEYORS NOTES:

- BELMONT ESTATES, recorded in book 29 at page 9, Johnson County Register of Deeds.
- Flood Note: A portion of this property lies within Flood ZONE X, defined as areas determined to be outside the 0.2% annual chance floodplain. And a portion of this property lies within Zone X (Future Base Flood), defined as Areas of 1% annual chance flood based on future conditions hydrology. No Base Flood Elevations determined. As shown on the Flood Insurance Rate Map 2009IC0002G, revised August 3, 2009 prepared by the Federal Emergency Management Agency. The limits of the Zone X (Future Base Flood) as shown on this survey have been scaled from this map.
- Area: Surveyed area Lots 8-13: ±155237.8 square feet or 3.564 acres ±. Area includes platted right of way for Belmont Drive (Brier Drive platted) adjacent to Lots 8-13.
- Zoning: R-1 (Single Family Residential). Zoning codes for Lenexa as defined on Johnson County AIMS Mapping.
- Basin of bearings is the Plat of GLEASON GLEN, SIXTH PLAT as recorded in Johnson County Register of deeds in Book 20709 at Page 003574. North line of the South One-Half of the Northwest One-Quarter bearing North 88 degrees 01 minutes 19 seconds West.
- Property is subject to Declarations of Restrictions for BELMONT ESTATES as recorded in Book 186 at Page 207 in the Johnson County Register of Deeds.
- Property is Subject to Commitment for Title Insurance, Commitment No. 15100427 prepared by Continental Title Company dated October 12, 2015.
- All Construction is proposed to be completed in single phase.

The use of all Lots and Tracts of land in this subdivision shall hereafter be subject to the covenants and restrictions which will be filed at the office of the Register of Deeds of Johnson County, Kansas and shall hereby become a part of the dedication of this plat as though fully set herein.

The "Stormwater Quality / Quantity Easement" or "BMPE" is to be maintained by the property owners and their successors and assigns.

The ground floor area classification for lots in this plat is Class "E".

SITE DEVELOPMENT DATA PANEL:

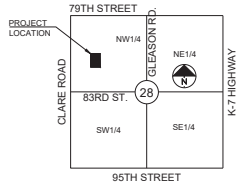
Site Area:	3.56 acres
Total Lot Coverage (Buildings & Pavement):	20,000 sf (110,000 per lot)
Density:	0.50 units/acre
Open Space Percentage:	47% preserved native



DATE	11-28-2022	PRELIMINARY PLAT OF BELMONT ESTATES SECOND PLAT
DRAWN BY	SCH	
CHECKED BY	AR	
PROJ. NO.	15-226	SHEET NO. 1

FINAL PLAT OF BELMONT ESTATES SECOND PLAT

A REPLAT AND SUBDIVISION OF ALL OF LOTS 8, 9, 10, 11, 12 and
13, AND PART OF BRIER DRIVE, BELMONT ESTATES
IN THE NW 1/4 OF SEC. 28-12-23 IN THE CITY OF LENEXA,
JOHNSON COUNTY, KANSAS



SECTION 28-12-R23
LOCATION MAP
SCALE 1" = 2000'

LEGEND:

- FOUND 1/2" REBAR WITH KSLS 54 CAP UNLESS OTHERWISE NOTED
- FOUND MONUMENT AS NOTED
- SET 1/2" REBAR W/LS-54 CAP UNLESS OTHERWISE NOTED
- BL BUILDING LINE
- RW RIGHT-OF-WAY
- LE LANDSCAPE EASEMENT
- SE STREET EASEMENT
- UE UTILITY EASEMENT
- (P) PLATTED
- DE DRAINAGE EASEMENT
- AE ACCESS EASEMENT



SCALE: 1" = 40'

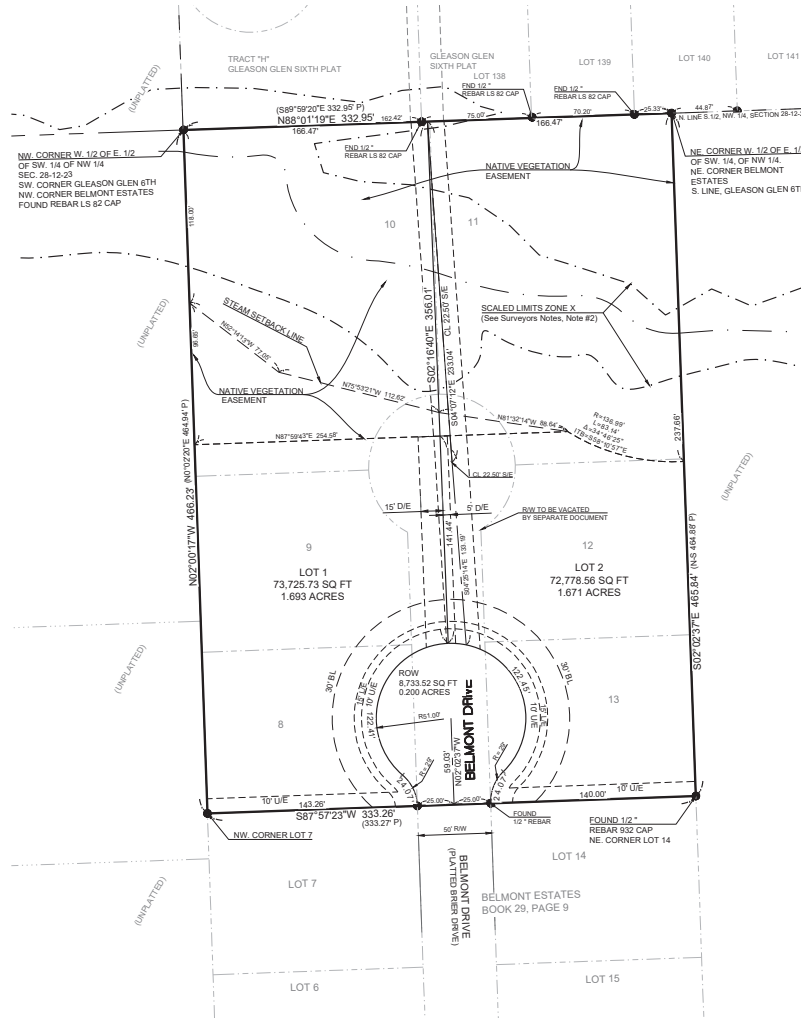
SURVEYORS NOTES:

- BELMONT ESTATES, recorded in book 29 at page 9, Johnson County Register of Deeds.
- Flood Note: A portion of this property lies within flood ZONE X, defined as areas determined to be outside the 0.2% annual chance floodplain. And a portion of this property lies within Zone X (Future Base Flood), defined as Areas of 1% annual chance flood based on future conditions hydrology. No Base Flood Elevations determined. As shown on the Flood Insurance Rate Map 20091C0032G, revised August 3, 2009 prepared by the Federal Emergency Management Agency. The limits of the Zone X (Future Base Flood) as shown on this survey have been scaled from this map.
- Area: Surveyed area Lots 8-13: ±155237.8 square feet or 3.564 acres ±. Area includes platted right of way for Belmont Drive (Brier Drive platted) adjacent to Lots 8-13.
- Zoning: R-1 (Single Family Residential). Zoning codes for Lenexa as defined on Johnson County AIMS Mapping.
- Basis of bearings is the Plat of GLEASON GLEN, SIXTH PLAT as recorded in Johnson County Register of Deeds in Book 200709 at Page 003574. North line of the South One-Half of the Northwest One-Quarter bearing North 88 degrees 01 minutes 19 seconds West.
- Property is subject to Declarations of Restrictions for BELMONT ESTATES as recorded in Book 186 at Page 207 in the Johnson County Register of Deeds.
- Property is Subject to Commitment for Title Insurance, Commitment No. 15100427 prepared by Continental Title Company dated October 12, 2015.

I HEREBY CERTIFY THIS PLAT WAS PREPARED UNDER MY DIRECT SUPERVISION BASED ON A FIELD SURVEY PERFORMED IN NOVEMBER 2015 AND NOVEMBER 2022. THE DETAILS SHOWN ARE TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.



Aaron T. Reuter - Land Surveyor
KSLS-1429



DESCRIPTION:

A replat of all of Lots 8, 9, 10, 11, 12 and 13, and a part of Brier Drive adjacent to 8, 9, 10, 11, 12 and 13, BELMONT ESTATES, a subdivision in the City of Lenexa, Johnson County, Kansas, according to the recorded plat thereof. And containing 3.5638 acres more or less.

DEDICATION:

The undersigned proprietor of the above described tract of land has caused the same to be subdivided in the manner shown on the accompanying plat which hereafter shall be known as "BELMONT ESTATES SECOND PLAT".

The streets and roads shown on this plat and not heretofore dedicated as thoroughfares are hereby dedicated to the City of Lenexa, Johnson County, Kansas.

An easement or license to enter upon, locate, construct and maintain or authorize, the location, construction or maintenance and use of conduits, pipes, inlets, manholes, surface drainage facilities and other similar facilities, upon, over, and under those areas outlined and designated on this plat as "Drainage Easement" or "DE", is hereby granted to the City of Lenexa, Kansas. Drainage Easements shall remain free of fences, shrubs, trees and other obstacles that would restrict the flow of drainage.

The City of Lenexa is under no duty or obligation to maintain or repair the stormwater drainage facilities placed within the limits of this easement, and in no event shall this easement be construed to impose any such obligation on the City of Lenexa.

An easement or license to lay, construct, maintain, alter, repair, replace and operate one or more sewer lines and all appurtenances convenient for the collection of sanitary sewage, together with the right of ingress and egress, over and through those areas designated as "Sanitary Sewer Easement" or "SSE" on this plat, together with the right of ingress and egress over and through adjoining land as may be reasonably necessary to access said easement and is hereby dedicated to the Consolidated Main Sewer District of Johnson County, Kansas or their assigns. Alteration of land contours will be permitted only with the express written approval of JCW. Any placing of improvements or planting of trees on said permanent right-of-way will be done at the risk of subsequent damage thereto without compensation thereof.

An easement or license to enter upon, locate, construct and maintain or authorize the location, construction or maintenance and use of conduits, pipes, poles, wires, drainage facilities, sanitary sewers, ducts and cables, and similar facilities, upon, over and under those areas outlined and designated on this plat as "Utility Easement" or "UE" is hereby granted to the City of Lenexa, Kansas. No above ground facilities associated with the uses herein permitted shall be constructed, located, or maintained in any area of the "UE" that crosses or intersects with a "Drainage Easement" or "DE".

A 15 foot wide Utility Easement or "UE" is hereby dedicated to the City of Lenexa, Johnson County, Kansas, off of lots or tracts adjacent to and parallel with all proposed Interior Street Right-of-Way lines.

A 15 foot wide Landscape Easement or "LE" is hereby dedicated to the City of Lenexa, Johnson County, Kansas, off of lots adjacent to and parallel with all Street lines. Maintenance of the "UE's" within Lots shall be the responsibility of the Homeowner.

A private easement or license outlined and designated on this plat as "Native Vegetation Easement" is hereby dedicated for the property owners. Trees within the Native Vegetation Easement with a caliper dimension of 6" or larger shall not be removed unless such trees are dead, diseased, or pose a threat to the public or adjacent property. Maintenance of Native Vegetation Easement shall be the responsibility of the owners of the lots within which they are located. Maintenance shall include, but not be limited to removal of dead or diseased trees or trees posing a threat to the public or adjacent property. Development is prohibited within the Native Vegetation Easement.

RESTRICTIONS:

The use of all Lots and Tracts of land in this subdivision shall hereafter be subject to the covenants and restrictions which will be filed at the office of the Register of Deeds of Johnson County, Kansas and shall hereby become a part of the dedication of this plat as though fully set herein.

This plat shall not be filed by the Register of Deeds unless filing is within 730 calendar days after the City of Lenexa Governing Body approval date, inclusive.

The ground floor area classification for lots in this plat is Class "D".

CONSENT TO LEVY:

The undersigned proprietor of the above described land hereby consents and agrees that the Board of County Commissioners of Johnson County, Kansas, and the City of Lenexa, Johnson County, Kansas, shall have the power to release such land proposed to be dedicated for public use from the lien and effect of any special assessments, and that the amount of the unpaid special assessments on such land dedicated shall become and remain a lien on the remainder of this land fronting and abutting on such dedicated public way or thoroughfare.

APPROVALS:

Approved by the Planning Commission of the City of Lenexa, Johnson County, Kansas, this ___ day of ___, 20__.

Chris Poss, Chairman

Approved by the Governing Body of the City of Lenexa, Johnson County, Kansas, this ___ day of ___, 20__.

Michael A. Boehm, Mayor

Jennifer Martin, City Clerk

ACKNOWLEDGMENT:

IN TESTIMONY WHEREOF, the undersigned proprietors have caused this instrument to be executed this ___ day of ___, 20__.

Bryan P Rapp

Monica N Toepfer

STATE OF KANSAS)

COUNTY OF JOHNSON)

THIS INSTRUMENT was acknowledged before me on this ___ day of ___, 20__ by

Notary Public

My Appointment Expires



1402 West 107th Street • Lenexa, Kansas 66151
Ph: (913) 492-6158 • Fax: (913) 492-6402 • WWW.SCHLAGEL-ASSOCIATES.COM
Kansas State Certified Public Accountant
KS-200-063629-06-54

DATE 4-26-2022
DRAWN BY SCH
CHECKED BY AR
PROJ. NO. 15-226

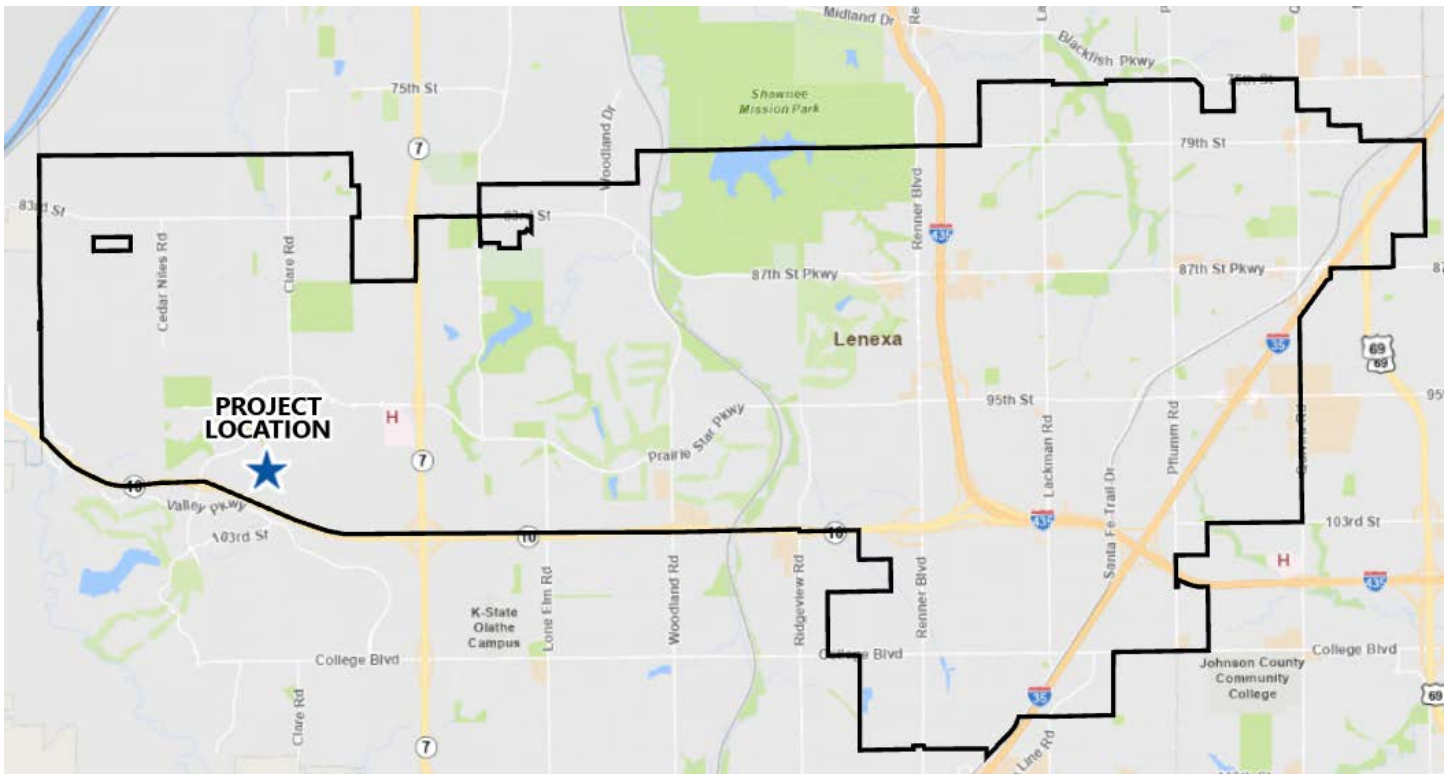
FINAL PLAT OF
BELMONT ESTATES
SECOND PLAT

SHEET NO. 1

REV: 11-29-2022

CEDAR CANYON WEST

Project #:	RZ22-09 & PL22-04CP	Location:	Northeast corner of Canyon Creek Boulevard and K-10 Highway
Applicant:	Dan Foster, Schlagel & Associates	Project Type:	Rezoning/Concept Plan
Staff Planner:	Christa McGaha, AICP	Proposed Use:	Commercial, Residential, and Industrial



REMAND SUMMARY

The Lenexa Planning Commission completed a public hearing for said applications on November 7, 2022 and submitted its recommendation for approval of all applications to the City Council. After a staff presentation, applicant presentation, and public comment on the applications on December 6, 2022, the City Council voted unanimously to return both applications to the Planning Commission for further discussion, review, and consideration of the following specific items related to the project:

1. Discuss and consider whether the applicant's proposed density and concept plan for the multi-family residential component of the development, known as Tract 4, could be accommodated by a lesser zoning district, particularly the RP-4 District; and
2. Discuss truck traffic as it relates to the business park/industrial component of the development, known as Tract 7, and whether the concept plan should include a condition that this proposed land use is supportable only if/when appropriate street infrastructure and truck routes are available to serve the project. Specifically discuss whether the interchange of Clare Road and K-10 Highway will be necessary to support the project.

STAFF RECOMMENDATION: APPROVAL

PROPOSED CHANGES TO TRACT 4

The issue at hand is that Tract 4 was submitted as a request for rezoning to RP-5 when the proposed density of just over 13 du/acre falls within the RP-4 density range. The applicant represented that the RP-5 Zoning District was proposed because it provided greater building height allowance and the applicant anticipates two buildings will exceed the RP-4 height limitations due to the topography of the site. At the City Council meeting, an alternative was discussed that would alter Tract 4's rezoning request from RP-5 to RP-4 to align better with the density range of RP-4 and be paired with a height deviation to mitigate the applicant's concern about the maximum height limitations within the RP-4 Zoning District. The applicant appeared to be in agreement with returning the application to the Planning Commission and subsequently revised the rezoning request and concept plan to align with the City Council's direction.

TABLE 1: COMPARISON OF SUBMITTALS FOR TRACT 4

Site Information	Original Submittal	Current Remand Submittal
Proposed Zoning	RP-5	RP-4
Maximum Building Height	48 ft	35 ft and a deviation for two buildings to have 3-4 stories and a maximum height of up to 48 ft
Number of Dwelling Units	276-300	300
Proposed Density	Approx. 12.02 - 13 du/ac	13.07 du/ac

DEVIATION

The applicant requests a height deviation to allow two buildings to exceed the maximum allowable height in the RP-4 Zoning District. The increase in height would only apply to the east portion of two buildings near the southeast corner of Tract 4. The maximum proposed building height for these two buildings is 48 feet (4 stories). The buildings would be split with three stories on the west side and four stories on the east side.



Exhibit 1: Updated Site Plan for Tract 4 Showing Proposed Four-Story Height Deviations (Orange)

TRUCK TRAFFIC ANALYSIS FOR TRACT 7

The issue at hand is whether a condition should be attached to the concept plan for Tract 7, which reflects heavy truck warehousing/logistics uses, that prohibits development until the Clare Road/K-10 Highway interchange is constructed or at least inevitable.

All development must meet minimum street standards and Tract 7 is no exception. Trucks are typically assigned to main arterial streets for the most direct route to the state highway system. The following exhibit reflects the various ways that trucks could navigate to either K-7 Highway or K-10 Highway with and without a future interchange at Clare Road and K-10.

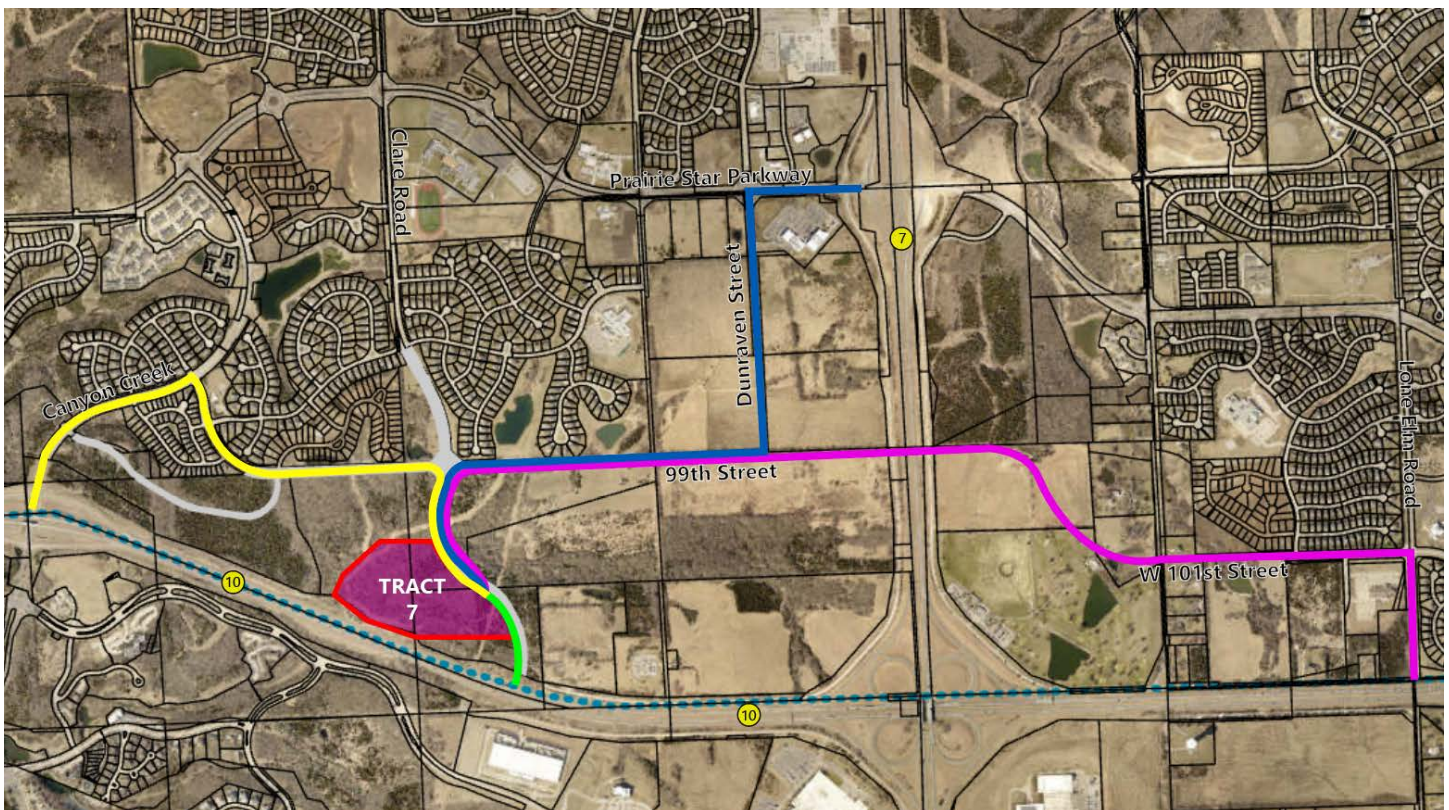


Exhibit 2: Potential Truck Traffic Routes

- **Green:** The truck route in the event the Clare Road/K-10 Highway interchange is constructed.
- **Yellow:** The truck route in the event the Clare Road/K-10 Highway interchange is not constructed.
- **Blue:** The preferred truck route once Future 99th Street and Future Dunraven Street segments are completed if the Clare Road/K-10 Highway interchange is not constructed.
- **Magenta:** The preferred truck route once Future 99th Street, the K-7 flyover bridge, Future 101st Street, and the Lone Elm Road/K-10 Highway interchange are constructed if the Clare Road/K-10 Highway interchange is not constructed.

Staff concludes that future truck routes will be driven by the timeline for infrastructure improvements in this region of Lenexa. Each of these routes depends on road segments or highway interchanges which have not been constructed and of which timelines are uncertain. It is likely that the yellow route will be the most feasible until the proper road infrastructure is in place for the green, magenta, and blue truck routes to be utilized.

REVIEW PROCESS

- *This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on February 7, 2023.*
- *The applicant should inquire about additional City requirements, such as permits and development fees.*

RECOMMENDATION FROM PROFESSIONAL STAFF

★ Staff recommends approval of the Rezoning and Concept Plan for Cedar Canyon West.

This project is consistent with Lenexa's goals through **Responsible Economic Development** to create **Vibrant Neighborhoods** and **Inviting Places**.

REZONING

Staff recommends **approval** of rezoning property from AG and CP-2 to CP-2, RP-2, RP-4, CP-O and BP-2 for RZ22-09 – **Cedar Canyon West** at the northeast corner of Canyon Creek Boulevard and K-10 Highway.

CONCEPT PLAN

Staff recommends **approval** of the concept plan for PL22-04CP – **Cedar Canyon West** at the northeast corner of Canyon Creek Boulevard and K-10 Highway, for commercial, residential, office, and manufacturing land uses with the following deviation:

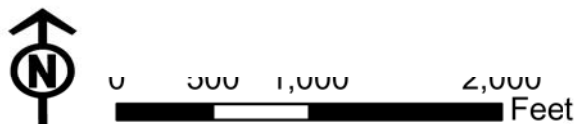
1. A building height deviation for the two easternmost buildings of Tract 4 to allow a maximum height of 48 feet (four stories) on the east sides of the buildings.



Document Path: C:\Users\cdchrista\Documents\ArcGIS\Projects\City Council Template\City Council Template.aprx

Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Cedar Canyon West RZ22-09 & PL22-04CP



MEETING DATE: November 7, 2022 (Continued from the October 3, 2022 meeting)
PUBLIC HEARING REQUIRED

REGULAR AGENDA ITEM: 8 a. & b.

PROJECT TITLE: Cedar Canyon West

PROJECT # / REQUEST: RZ22-09 & PL22-04CP – Rezoning & Concept Plan

APPLICANT & DESIGN PROFESSIONAL:
Dan Foster, Schlagel Associates

OWNER:
Speedway II, LLC.

PROPERTY LOCATION:
Located approximately at the northeast corner of
Canyon Creek Boulevard and K-10 Highway

STAFF PLANNER:
Christa McGaha, AICP

SUMMARY OF RECOMMENDATION

Staff recommends **APPROVAL** of the rezoning for Cedar Canyon West located approximately at the northeast corner of Canyon Creek Boulevard and K-10 Highway from the AG, Agricultural and the CP-2₁ Planned Community Commercial Zoning Districts to CP-2, Planned Community Commercial; RP-2, Planned Residential (Intermediate-Density); RP-5, Planned Residential (High-Density); CP-O, Planned General Office; and BP-2, Planned Manufacturing Zoning District.

Staff recommends **APPROVAL** of the concept plan for Cedar Canyon West.

PROPOSED PROJECT DESCRIPTION AND BACKGROUND INFORMATION

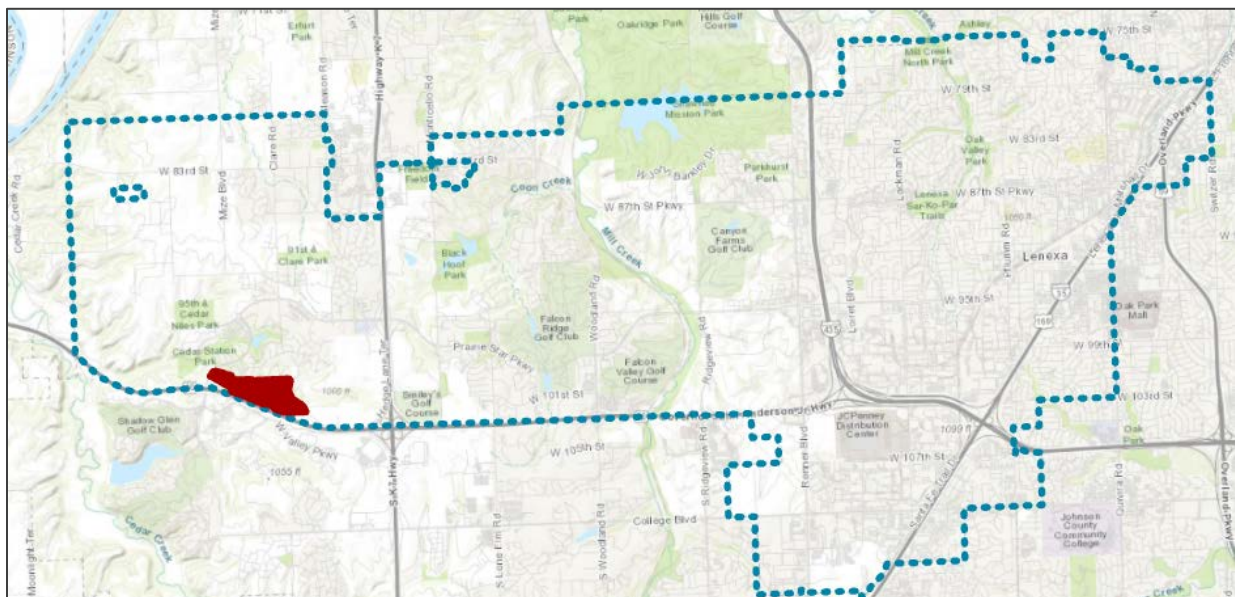


Exhibit 1, Subject Site Location in Context of the City of Lenexa City Limits.

The site is located on the northeast corner of the K-10 Highway and Canyon Creek Boulevard interchange in Lenexa. K-10 Highway is along the south boundary, Canyon Creek Boulevard is along the west boundary, future 99th Street is along the north boundary, and future Clare Road is along the east boundary. The site is currently comprised of six complete parcels and portions of three other parcels for an approximate total 112 acres. The 7.2-acre parcel located in the southwest corner is zoned CP-2, Planned Community Commercial. The 0.58-acre parcel located in the northwest corner is zoned CP-2 and is the future right of way for 101st Street. The remainder of the site is zoned AG, Agricultural.

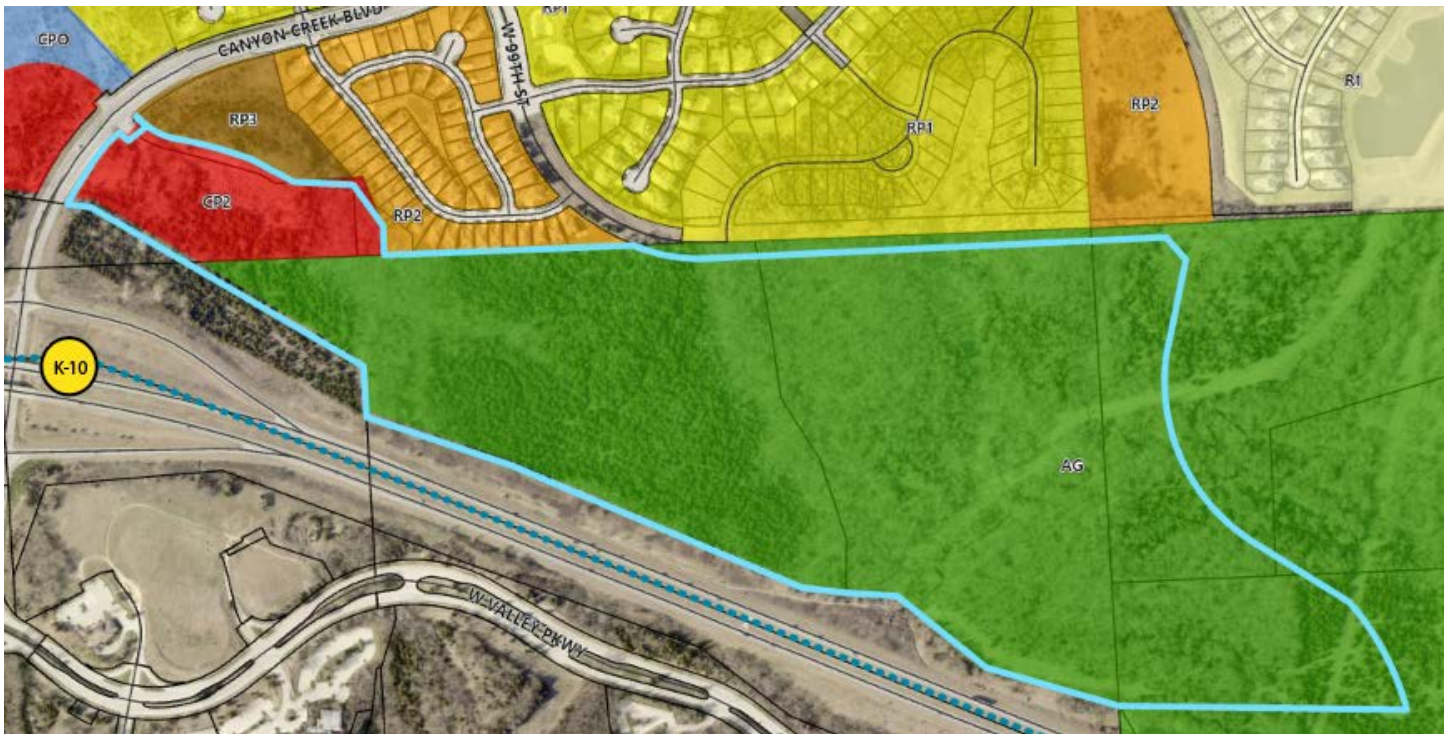


Exhibit 2, Existing Zoning.

The proposed Cedar Canyon West project includes two-family residential, multi-family residential, commercial, office, and business park development on 112 acres. The project includes one rezoning application and one concept plan application reflecting seven sub-area projects.

Concept plans are less refined than preliminary plans. The information provided in a preliminary plan shows building locations and sizes along with the architectural character of the project. Concept plans do not show actual building outlines but provide general information about distribution of land uses and the overall maximum density or square footage of a proposed development. The layout of buildings and parking areas are provided as part of a preliminary plan, which is required as the next step after a concept plan. A preliminary plan application is still required after concept plan approval. Each of the seven sub-area projects is described in detail in the analysis portion of the staff report.

The following exhibits show the Future Land Use Map as it currently exists alongside a version of the Future Land Use Map with each component of the proposed rezoning shown in hatched colors representing the proposed land uses.

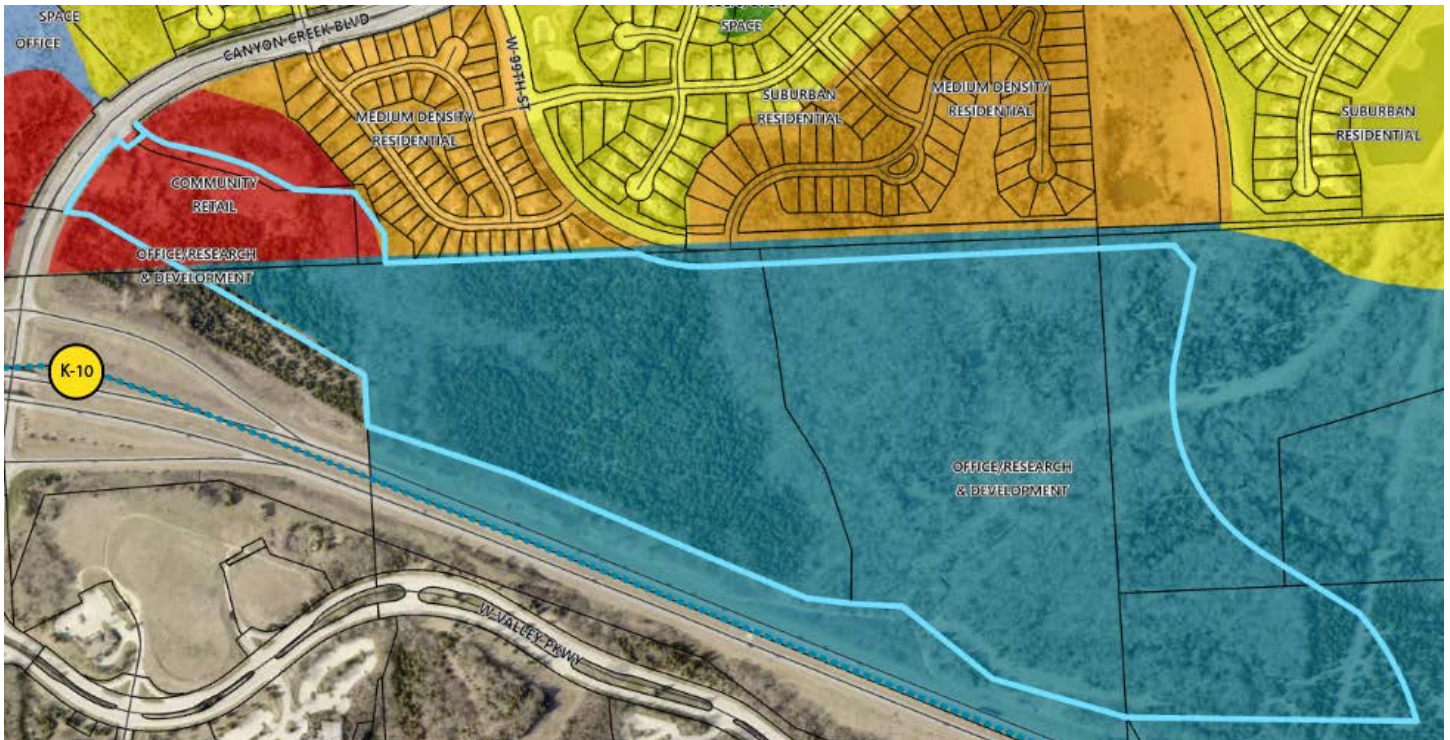


Exhibit 3, Future Land Use Designations from the Comprehensive Plan.

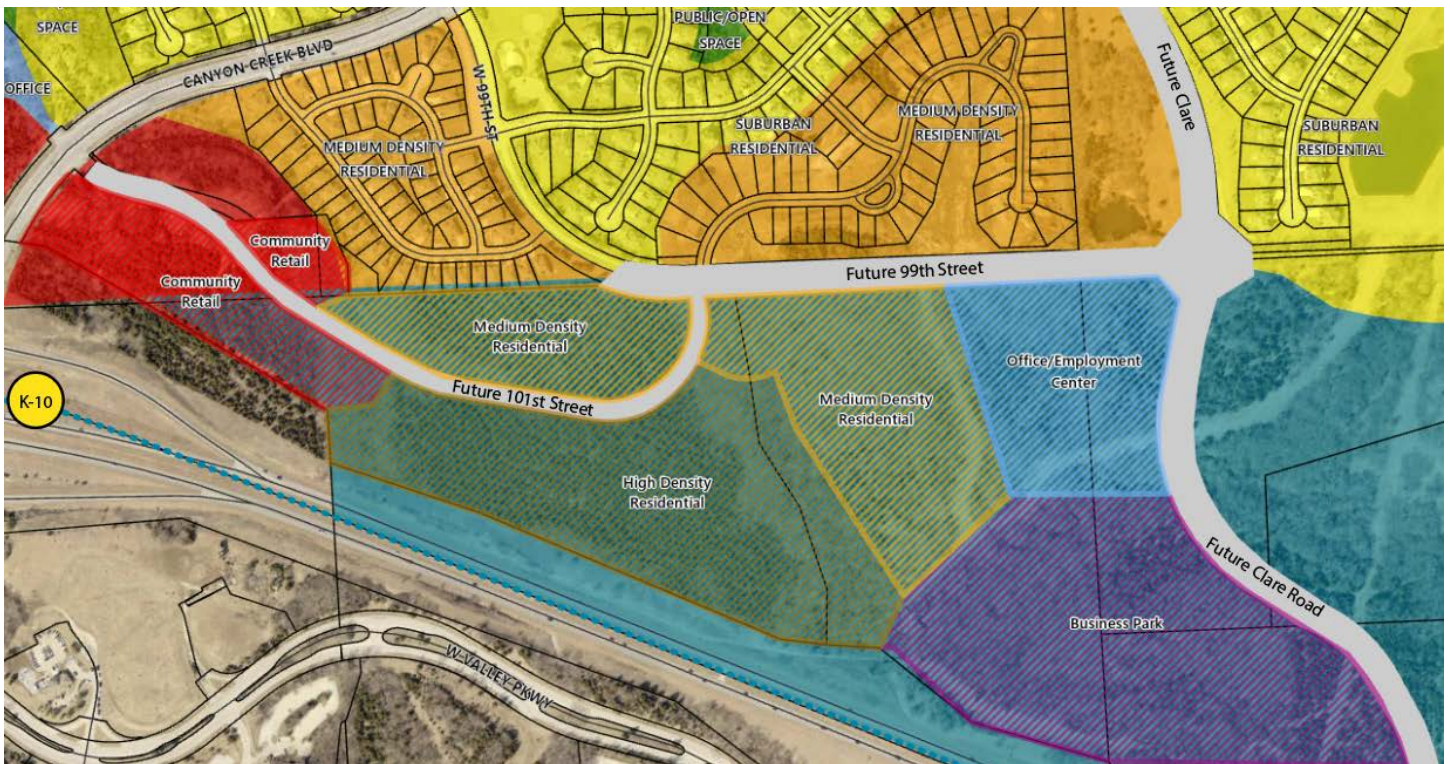


Exhibit 4, Existing and Proposed Future Land Use.

PROJECT COMPONENTS

The overall project is a large development containing 112 acres. There are seven distinct components of the project, and seven separate tracts. The seven tracts are referenced throughout the staff report. The following exhibit shows the seven individual tracts.

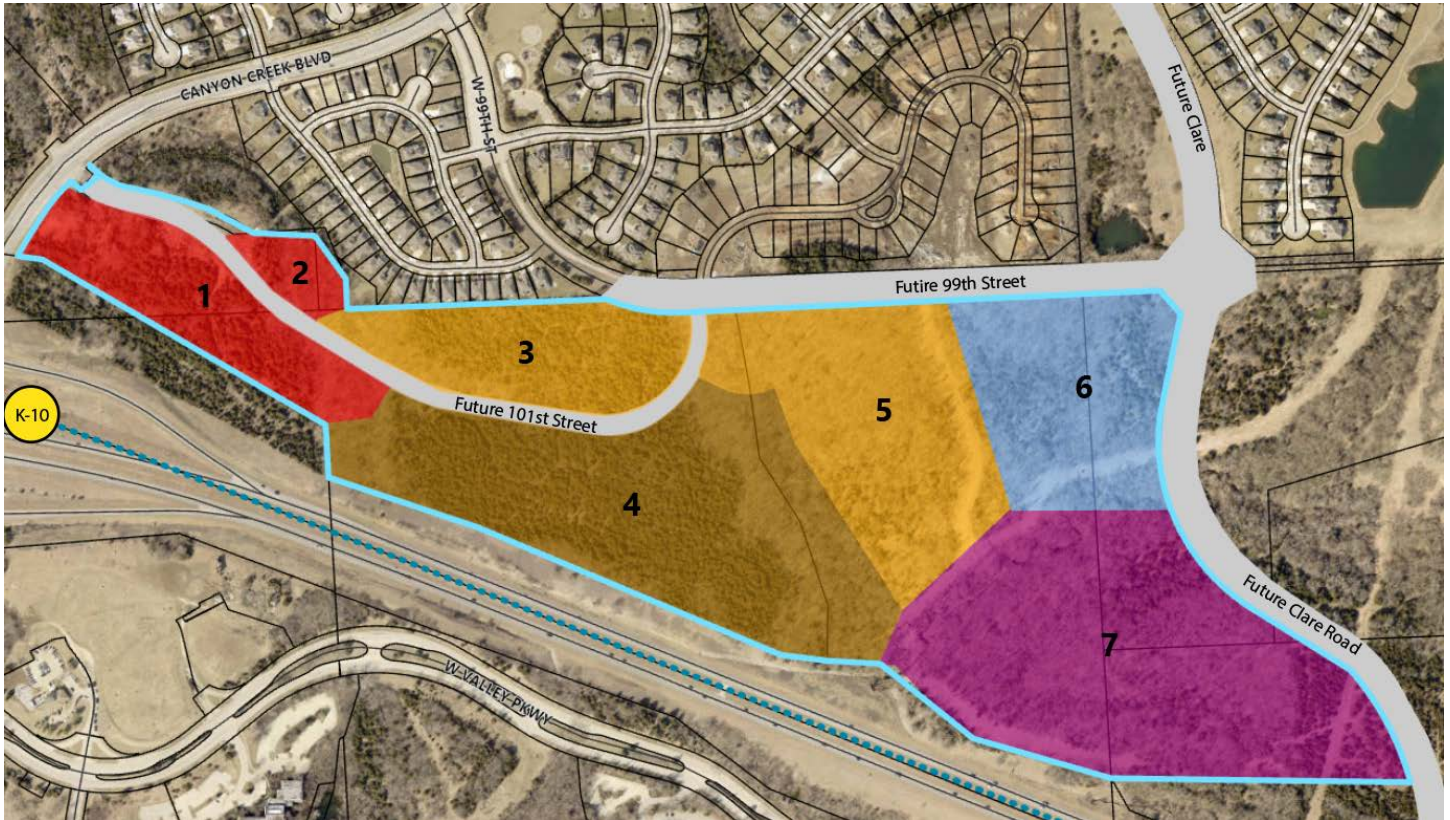


Exhibit 5, Seven Proposed Tracts.

CURRENT ZONING:

CP-2, Planned Community Commercial
AG, Agricultural District

CURRENT USE:

Undeveloped

ACREAGE:

112

PROPOSED ZONING:

CP-2, Planned Community Commercial
RP-2, Planned Residential (Intermediate – Density)
RP-5, Planned Residential (High – Density)
CP-O, Planned General Office
BP-2, Planned Manufacturing

PROPOSED USE:

Community Commercial
Medium Density Residential
High Density Residential
Office
Business Park

STREETS

The submitted plan shows the layout for the future transportation network, including the construction of three new road segments: Future 101st Street, Future 99th Street, and Future Clare Road.

Future 101st Street:

- Classified as a collector street which is for traffic volumes generally less than 10,000 trips per day with the primary function of moving traffic in a small area from adjacent developments to the arterial streets.
- Designed with a through lane in each direction and a center turn lane.
- Intersection at Canyon Creek Boulevard has turn lanes and is possible to be a future signalized intersection if traffic warrants.
- Will be constructed by developer when the adjacent tracts are developed per phasing plan included with future preliminary plans.

Future 99th Street:

- Classified as a minor arterial street which is for traffic volumes of approximately 10,000 - 25,000 trips per day with the primary function of moving traffic in a more localized area of the City.
- Design stage is complete.
- Will be installed by the City with construction starting in early 2023.

Future Clare Road/Interchange:

- Classified as an arterial road which is for traffic volumes above 20,000 trips per day with the primary function of moving traffic across the City and with access to abutting properties being a secondary function.
- Future Clare Road north of 99th Street is a current CIP project being constructed by the City and is anticipated to be completed in 2023.
- Future Clare Road south of 99th Street is to be constructed by the developer and the timeline of this project depends on the road phasing plan approved with the future preliminary plans for development adjacent to Future Clare Road
- The K-10/Clare Road interchange is still on all City and KDOT plans, but the status and timing of this project is uncertain.

The design for the public streets illustrated on this plan are conceptual based on City staff comments. A traffic study shall be submitted with the preliminary development plan. The final design for the public streets shall be based on the traffic study recommendations. With preliminary plan approval, the applicant will be required to show a phasing plan indicating which sections of 101st Street, 99th Street, and Clare Road to be completed with, or prior to, the phase.

STORMWATER

The design and layout shown demonstrates the intent to meet the City's stormwater management requirements. Stream buffers have been preliminarily identified with development staying out of said buffers, and stormwater facilities have been conceptually identified throughout the varied developments, typically with each development providing facilities for that specific development.

COMPREHENSIVE PLAN RECOMMENDATION FOR AREA

Community Commercial Center – Commercial uses with a combined or total gross leasable area of 125,000 to 400,000 square feet that provides services for a 3-to-6-mile trade area radius. Uses often include general merchandise offerings with a wider range of goods than Neighborhood Commercial Centers. Community Centers often have two or more anchor tenants which could include a discount store, supermarket, or large specialty discount store and are developed on land 10 to 40 acres in size.

Office, Research & Development - Uses that combine office with research and related manufacturing or assembly with little or no sales of product. Examples may include medical research spaces, bioscience, technology, or product development testing. Research and Development uses vary widely in terms of what they do, in some cases they are predominantly office with minimal lab or production space, and in others they are more industrial in nature.

COMPREHENSIVE PLAN & UPDATE

The Comprehensive Plan shows the area adjacent to Canyon Creek Boulevard to be *Community Commercial* and the remainder of the areas to be *Office, Research and Development*. The comprehensive plan identifies this area as one of four activity centers. These areas are more active with more concentration of more intense uses. These areas would have a variety of land uses, including different housing choices, commercial, office and cultural uses. The Comprehensive Plan states the mix of uses will generally reflect a stepping of intensity to avoid incompatibilities, and the use of design and buffering techniques to mitigate potential negative impacts of certain adjoining.

Principles stated in the Lenexa Comprehensive Plan include the following:

1. Encourage a mix of residential opportunities for a diverse population
2. Continues to be a desirable business location
3. Provides for a broad range of retail opportunities

The Vision 2040 plan neighborhood vision states the following:

1. We want to be inclusive and have lots of housing choices for people who work here, have different abilities and are from all generations, cultures and backgrounds. We need attainable housing for families, individuals, professionals, hourly workers, entrepreneurs, young people, empty nesters and retirees.
2. Lenexa primarily has single-family detached homes (59%). A variety of housing structure types is important to meet the needs of all residents and future demand.

In summer 2021, the City of Lenexa started the process to update the City's Comprehensive Plan. Staff is navigating the new Comprehensive Plan update and with the update in mind, has analyzed the possibilities for this site. It is Staff's opinion that the site is large enough to constitute a mixture of different land uses. Staff anticipates the Future Land Use (FLU) designation of the subject area to change with the updated Comprehensive Plan to increase diversity of uses in this area. Large scale *Office, Research and Development* uses are not likely to be in high demand over the next twenty years for reasons noted below and so the update process is attempting to acknowledge the changing market conditions in land use planning. This new Comprehensive Plan is projected to be complete the winter of 2022/2023.

REZONING ANALYSIS

The subject area is 112 acres of undeveloped land. The site is zoned almost entirely AG, Agricultural except for approximately 8 acres located at the west end of the site adjacent to Canyon Creek Boulevard that is zoned CP-2, Planned Community Commercial zoning. This was zoned in 2001 as a part of the a concept plan for Canyon Creek (RZ01-07/PL01-01CP) which consisted of 487-acres around Canyon Creek Boulevard. The current FLU map shows the 8-acres of CP-2 zoning as designated for *Community Retail*. The existing CP-2 zoning is in line with this *Community Retail* designation. The applicant proposes to leave the existing CP-2 zoning as-is and expand the zoning district southeast to result in approximately 12.52 gross acres of CP-2 zoning.

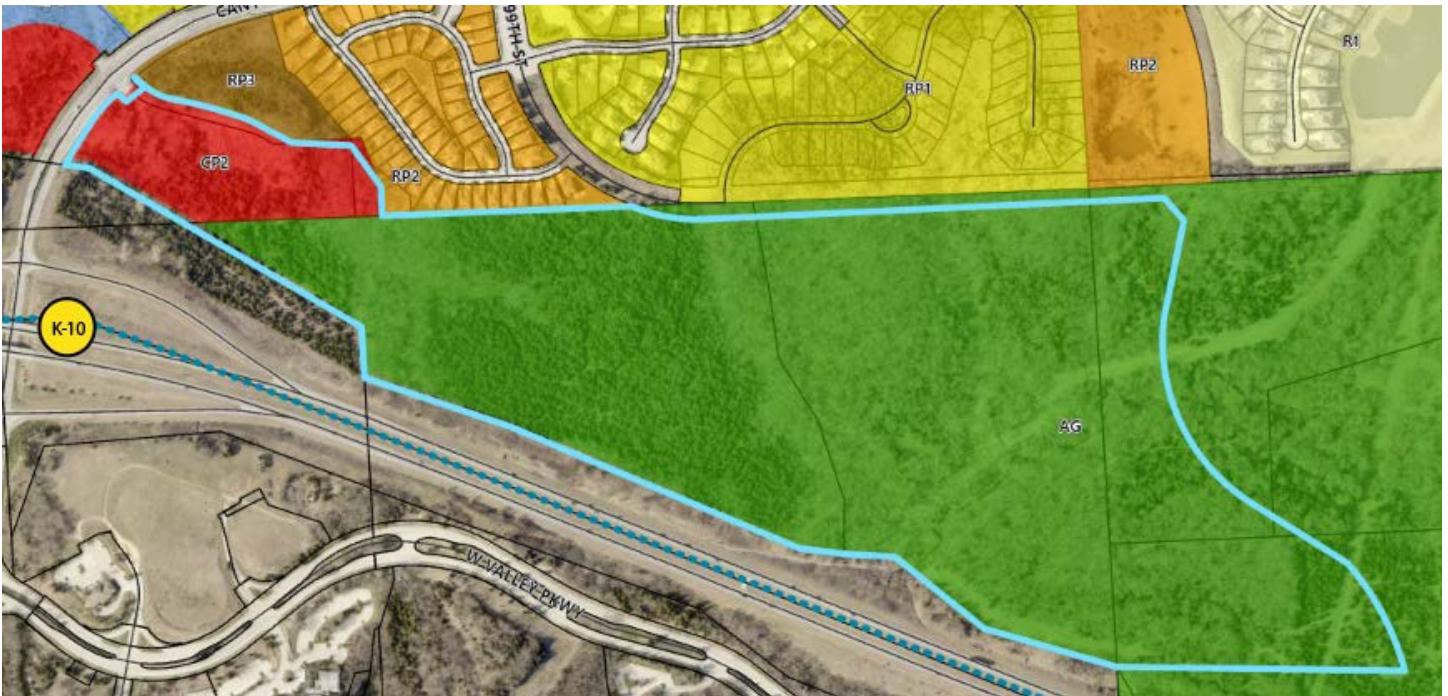


Exhibit 6, Existing Zoning.

The approximate 96-acres currently zoned AG, Agricultural and the entire area is currently designated as future *Office, Research and Development* on the FLU map. Due to current trends, the increased popularity of remote working, and the resulting decline in demand for office space, Staff anticipates the FLU designation of the subject area to change with the updated Comprehensive Plan to result in increased diversity of uses in this area. The applicant is proposing to rezone the 96-acres of AG, Agricultural zoned land to five separate zoning districts separated into seven separate tracts. The proposed Zoning Districts include:

TRACTS 1 & 2	Rezone 4 acres (Tract 1) and .5 acres (Tract 2) of AG, Agricultural to CP-2, Planned Community Commercial to expand the existing CP-2 zoned area
TRACTS 3 & 5	Rezone 10.10 acres (Tract 3) and 16.46 acres (tract 5) of AG, Agricultural to RP-2, Planned Residential (Intermediate-Density)
TRACT 4	Rezone 22.96 acres of AG, Agricultural to RP-5, Planned Residential (High-Density)
TRACT 6	Rezone 13.56 acres of AG, Agricultural to CP-O, Planned General Office
TRACT 7	Rezone 32.24 acres of AG, Agricultural to BP-2, Planned Manufacturing

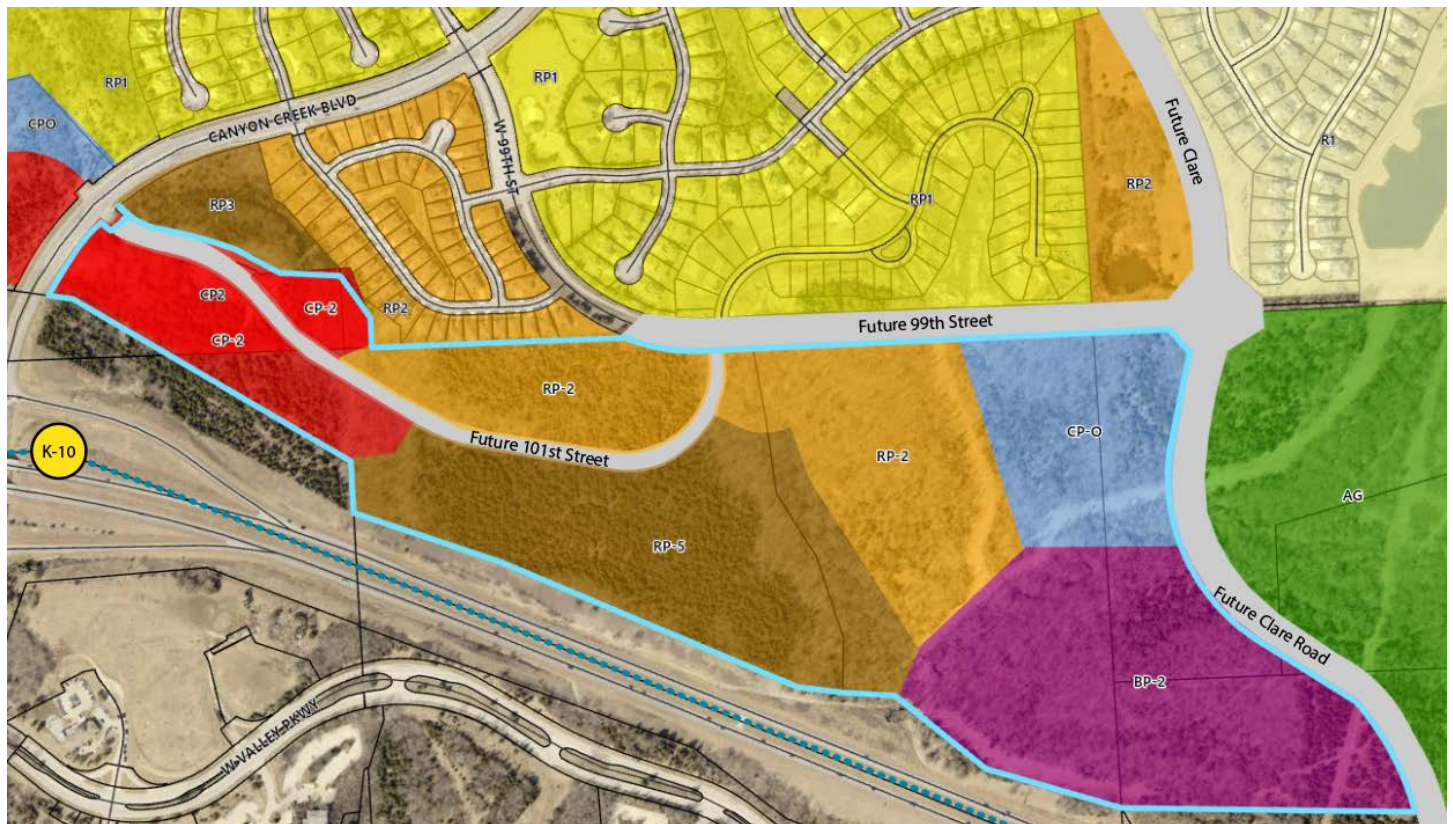


Exhibit 7, Proposed Zoning.

With regard to the review criteria contained within [Section 4-1-G-5](#) of the UDC, Staff has the following comments:

1. *The character of the neighborhood.*

The overall subject site is located in a part of the City that is currently developing. Large tracts of undeveloped land, in excess of 10 acres or larger, are to the east and west of the subject property. Single-family residential subdivisions are located north of the subject site. The property is partially used for agricultural purposes or open fields. Low-lying areas and sloped parts of the site include wooded areas and tree lined stream channels. K-10 Highway runs along the south side of the property.

The existing residential in Canyon Creek is single-family RP-1, villa RP-2 and attached villa RP-2 uses. There is one apartment community in the area and one recently approved attached villa neighborhood in the area. The comprehensive plan suggests that additional housing types and choices should be provided. Additional multi-family housing would provide housing choices for future residents that would work in the business park. Additional residential would add to the population needed to successfully

support commercial, retail and office uses. Providing additional medium density housing along 99th Street would increase the size of the existing transition area and provide the stepping of intensity stated in the comprehensive plan. Multi-family uses have generally worked well in areas along major highways in Lenexa and provide good transition to less intense uses as one travels away from the impacts of the highway.

TRACTS 1 & 2	Tracts 1 & 2 are 12.52 acres at the western end of the site abut Canyon Creek Boulevard. The site is undeveloped and proposed for commercial uses. Across Canyon Creek Boulevard to the west is undeveloped land designated for commercial and office uses. Directly to the north is an undeveloped property that is zoned for medium high-density residential. South is K-10 Highway and the City of Olathe where land is designated for business/office. To the east is undeveloped and is the site of Tracts 3 and 4 proposing duplexes on the north of future 101 st Street and apartments to the south.
TRACT 3	Tract 3 is 10.10 acres of currently undeveloped land proposed for duplexes. Canyon Creek Villas are single-family homes to the north of this property. West is undeveloped and is designated commercial with Tracts 1 & 2 of this plan. South is undeveloped with 101 st Street and apartments proposed with Tract 5 of this proposal. East is undeveloped with townhomes on Tract 5 of this plan.
TRACT 4	Tract 4 is 22.96 acres of undeveloped land proposed for apartments. West is undeveloped and is designated commercial with Tracts 1 & 2 of this plan. North is undeveloped and north of future 101 st Street is designated for duplexes with Tract 3. East is undeveloped land, consisting of stream buffer and proposed townhomes with Tract 5 of this plan.
TRACT 5	Tract 5 is 16.46 acres of undeveloped land proposed for townhomes. West is undeveloped land with 101 st Street and apartments proposed with Tract 5 and duplexes proposed with Tract 3. North is future 99 th Street and Canyon Creek Forest single-family subdivision, which is under construction. East is undeveloped with large areas of stream buffer with proposed office uses with Tract 6. South is undeveloped stream buffer and K-10 Highway.
TRACT 6	Tract 6 is 13.56 acres of undeveloped land proposed for office uses in the areas not within stream buffer. North is future 99 th Street and undeveloped land that has been approved for duplexes. South is undeveloped land proposed for an office/warehouse with Tract 7. West is stream buffer and undeveloped land proposed for townhomes with Tract 5. East is a large area of undeveloped land.
TRACT 7	Tract 7 is 32.24 acres of undeveloped land proposed for office/warehouse. North is stream buffer and undeveloped land proposed for office uses with Tract 6. South is K-10 Highway and the City of Olathe. East is a large area of undeveloped land and west of the property is stream buffer and undeveloped land designated for apartments with Tract 4 of this proposal.

The applicant held a neighborhood meeting on September 22, 2022, where the applicant discussed the rezoning and concept plan applications with approximately 40 neighbors. Topics discussed included the land use change from *Office Research and Development* to have more diverse and residential uses, the current zoning and rezoning process, the transportation network and City projects, truck traffic, the Clare Road and K-10 interchange, tree preservation, and landscape buffering.

2. The zoning and use of properties nearby.

The following vicinity description is for the overall project:

VICINITY ZONING PATTERN:

North: RP-1, Planned Residential (Low-Density)
RP-2, Planned Residential (Intermediate-Density)
RP-3, Planned Residential (Medium High-Density)
South: BP, Business Commercial (City of Olathe)
East: AG, Agricultural
West: CP-2, Planned Community Commercial
CP-O, Planned General Office

VICINITY LAND USE PATTERN:

North: Suburban Residential – Canyon Creek by the Lake, Canyon Creek Villas, Canyon Creek Forest, Wheatley Pointe, Second
South: Undeveloped (City of Olathe)
East: Undeveloped
West: Undeveloped

3. The suitability of the subject property for the uses to which it has been restricted.

The overall development is currently restricted to agricultural and a small amount of commercial as zoned. While the property is suitable for agricultural uses today, agricultural uses become less suitable as suburban development occurs around it and conflicts between agricultural and residential uses occur. Rezoning the 112 acres to a mix of commercial, office, residential, and business park uses will result in a much more suitable development to serve the neighborhoods of western Lenexa by providing a variety of land uses available to the nearby residents.

4. The extent to which the proposed use will detrimentally affect nearby property.

Each component includes different land uses and has differing contextual relationships to neighboring development. Impacts such as increased traffic and activity will occur with any suburban type of development. It is staff's opinion the overall project will have a minimal impact on the neighboring properties.

TRACTS 1 & 2	12.52 acres of undeveloped land proposed for CP-2. This area is surrounded by undeveloped land and commercial development in this location is not expected to detrimentally affect nearby property.
TRACT 3	10.10 acres of undeveloped land proposed for RP-2. Surrounded by undeveloped land except Canyon Creek Villas single-family homes to the north of this property. The concept plan proposes a 15-foot landscape buffer tract between the proposed duplexes and the neighborhood. This development will provide a transition of zoning between existing single-family uses to the north and more intense multi-family uses and K-10 Highway to the south. Staff does not believe this development will detrimentally affect the surrounding property.
TRACT 4	22.96 acres of undeveloped land proposed for RP-5. This area is surrounded by undeveloped land and development of apartment in this location provides an appropriate residential use adjacent to a busy highway. This development is not expected to detrimentally affect nearby property.
TRACT 5	16.46 acres of undeveloped land proposed for RP-2. Surrounded by undeveloped land except Canyon Creek Forest single-family homes (under construction) to the north of this property. Future 99 th Street lies between the proposed townhomes and the single-family subdivision to the north. Construction of future 99 th will require additional landscape buffer on both sides of the road. This development will provide a transition of zoning between existing single-family uses to the north and more intense multi-family uses and K-10 Highway to the south. Staff does not believe the development of townhomes in this location will detrimentally affect the surrounding property.
TRACT 6	13.56 acres of undeveloped land proposed for CP-O. This area is surrounded by undeveloped land and is office development in this location is anticipated to provide a transition of zoning from single-family to the north to business park uses further south. This development is not expected to detrimentally affect nearby property.
TRACT 7	32.24 acres of undeveloped land proposed for BP-2. This area is not adjacent to any proposed residential and is surrounded by undeveloped land and office/warehouse development in this location is not expected to detrimentally affect nearby property.

5. *The length of time the subject property has remained vacant as zoned.*

The property is currently undeveloped. Agricultural uses have been conducted on cleared fields of the overall site for several decades. The property was annexed into the City in two parts. A small portion of the west of the subject property was annexed into Lenexa in 1988 with 327.10 acres around Canyon Creek Boulevard. The majority of the property was later annexed into the City as a part of 1,934.80 acres of land annexed into west Lenexa in 1999. There are no previously approved plans for this property.

6. *The relative gain to public health, safety and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.*

It is staff's opinion the proposed rezoning and concept plans demonstrate an appropriate request for zoning designations, transition of uses and provision of adequate public street infrastructure to satisfy the City's development regulations. The site is anticipated to develop in some manner with a mix of residential and nonresidential uses in various levels of intensity.

7. *Recommendation of City's permanent professional staff.*

See the Staff's Recommendation section of the staff report.

8. *Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.*

All components of the project are shown to be designated for *Office, Research and Development* on the City's Future Land Use (FLU) Map. The FLU Map is not intended to identify the only possible type of land use but to provide a guide for the potential land use of a particular region of the City. Land uses identified on the FLU Map may be rezoned to different types of uses for various reasons such as development trends, feasibility of certain uses compared to others and due to preceding changes to the neighboring properties as they are developed. An example of this is the Watercrest Landing subdivision. This subdivision has four separate FLU designations across the subdivision. The owner/developer envisioned a plan that was not fully consistent with the FLU Map, yet the plan was consistent with the City's development regulations.

9. *The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.*

Utilities are readily available and services are in close proximity. The developer is responsible for extending the required utility connections. Staff does not anticipate any limitations of the public utilities or services because of the proposed residential and nonresidential development.

10. ***The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.***

It is staff's opinion that the adjacent and surrounding street network, in combination with the existing and proposed street network, will adequately support the volume of traffic generated by the proposed residential, commercial, and business park development. The overall project will have an arterial street on the east and west side and a local collector street north and proposed through the middle of the development. Staff anticipates the street network to have adequate capacity to provide access into each component of the development without adversely affecting the existing development.

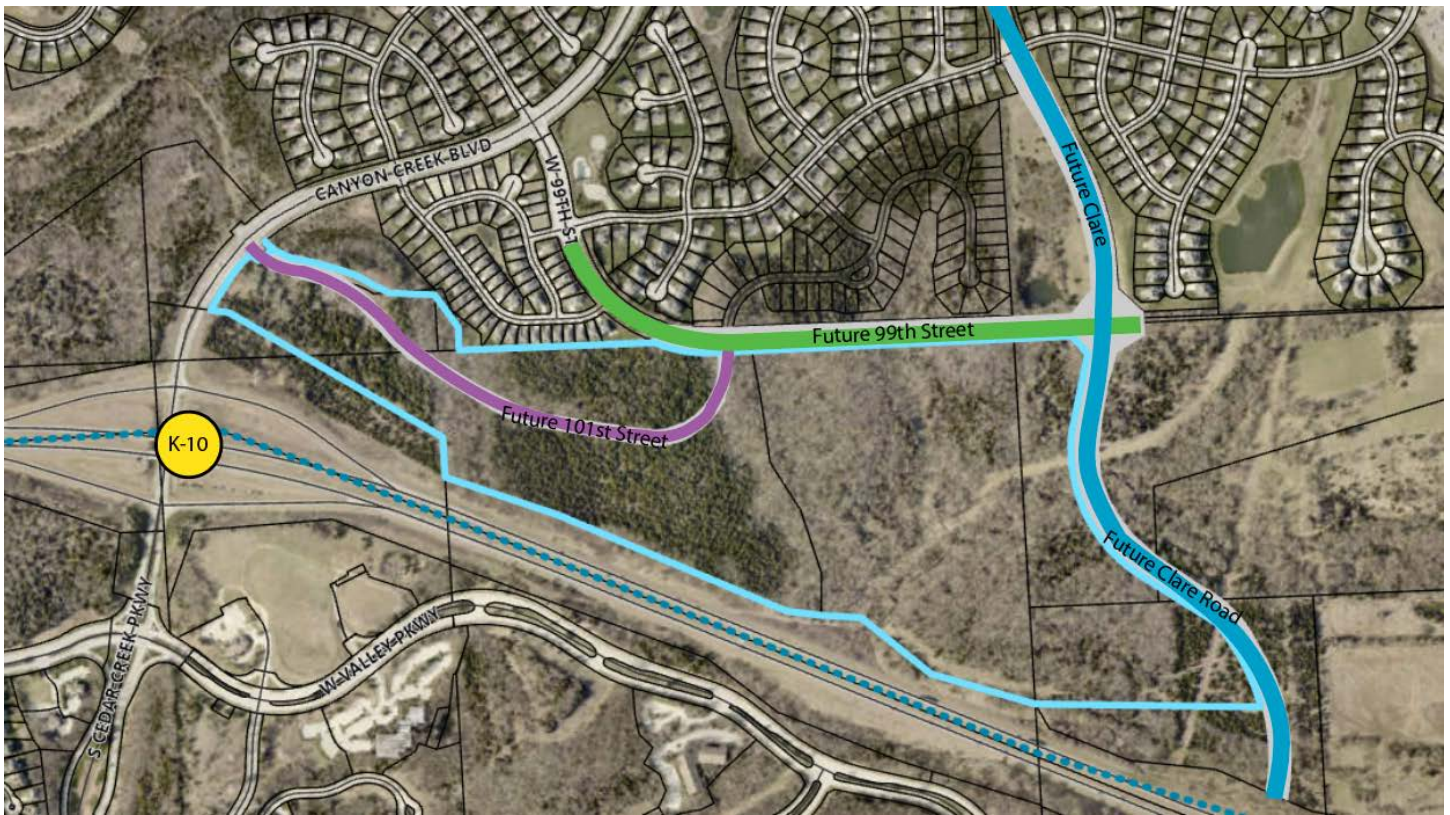


Exhibit 8, Classifications for Proposed Streets.

The exhibit above shows the proposed major roadways for this development. Future 101st Street (purple) is a designated collector, Future 99th Street (green) is a minor arterial, and Future Clare Road (blue) is a major arterial roadway. The design for the public streets illustrated on this plan are conceptual based on City staff comments. A traffic study shall be submitted with the preliminary development plan. The final design for the public streets shall be based on the traffic study recommendations. With preliminary plan approval, the applicant will be required to show a phasing plan indicating which sections of 101st Street, 99th Street, and Clare Road to be completed with, or prior to, the phase.

11. ***The environmental impacts the proposed use will generate including, but not limited to, excessive storm water runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting or other environmental harm.***

It is staff's opinion that the proposed development will not impact the environment to any greater level than development that is consistent with the FLU Plan designations. Detention and BMP size and locations indicated on the plan are conceptual. All City of Lenexa stormwater requirements will need to be met at the time of preliminary plan including all stream buffer requirements. A stormwater management report shall be provided with the preliminary development plan. Development of the site shall meet the City's stormwater requirements regardless of the use of the site.

12. ***The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.***

The applicant intends to comply with the City's regulations for development of the site for each of the components of the project. Alternatives to ordinance requirements may be approved as deviations from the Unified Development Code (UDC). Requests for deviations are reviewed with the preliminary or final plan for the project.

STAFF RECOMMENDATION

Staff recommends **APPROVAL** of the rezoning for Cedar Canyon West located approximately at the northeast corner of Canyon Creek Boulevard and K-10 Highway from the AG, Agricultural and the CP-2, Planned Community Commercial Zoning Districts to CP-2, Planned Community Commercial; RP-2, Planned Residential (Intermediate-Density); RP-5, Planned Residential (High-Density); CP-O, Planned General Office; and BP-2, Planned Manufacturing Zoning District as depicted on the applicant's concept plan.

CONCEPT PLAN ANALYSIS

OVERALL PLAN ANALYSIS

The proposed development is a large-scale development concept. The developer is aggregating multiple parcels of land with multiple zoning designations and uses. Showing the entire 112-acre area provides a more complete picture of how the multiple components will relate to each other in this larger context. It is beneficial to look at the proposed development in the larger context for coordination of various land uses and the distribution of the traffic that is anticipated with the project.

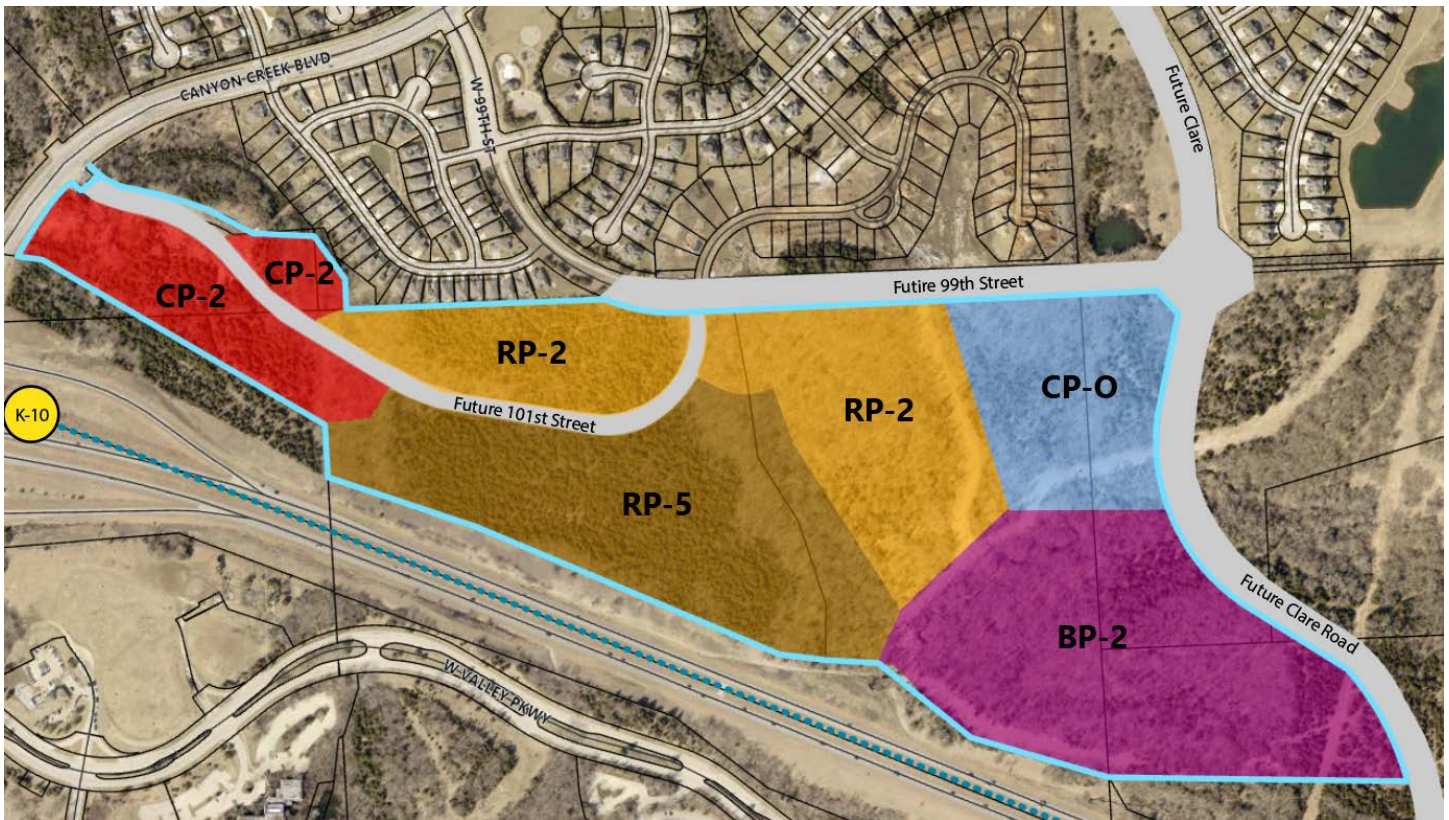


Exhibit 9, Overall Proposed Zoning

The project is bordered by K-10 Highway on the south, suburban density single-family subdivisions across future 99th Street to the north, future Clare Road and undeveloped property to the east and Canyon Creek Boulevard and undeveloped land to the west. Future 101st Street, a local collector street, will bisect the west of the site. Future 101st Street, future Clare Road, and future 99th Street are all critical to the project for the distribution of traffic from each component of this project, and the surrounding uses, out to Canyon Creek Boulevard to the west.

The exhibit on the next page shows the rendering for the overall development provided by the applicant. Looking at the overall site is important to understand how all the different proposed land uses will interact with one another, adjacent development and the street network. Beginning at the west end of the site, turning onto future 101st Street, there are multiple buildings of community commercial uses proposed to the south (Tract 1) and a small office building (Tract 2) to the north of the street. Continuing east, a local road branches off to the north with duplexes on each side (Tract 3) and apartments are proposed to the south (Tract 4) providing a buffer between K-10 Highway and the residential uses north of future 101st Street. Future 101st Street continues northeast to connect into 99th Street. Before this

intersection, a local road branches off future 101st Street to the east and connects to both the apartments to the south and the proposed townhome development to the east (Tract 5). This proposed townhome development is designed in a clustered style to avoid stream buffer surrounding the site. A right in/out access is available from the townhome development onto future 99th Street to the north. Two office buildings are proposed at the northeast corner of the development (Tract 6) with access to both future 99th Street to the north and future Clare Road to the east. South of the office buildings on the west side of future Clare Road is a potential office/warehouse building with two points of access onto future Clare Road.



Exhibit 10, Overall Proposed Concept Plan.

Overall, while consisting of many different components, staff believes that the overall proposed concept plan for the site provides a positive and diverse mix of uses that better serve the community than the FLU designated *Office, Research and Development* land use designation. The conceptual site plan is designed around existing natural features and stream buffer. Staff believes that the proposed concept plan provides a variety of commercial, retail, and office that meets a need for these land uses in western Lenexa, which is primarily residential. Staff is supportive of the overall concept plan.

Tracts 1 & 2 – COMMUNITY COMMERCIAL CENTER CONCEPT PLAN ANALYSIS

SITE LAYOUT



Exhibit 11, Tracts 1 and 2 – Location Map.

TRACT 1

SITE INFORMATION TRACT 1 – CP-2	
Current Zoning	CP-2, Planned Community Commercial
Proposed Zoning	CP-2, Planned Community Commercial
Gross Site Area Tracts 1 & 2	12.52 acres
Tract 1 Lot Area	8.40 acres
Provided Open Space	46.52% (25% required)
Proposed Parking	235 spaces (197 minimum required)

Site Plan Information:

- Site plan includes a mix of commercial and office services
- Five proposed buildings (in order from west to east):
 - Building 1 - 4,550 square foot Convenience Store
 - Building 2 – 5,000 square foot Retail & 5,000 square foot Restaurant
 - Building 3 – 2,500 square foot Retail 2,500 square foot Restaurant
 - Building 4 – 10,000 square foot Retail/Office
 - Building 5 - 10,000 square foot Retail/Office

- Project includes open space, stream buffers, storm detention and water quality features and landscape buffers.
- Three access points to 101st Street (collector) to the north of the site:
 - Right in/out access – northwesternmost access point servicing eastbound traffic only.
 - Full access – access point in the middle of the property aligns with Tract 2 drive entrance. Serves traffic in all directions.
 - Full access – access point further east on the property serving traffic in all directions.
- Proposed buildings meet 30-foot building setback from future 101st Street.
- Proposed parking areas are set back 20—25 feet from future 101st Street for landscape easement.
- Deviation for parking areas to the 100-street frontage setback from K-10 Highway will be requested with the preliminary plan application. Site plan shows all proposed buildings are outside of the 100-foot setback and the deviation will only be necessary for parking areas.

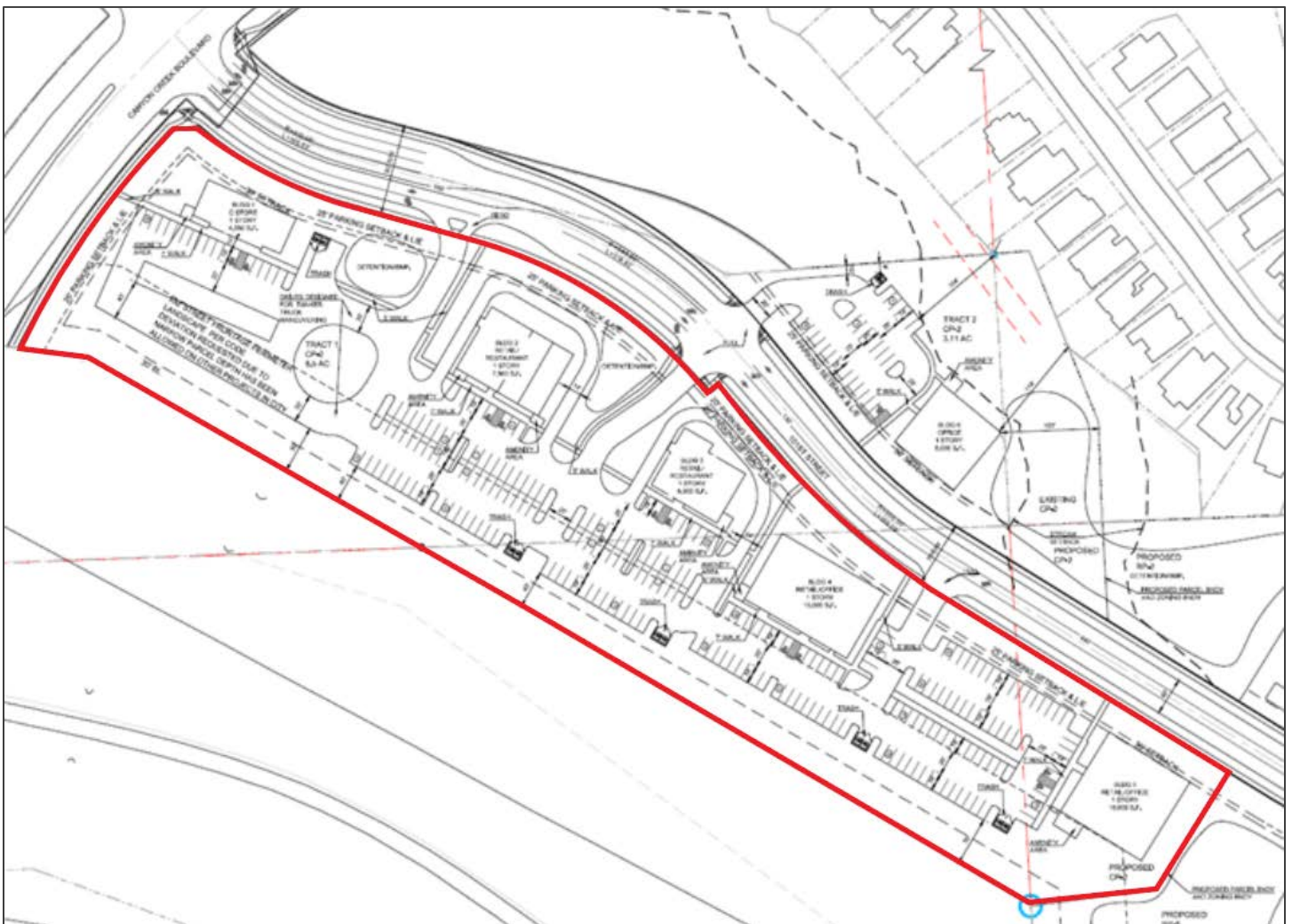


Exhibit 12, Tract 1 – Commercial Center Concept Plan.

TRACT 2

SITE INFORMATION TRACT 2 – CP-2	
Current Zoning	CP-2, Planned Community Commercial
Proposed Zoning	CP-2, Planned Community Commercial
Gross Site Area Tracts 1 & 2	12.52 acres
Tract 2 Lot Area	1.94 acres
Provided Open Space	74.55% (25% required)
Proposed Parking	24 spaces (24 minimum required)

Site Plan Information:

- Site plan includes one office building:
 - Building 6 – 6,000 square foot 1 story office building
- One access point to future 101st Street (collector) at northwest of site.
- Proposed building located 114-feet south of property line of closest home to the north.
- Proposed parking lot is set 25-feet from future 101st Street.
- Proposed office building is set 30-feet from future 101st Street.
- Tract 2 includes open space, stream buffers, storm detention and water quality features, landscape buffers, pedestrian amenity area, and sidewalks.
- No deviation requests are anticipated with this site plan.

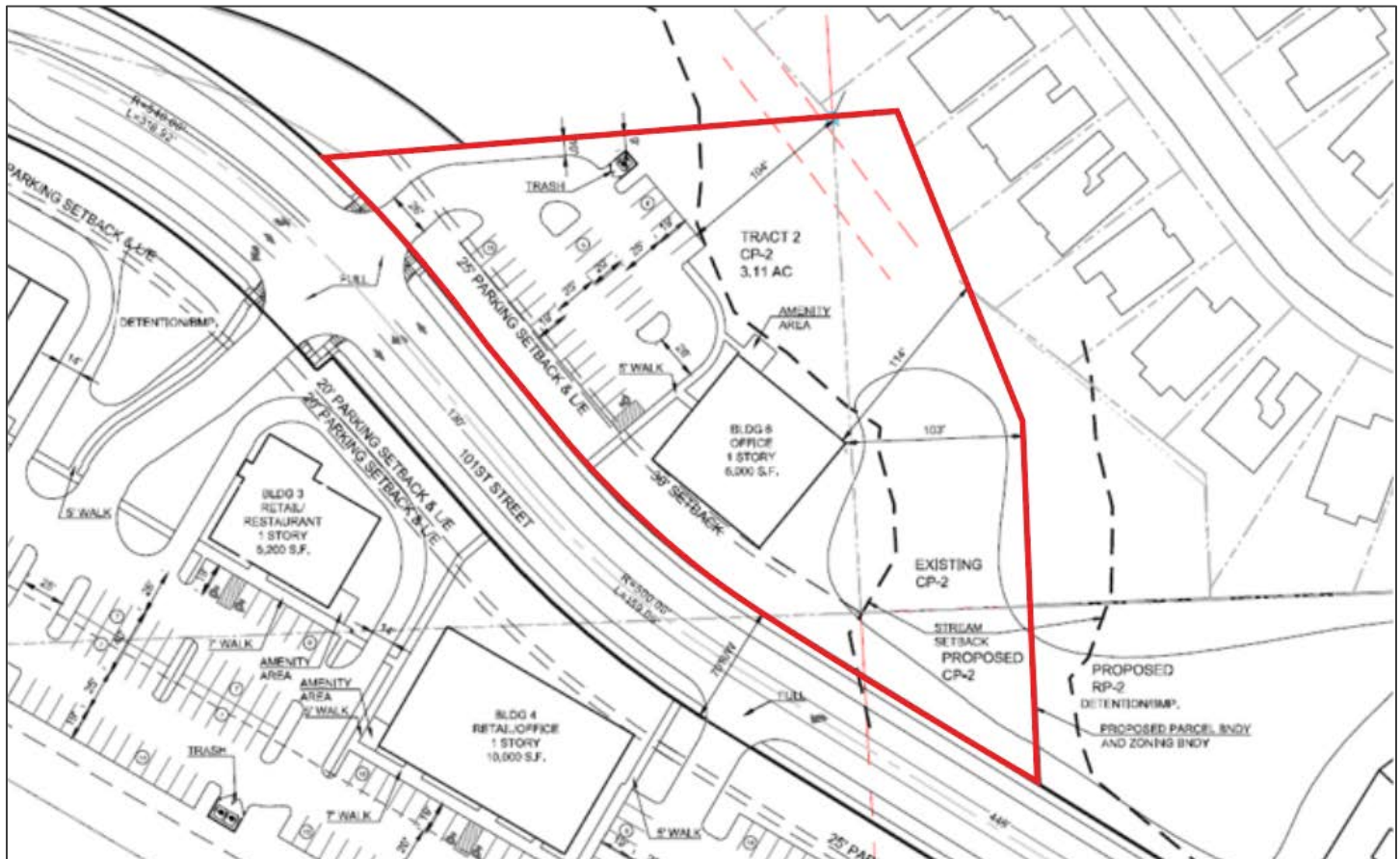


Exhibit 13, Tract 2 – Community Commercial Concept Plan

ARCHITECTURAL COMPATIBILITY



Exhibit 14, Tracts 2 and 3 – Commercial Architectural Examples.

Architectural example images have been included by the applicant for commercial development. The architectural style will be determined by the developer of each parcel and will be provided with the preliminary development plan.

Tract 3 – MEDIUM DENSITY RESIDENTIAL CONCEPT PLAN ANALYSIS

SITE LAYOUT

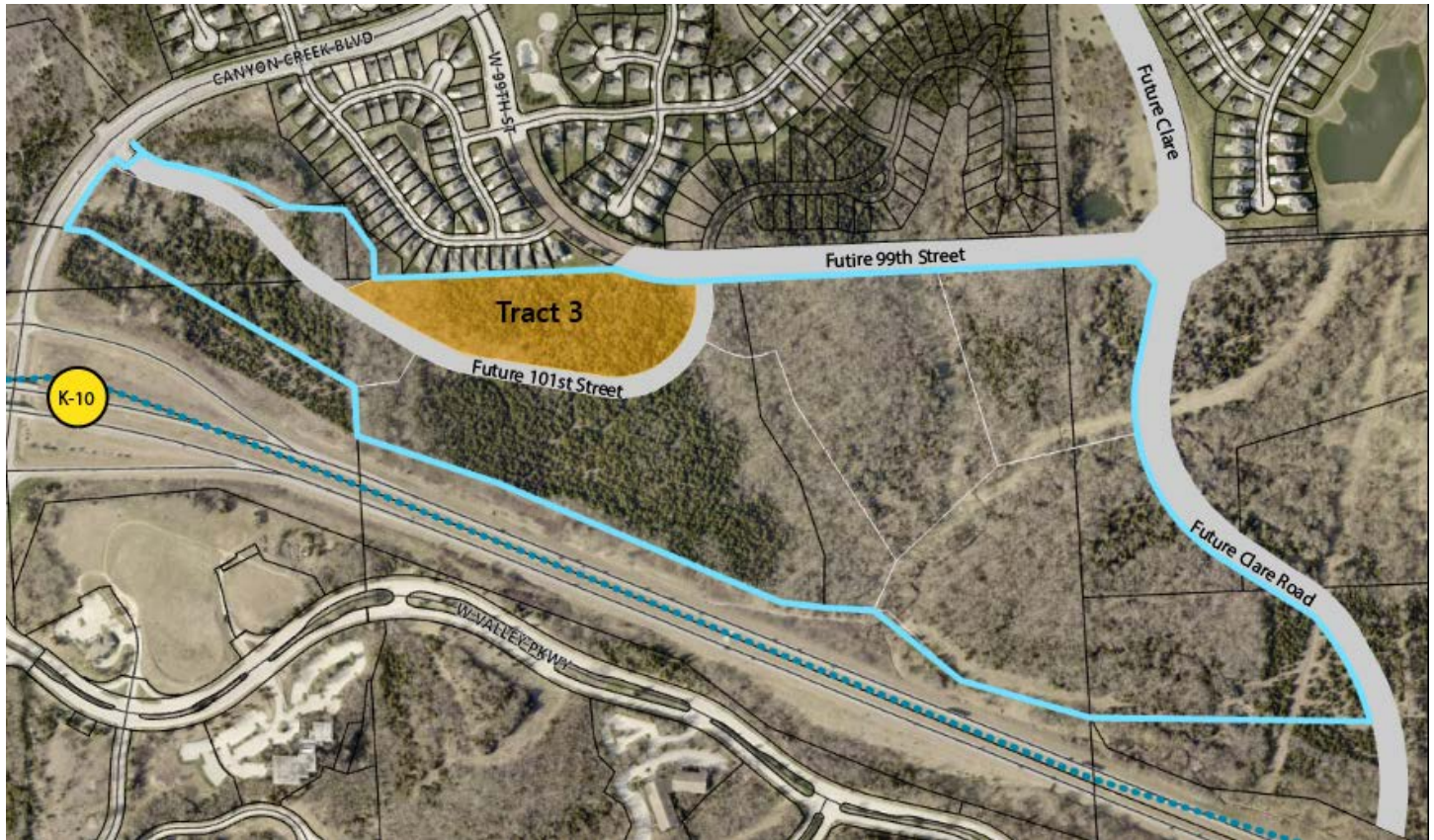


Exhibit 15, Tract 3 – Location Map.

SITE INFORMATION TRACT 3 – RP-2

Current Zoning	AG, Agricultural
Proposed Zoning	RP-2, Planned Residential (Intermediate-Density)
Net Site Area	8.46 acres
Number of Lots	20 lots, 2 tracts
Number of Units	40 units
Density Proposed	4.72 du/acre (8 du/acre max)
Provided Open Space	60% (60% required)

Site Plan Information:

- Site plan includes two-family homes.
- 20 proposed buildings, 40 units.
- Project includes open space, stream buffers, storm detention and water quality features and landscape buffers.
- Adjacent to existing medium density uses and increases transition zoning area between existing single-family and other uses and K-10 Highway.
- Two full access points to 101st Street (collector) on the east and west of the site. Both access points align with other proposed roads and drive aisles. Access to 101st Street collector street that provides an alternative direct access to Canyon Creek Boulevard.

- Proposed buildings meet 30-foot building setback from future 101st Street and the 25-foot setback from the proposed local road where the duplexes front.
- No deviation requests are anticipated with this site plan.

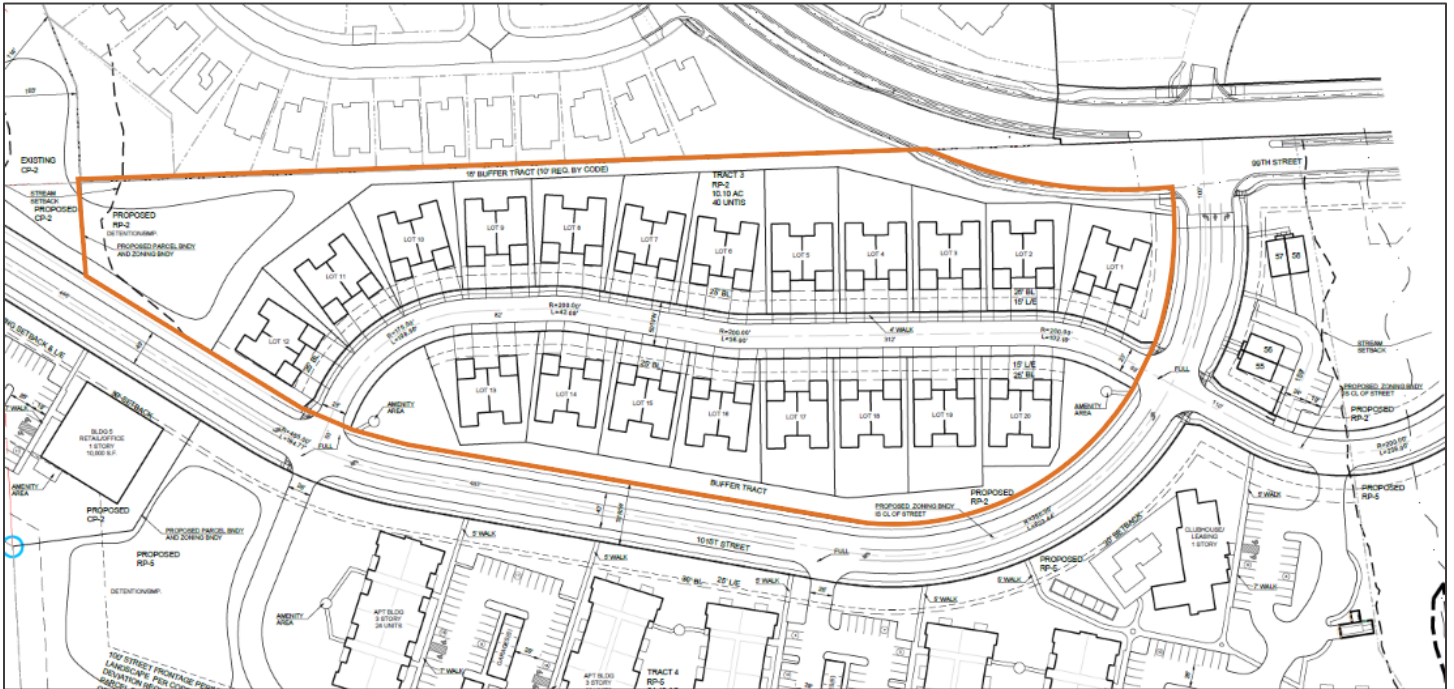


Exhibit 16, Tract 3 – Concept Plan Attached Villas.

ARCHITECTURAL COMPATIBILITY



Exhibit 17, Tract 3 – Attached Villas/Townhomes Architectural Examples.

Architectural example images have been included by the applicant for attached villas. The architectural style will be determined by the developer of each parcel and will be provided with the preliminary development plan.

Tract 4 – HIGH DENSITY RESIDENTIAL CONCEPT PLAN ANALYSIS

SITE LAYOUT

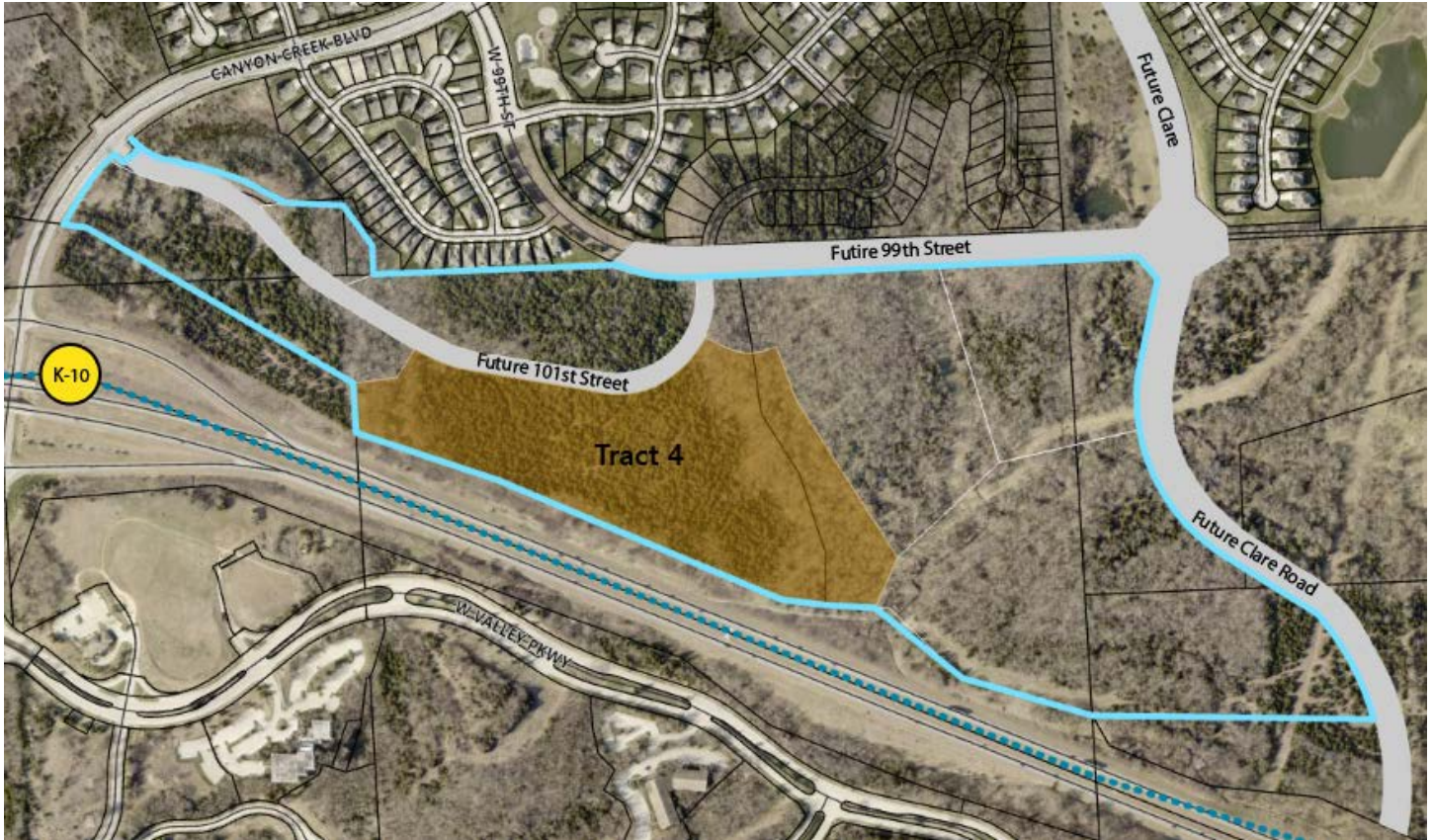


Exhibit 18, Tract 4 – Location Map.

SITE INFORMATION TRACT 4 – RP-5

Current Zoning	AG, Agricultural
Proposed Zoning	RP-5, Planned Residential (High-Density)
Net Site Area	22.02 acres
Number of Lots	1 lot
Number of Units	276 units
Density Proposed	12.02 du/acre (16 du/acre max)
Provided Open Space	64.93% (40% required)

Site Plan Information:

- Site plan includes 9 apartment buildings and a clubhouse/leasing office.
- Apartment buildings range from 12-42 units and from 3 to a split of 3 to 4 stories.
- Plan includes open space, stream buffers, storm detention and water quality features and landscape buffers.
- A public access trail easement is required from Clare Road east to connect to Canyon Creek Boulevard. The trail location will give trail access to the apartments.
- Three access points serve the property:

- Full access – westernmost access onto 101st Street (collector) to the north of the site. Aligns with local road proposed with Tract 3's concept plan. Serves traffic in all directions.
- Full access – access point in the middle of the property onto 101st Street. Serves traffic in all directions.
- Full access – access point at northeast of property onto local road. Serves traffic in all directions.
- Proposed buildings meet 30-foot building setback from future 101st Street.
- Proposed parking areas are setback 20—25 feet from future 101st Street for landscape easement.
- Deviation for paved areas to the 100-street frontage setback from K-10 Highway will be requested with the preliminary plan application. Site plan shows all proposed buildings are outside of the 100-foot setback and the deviation will only be necessary for the paved drive aisle on the south side of the development.

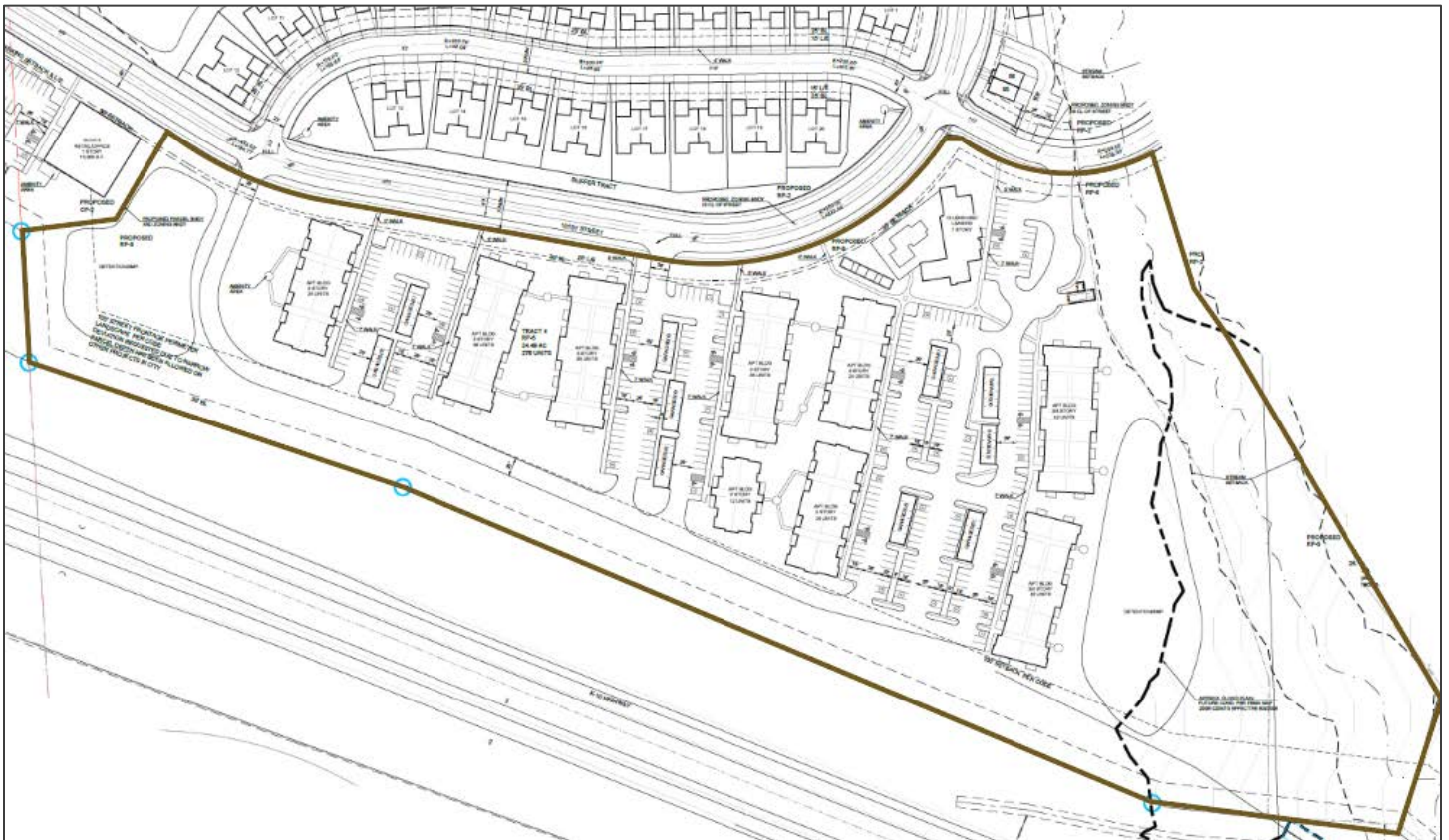


Exhibit 19, Tract 4 – Apartments Concept Plan.

ARCHITECTURAL COMPATIBILITY



Exhibit 20, Tract 4 – Apartments Architectural Examples.

Architectural example images have been included by the applicant for apartments. The architectural style will be determined by the developer of each parcel and will be provided with the preliminary development plan.

Tract 5 – MEDIUM DENSITY RESIDENTIAL CONCEPT PLAN ANALYSIS

SITE LAYOUT



Exhibit 21, Tract 5 – Location Map.

SITE INFORMATION TRACT 5 – RP-2

Current Zoning	AG, Agricultural
Proposed Zoning	RP-2, Planned Residential (Intermediate-Density)
Gross Site Area	16.48 acres
Number of Lots	2 lots
Number of Units	58 units
Density Proposed	3.05 du/acre (8 du/acre max)
Provided Open Space	60% (60% required)

Site Plan Information:

- Site plan includes 58 proposed townhome units in 19 buildings.
- Proposed townhomes range in size from duplexes to fourplexes.
- Project includes open space, stream buffers, storm detention and water quality features and landscape buffers.
- Increases size of transition zoning area between existing single-family and other uses and K-10 Highway.
- Two access points to the north and west of the site:

- Right in/out access – access point to future 99th Street to the north. Serves eastbound traffic.
- Full access – access point to future 101st Street to the east. Aligns with Tract 3's proposed local road. Serves traffic in all directions.
- Proposed buildings meet 30-foot building setback from all streets.
- No deviation requests are anticipated with this site plan.

Exhibit 22, Tract 5 – Townhomes Concept Plan.

ARCHITECTURAL COMPATIBILITY



Exhibit 23, Tract 5 – Townhomes/ Attached Villas Architectural Examples.

Architectural example images have been included by the applicant for townhomes. The architectural style will be determined by the developer of each parcel and will be provided with the preliminary development plan.

Tract 6 – OFFICE CONCEPT PLAN ANALYSIS

SITE LAYOUT

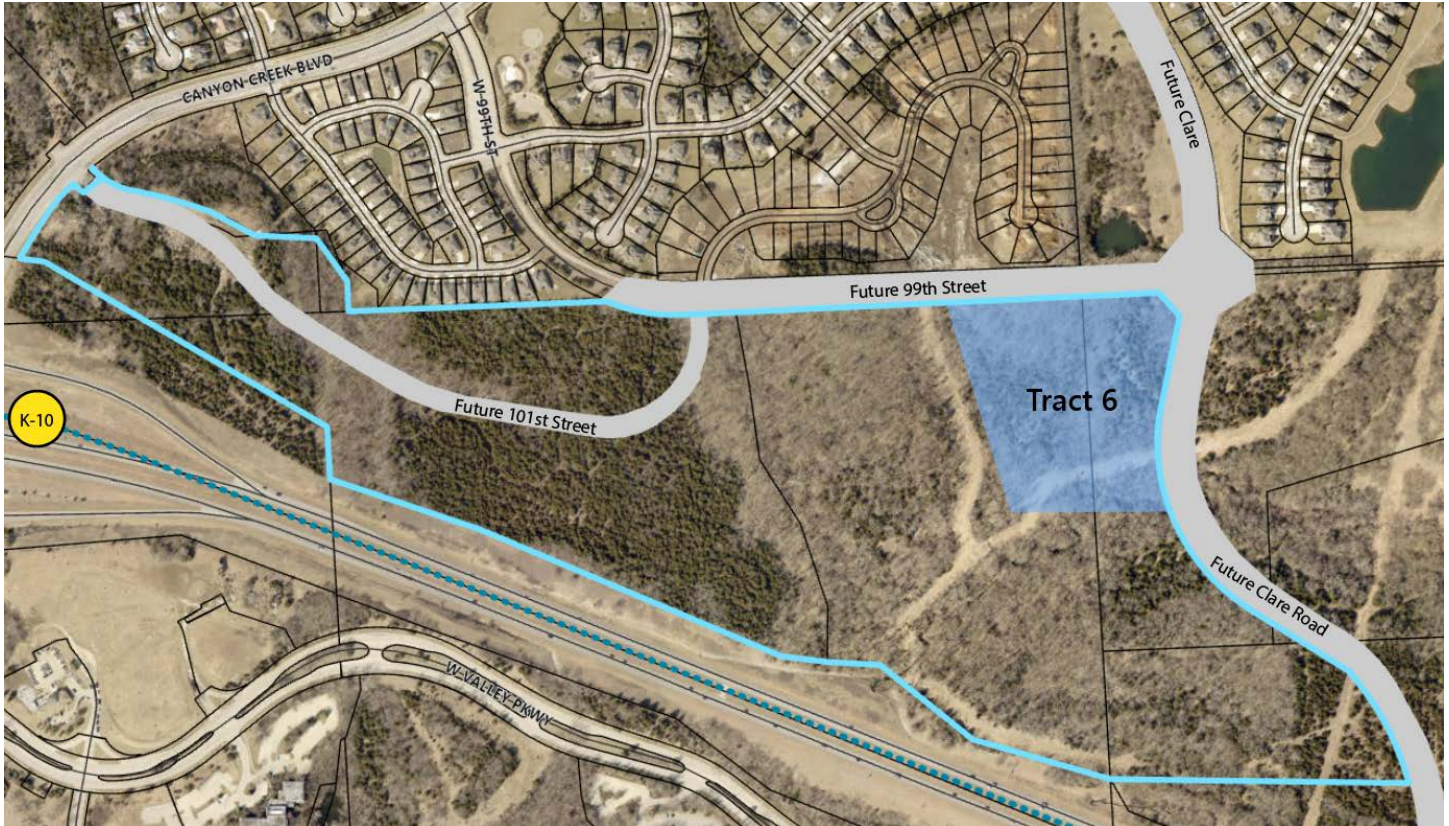


Exhibit 24, Tract 6 – Location Map.

SITE INFORMATION TRACT 6 – CP-O	
Current Zoning	AG, Agricultural
Proposed Zoning	CP-O, Planned General Office
Net Site Area	11.04 acres
Provided Open Space	82.75% (35% required)
Proposed Parking	137 spaces (72 minimum required)

Site Plan Information:

- Site plan includes two office buildings:
 - Building 1 – 8,050 square foot, one-story office building
 - Building 2 – 19,750 square foot, one-story office building
- Project includes open space, multiple stream buffer setbacks, storm detention and water quality features and landscape buffers.
- Provides transition of zoning from single-family to higher intensity office/research and development uses.
- Two access points are provided into the site. One to future 99th Street north of the site and one to future Clare Road to the east of the site:

- Right in/out access – northwesternmost access point to future 99th Street, servicing eastbound traffic only.
- Full access – Access point to future Clare Road (arterial) to the east in the middle of the property. Anticipated to align with any proposed local roads to the west of this development. Serves traffic in all directions.
- Proposed buildings meet the 30-foot building setback from future 99th Street and future Clare Road.
- Proposed parking areas are set back 25 feet from future 99th Street and future Clare Road.
- No deviation requests are anticipated with this site plan.

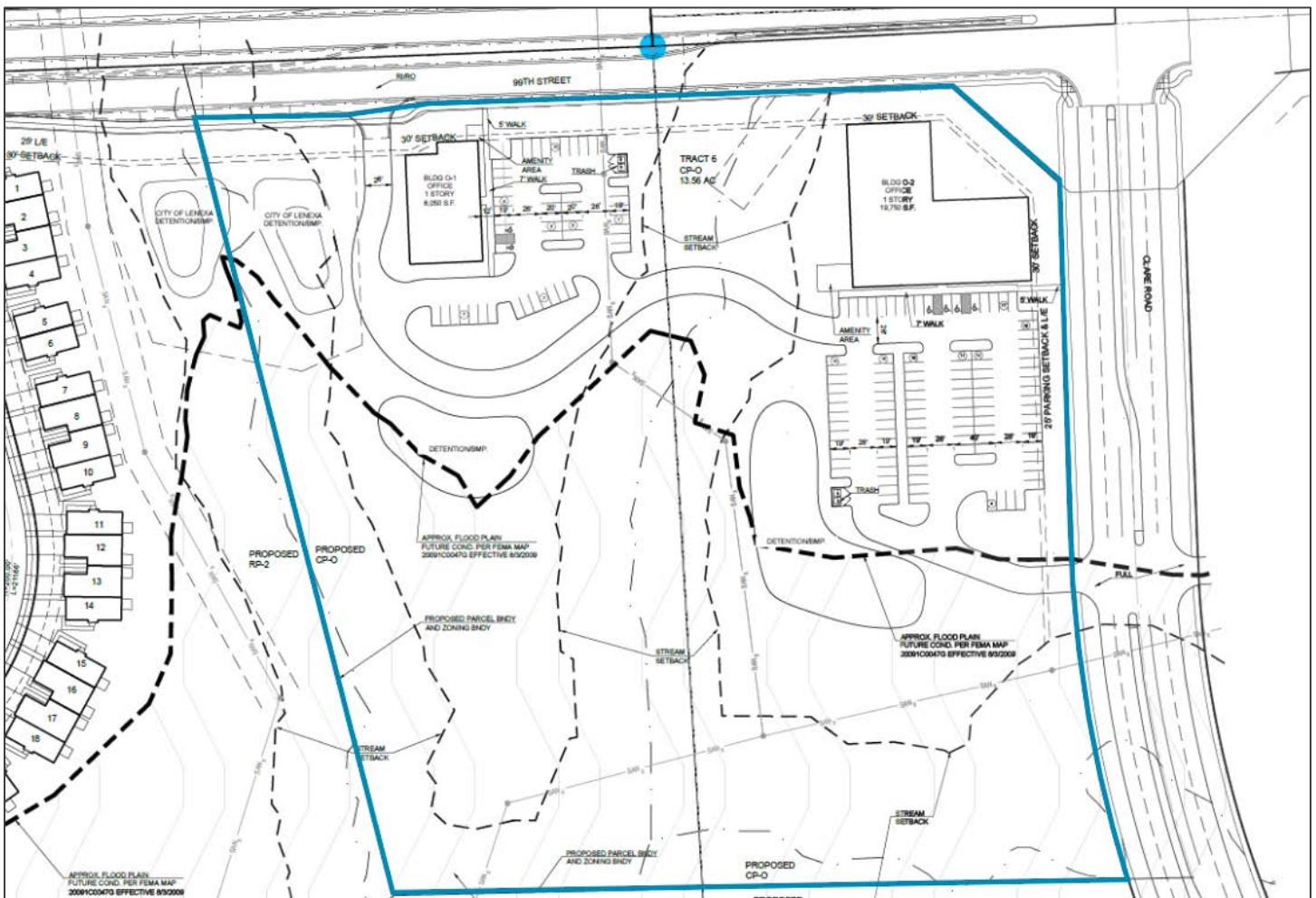


Exhibit 25, Tract 6 – Office Concept Plan.

ARCHITECTURAL COMPATIBILITY



Exhibit 26, Tract 6 – Office Architectural Examples.

Architectural example images have been included by the applicant for offices. The architectural style will be determined by the developer of each parcel and will be provided with the preliminary development plan.

Tract 7 – INDUSTRIAL/BUSINESS PARK CONCEPT PLAN ANALYSIS

SITE LAYOUT

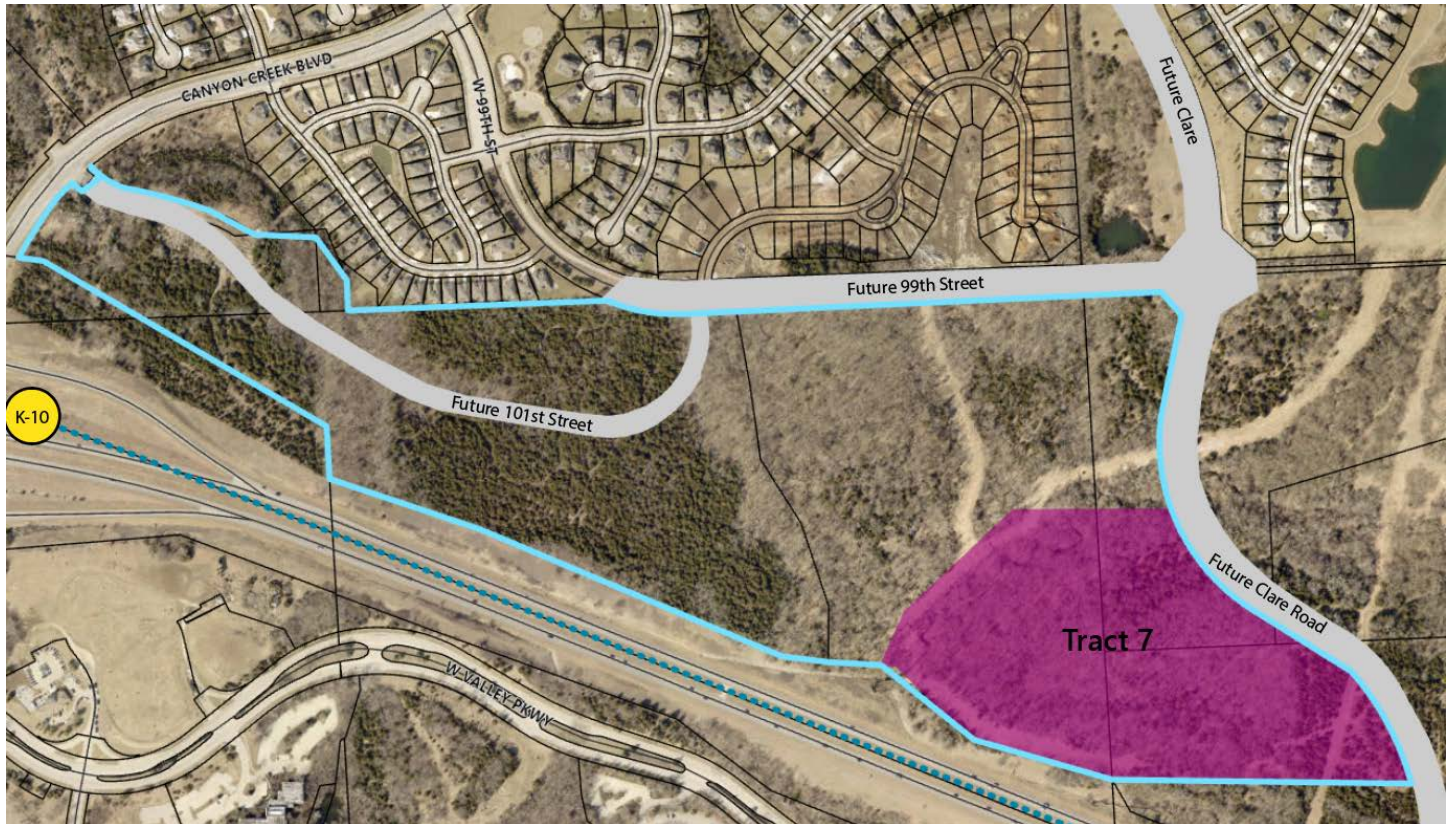


Exhibit 27, Tract 7 - Location Map.

SITE INFORMATION TRACT 7 – BP-2	
Current Zoning	AG, Agricultural
Proposed Zoning	BP-2, Planned Manufacturing
Gross Site Area	32.24 acres
Provided Open Space	58.22% (25% required)
Proposed Parking	194 spaces (150 required)

Site Plan Information:

- Site plan includes one large office/warehouse building:
 - Building 1 – 150,000 square feet
- Project includes open space, stream buffers, storm detention and water quality features and landscape buffers.
- Outdoor public pedestrian amenity area details will be provided with the preliminary plan.
- Two access points to future Clare Road (arterial) to the east of the site:
 - Right in/out access – north access point servicing southbound traffic only.
 - Full access – access towards the south of the property aligns with future 101st Street on the east side of future Clare Road. Serves traffic in all directions.
- Proposed buildings meet 50-foot building setback from future Clare Road.
- Proposed parking areas are set back 20 feet from future Clare Road for landscape easements.

- The concept plan meets the required 100-foot street frontage setback from K-10 Highway.

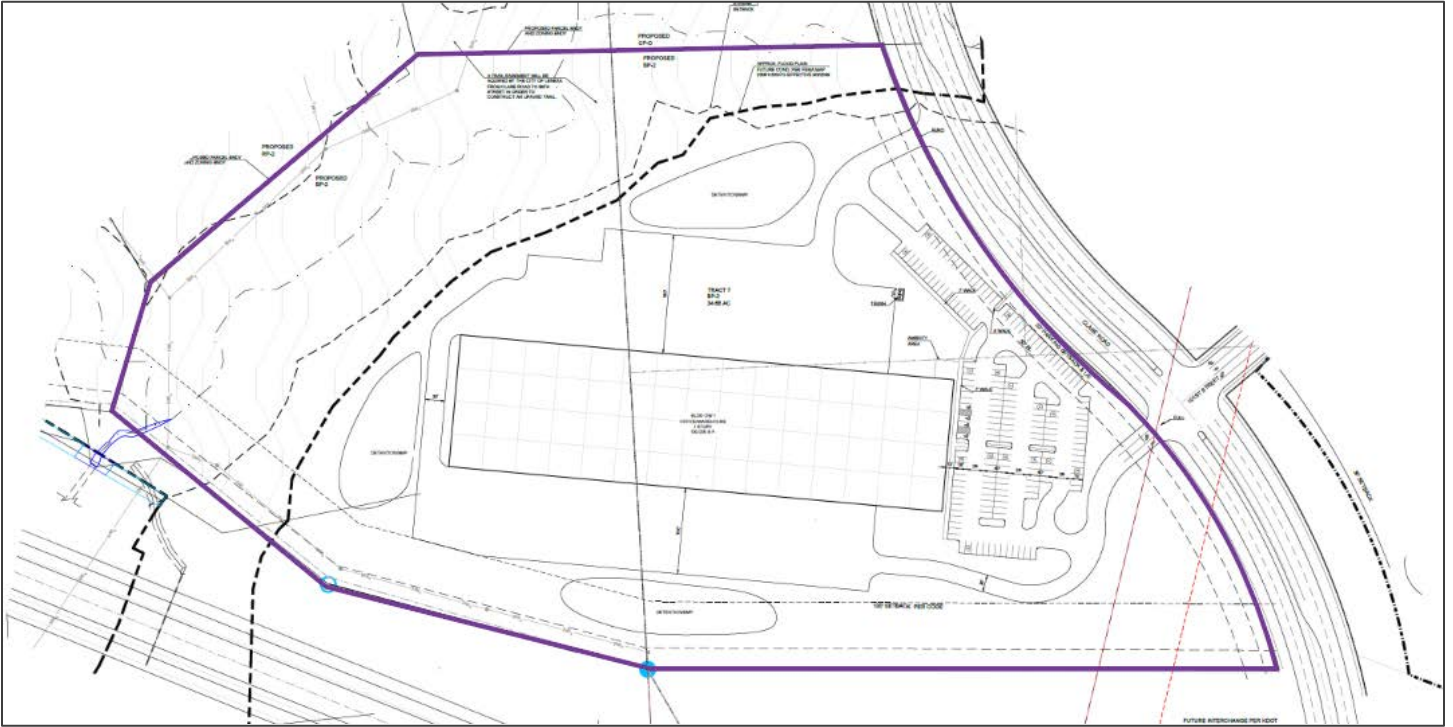


Exhibit 28, Tract 7 – Industrial/Business Park Concept Plan.

ARCHITECTURAL COMPATIBILITY



Exhibit 29, Tract 7 – Business Park Architectural Examples.

Architectural example images have been included by the applicant for business parks. The architectural style will be determined by the developer of each parcel and will be provided with the preliminary development plan.

INFORMATIONAL COMMENTS

The rezoning and concept plan applications are tentatively scheduled to be considered by the Governing Body on December 6, 2022.

STAFF RECOMMENDATION

Staff recommends **APPROVAL** of the concept plan for Cedar Canyon West located at the northeast corner of Canyon Creek Boulevard and K-10 Highway.

CEDAR CANYON WEST RP-4 ZONING REQUEST NARRATIVE

The Owner is requesting RP-4 for the area along K-10 Highway for the following reasons.

1. The proposed zoning is consistent with the uses and intensity within the 7/10 Crossroads Activity Center.
2. The comprehensive plan includes goals to have housing diversity to have multi-family housing dispersed throughout the City.
3. The proposed zoning is consistent with the zoning allowed for parcels with a similar context.
4. The parcel is located adjacent to a major expressway and is buffered from existing residential with less intense transitional zoning and located considerable distance from existing residential.
5. The proposed zoning would enhance the marketability for retail/commercial uses as stated in the Comprehensive Plan for the 7/10 Crossroads Activity Center.
6. The proposed use is less intense than the land use shown on the current Comprehensive Plan.
7. Roads and infrastructure, required by the City and previously approved, will allow access to and from the parcel that access the arterial street network directly. The proposed use would also offset the cost for the infrastructure.
8. A considerable amount of the overall site area is to remain as natural open space.
9. The proposed number of units is consistent with the existing RP-4 communities and the average number of units per community based on the National Apartment Association.

The parcel for the proposed RP-4 is located within the area that the City Comprehensive plan identifies as the 7/10 Crossroads activity center that is planned to have more concentration of more intense uses. The Comprehensive plan and the Vision 2040 plan contain goals to encourage a variety of housing choices and a mix of residential opportunities. The Comprehensive plan also states that multi family housing should be dispersed throughout the City. There are only a couple of apartment communities in the K-7/K-10 area. The proposed use would provide more housing choices and more variety of housing in this area to encourage new and support existing business in the area.

The context for this parcel is similar to the context of the areas where existing RP-4 and High Density Residential is located in the City. All of the existing apartment communities between 95th Street and north City limits along Interstate 435 expressway are zoned RP-4 or RP-5, all are located immediately adjacent to either RP-2 or RP-1 zoning districts and all are within close proximity to commercial and business uses. The proposed Cedar Canyon West RP-4 parcel is located immediately adjacent to K-10 expressway, located adjacent to RP-2 zoning districts and within close proximity to proposed commercial and business uses.

The land use on the current Comprehensive Plan is Office Research & Development, which would allow office and some manufacturing and assembly businesses. The intensity and allowed building height is significantly greater for these uses than the proposed RP-4 use. With this land use, the typical land use is CP-O which has no height restriction or BP-1/BP-2 which would allow heights up to 65 feet. In addition, the buildings could be significantly larger and have higher parking requirements and vehicular trips.

The alignment of 101st Street from Canyon Creek Boulevard to 99th Street, as required and previously approved by the City, allows drivers to and from the uses along the street, including the RP-4 site to travel to arterial streets without needing to travel through existing residential neighborhoods. The infrastructure cost because of the overall low density is extensive. The proposed use would also offset the extensive cost for the infrastructure.

A considerable amount of the overall site area is to remain as natural open space which reduces the density of the overall area considerably. The natural open space area east of the proposed RP-4 parcel is approximately 16 acres. This area combined with the open space within the RP-4 area is approximately 20 acres. This amount of open space reduces the intensity of the overall area to be well below what is found in similar existing RP-4 projects in the City. According to the National Apartment Association 2021 survey, the average number of units for mid/high rise multifamily communities is 388 units. Based on information from local apartment developers, apartment communities need over 300 units to cover the rising property, construction and operation costs. The proposed number of units is consistent with the number of units allowed in existing RP-4 communities in the City.

CEDAR CANYON WEST BP-2 ZONING REQUEST NARRATIVE

The Owner is requesting BP-2 for the area along K-10 Highway. The primary reasons for requesting BP-2 zoning is to allow more flexibility in the uses that could occur on the parcel and it is consistent with the zoning allowed for parcels with a similar context. The type of intersection at K-10 and Clare Road, interchange or flyover, would influence the types of uses that would occur on the parcel. There are uses allowed in the BP-2 that are not allowed in the BP-1 that could occur in either scenario.

The potential BP-2 uses that could occur without an interchange would be similar to the uses found in College Crossings Business Park, College Crossover West, 107th/108th and Pflumm Business Park and Pflumm Business Center. All of these business parks have small scale service, light manufacturing and light industrial that utilize routes on arterial streets that have similar context to this parcel. The BP-2 zoning includes the following uses found in these projects that are not allowed in the BP-1 district that could be potential uses for this parcel; General Construction sales and service (plumbing services, electrical services, specialty building product sales), fitness center, veterinary services, laundry services and manufacturing.

If an interchange is constructed the potential uses could include warehouse and distribution uses. It is unlikely that a warehouse distribution business would seek to locate on this parcel if there is no interchange. The existing warehouse distribution business in the City have direct access or are located within close proximity to interchanges.

The BP-2 zoning is consistent with the zoning allowed on parcels with a similar context. College Crossings Business Park, College Crossover West, 107th/108th and Pflumm Business Park and Pflumm Business Center are located adjacent to expressways, not adjacent to interchanges, and adjacent or near to residential uses. The City Comprehensive Plans has more intense uses extending along the north side of K-10 and along both sides of K-7. When 99th Street is extended east over K-7 and connected to 101st Street which extends to Lone Elm Road where the future interchange is planned, 99th Street and 101st Street would be similar to character of Pflumm Road adjacent to the business parks mentioned previously. In addition, since the business/office park uses shown on the Comprehensive Plan extend north on both sides of Dunraven Road along K-7 to Prairie Star Parkway, Dunraven would also be similar to the character of Pflumm Road.

As with any project, the City will review any plan and use submitted for approval on this parcel. The description of the BP-2 zoning district in Section 4-1-B-19 BP-2 of the UDO includes the criteria that “businesses within the BP-2 District shall not create incompatible land-use relationships or hazards to surrounding areas.” Section 4-1-B-27.E of the UDO includes provisions that allow the Governing Body to determine if a use would be allowed within a zoning district.

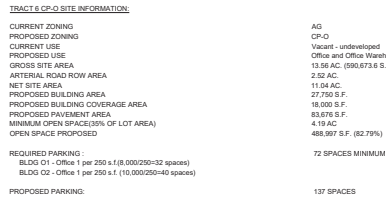


1. FLOOD NOTE: there was NO FEMA identified flood location on the proposed property per Flood Insurance Rate Map Panel No. 4901/2009/0046 and GAO 2009/0047/0 G dated 04/01/2009.
2. Existing utilities data is from a survey prepared by Schilling & Associates, Inc., P.A.
3. Boundary and easement lines have been shown to the greatest extent possible based upon the best available information and are not intended to constitute a warranty.
4. All adjacent parcel information is based upon JACO AIMS and information provided by the City of Jacksonville.
5. All parking lot lighting shall conform to the City codes and ordinances.
6. All exterior lighting shall conform to the City codes and ordinances.
7. All overhead electrical and/or telephone cables, shall be placed within the interior side of or behind building setbacks unless the planning official approves the placement of the cables.
8. All wall and monument signs shall require a sign permit through the codes division.
9. All parking spaces shall be required to be painted in the JCO.
10. All signage must meet the requirements of the City codes, standards and codes.
12. Retaining wall built colored Block Concrete masonry or buff colored Kystrone modular concrete blocks and/or concrete.
13. Approval of this plan does not result in generating a building permit. A separate building permit is required.
14. Approval of this plan does not constitute a complete review of the project for compliance with all applicable codes and ordinances. The applicant is responsible for obtaining all permits. Revisions may be required to make the project building code compliant.
15. The applicant must obtain all necessary permits from the City of Jacksonville, Department of the ADA. The project shall comply with all applicable regulations of the ADA.
16. Approval of this plan does not constitute compliance with the Fire Code. Complete fire code review is required.
17. All parking spaces will be 9' x 19' Drive aisle for a minimum of 25'. Drive aisles for emergency access shall be 26'.
18. All setbacks shall be a minimum of 4' width.
20. The architectural style will be determined by the reviewing of each parcel and will be consistent with the surrounding area.
21. The project will follow the applicable permit requirements when permit plans are required.
22. Fire flow requirements for the project shall be established in accordance with Appendix A of the City of Jacksonville Fire Department.
23. Buildings exceeding 30 feet in height have Proximity to building: At least one of the following: 1) A building with a height of 30 feet or more; 2) A building with a height of 15 feet and a minimum of 30 feet from the building, and shall be positioned parallel to the building; 3) A building with a height of 15 feet and a minimum of 30 feet from the building; 4) A building with a height of 15 feet and a minimum of 30 feet from the building; 5) A building with a height of 15 feet and a minimum of 30 feet from the building; 6) A building with a height of 15 feet and a minimum of 30 feet from the building; 7) A building with a height of 15 feet and a minimum of 30 feet from the building; 8) A building with a height of 15 feet and a minimum of 30 feet from the building; 9) A building with a height of 15 feet and a minimum of 30 feet from the building; 10) A building with a height of 15 feet and a minimum of 30 feet from the building; 11) A building with a height of 15 feet and a minimum of 30 feet from the building; 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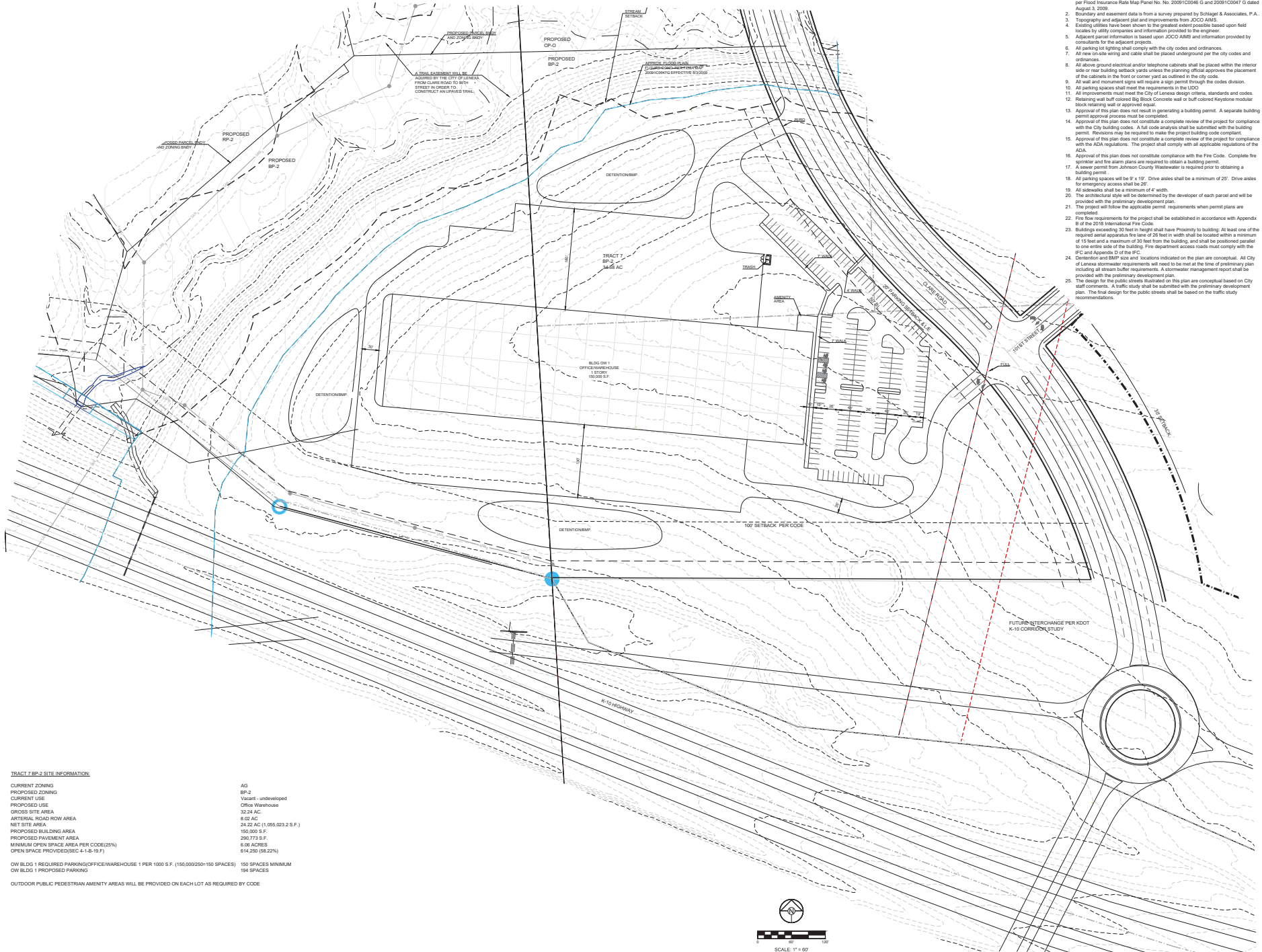
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CP-4

SHEET
CP-4



GENERAL NOTES:

1. FLOOD NOTE: There is no FEMA identified floodplain located on the proposed property. Flood Hazard Map Page No. 10, 09/20/2004M & 09/20/2004T GCD was dated August 3, 2009.
2. Boundary and easement data is from a survey prepared by Stottlage & Associates, L.P.A. dated 01/20/2010.
3. Existing utilities have been shown to the greatest extent possible based upon field investigation and/or records.
4. Applicable information is based upon the ACCO AIMS and information provided by the City of Dallas for the adjacent properties.
5. All parking lot lighting shall comply with the city codes and standards.
6. All electrical and telecommunications equipment shall be placed within the interior side or rear building substructure unless the planning official approves the placement of equipment on the front or side of the building as indicated on the plans.
7. All wall and monument signs shall require a sign permit through the codes division.
8. All parking spaces shall be marked with the City of Dallas standard markings.
9. Retaining walls must meet the City of Dallas design criteria, standards and codes.
10. Retaining walls taller than 6' shall be constructed of full buff colored Keystone masonry blocks.
11. All retaining walls or structures shall be constructed of full buff colored Keystone masonry blocks.
12. Approval of this plan does not result in generating a building permit. A separate building permit is required for construction of the building.
13. Approval of this plan does not constitute a complete review of the project for compliance with all applicable City codes and standards. The applicant is responsible for submitting the permit. Revisions may be required to make the project building code compliant.
14. Approval of this plan does not constitute a complete review of the project for compliance with the ADA regulations. The project shall comply with all applicable regulations of the ADA.
15. Approval of this plan does not constitute compliance with the Fire Code. Complete the Fire Alarm and Fire Protection Code requirements.
16. A sewer permit from Johnson County Wastewater is required prior to obtaining a building permit.
17. All parking spaces shall be 9' x 18'. Drive aisles shall be a minimum of 25'. Drive aisles for emergency access shall be 20'.
18. All parking spaces shall be a minimum of 20'.
19. The architectural style will be determined by the developer of each parcel and will be consistent with the prevailing architectural style of the surrounding area.
20. The project will follow the applicable permit requirements when permit plans are submitted.
21. Fire flow requirements for the project shall be established in accordance with Appendix A of the International Fire Code.
22. Buildings exceeding 30 feet in height shall have Proximity to Building at least one of the following:
 - a. A minimum of 10 feet from the building, and shall be positioned parallel to the building.
 - b. A minimum of 30 feet from the building, and shall be positioned parallel to the building.
23. All fire hydrants shall be indicated on the plans as required. All City of Dallas stormwater requirements will need to be met at the time of preliminary plan including all stormwater buffer requirements. A stormwater management report shall be submitted with the preliminary plan.
24. The design for the public streets illustrated in this plan are conceptual based on City of Dallas traffic study data and shall not be used for engineering or design purposes. The final design for the public streets shall be based on the traffic study.



TRACT 7 BP-2 SITE INFORMATION:

CURRENT ZONING	AG
PROPOSED ZONING	BP-2
CURRENT USE	Vacant / undeveloped
PROPOSED USE	Office Warehouse
GROSS SITE AREA	32.24 AC.
ARTERIAL ROAD ROW AREA	8.62 AC.
NET SITE AREA	24.22 AC. (1,056,023.2 S.F.)
PROPOSED BUILDING AREA	150,000 S.F.
PROPOSED PAVEMENT AREA	290,773 S.F.
MINIMUM OPEN SPACE AREA PER CODE(25%)	6.06 ACRES
OPEN SPACE PAVED/SEC 4.1(B-19%)	614.20 (58.22%)

OW BLDG 1 REQUIRED PARKING(OFFICE/WAREHOUSE 1 PER 1000 S.F. (150,000/250=150 SPACES)	150 SPACES MINIMUM
OW BLDG 1 PROPOSED PARKING	194 SPACES

OUTDOOR PUBLIC PEDESTRIAN AMENITY AREAS WILL BE PROVIDED ON EACH LOT AS REQUIRED BY CODE

GENERAL NOTES

1. FLOOD HAZARD: there is NO FEMA identified flood/hazard location on the proposed property. The Flood Hazard Report shall be prepared by the City Engineer and dated August 7, 2009.
2. The proposed development data is from a survey performed by Stritzinger & Associates, P.A.
3. Topography and adjacent plot and improvements from JCDDA maps.
4. The proposed development shall be in accordance with the City Engineer's review and upon local laws by utility companies and information provided to the engineer.
5. The proposed development shall be in accordance with the City Engineer's review and upon local laws for the adjacent properties.
6. All parking lot lighting shall comply with the city codes and ordinances.
7. All electrical and telephone cables shall be placed within the interior side of the road building setback yards unless the planning official approves the placement of the cables on the front or rear lot.
8. All wall and monument signs will require a sign permit through the city code division.
9. All improvements must meet the City of Lenoire design criteria, standards and codes.
10. All improvements must be constructed in accordance with the City of Lenoire design codes, standards and codes.
11. All improvements must be approved by the City of Lenoire.
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SCHLAGEL
ENGINEERS PLANNERS SURVEYORS LANDSCAPE ARCHITECTS
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PREPARED BY:

SCHLAGEL & ASSOCIATES, P.A.

CEDAR CANYON
REZONING AND CONCEPT PLAN

CEDAR CANYON
REZONING AND CONCEPT PLAN

CANYON CREEK BOULEVARD AND 99TH STREET LENEXA, KANSAS

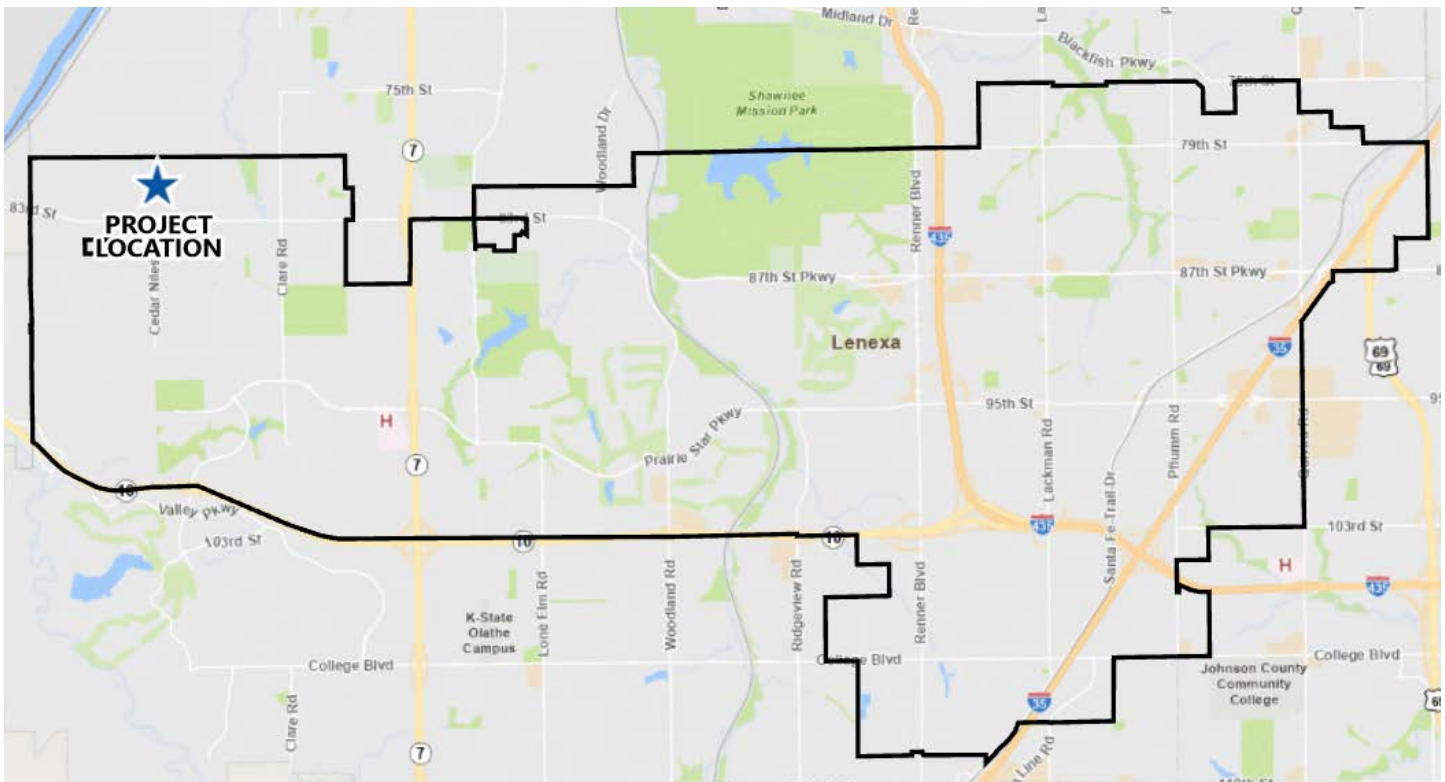
DRAWN BY	REVISION DATE	DESCRIPTION
ANA	9-19-2022	CITY COMMENTS
	10-12-2022	BUILDING
CHECH'D BY:	10-18-2022	TRACE RECORDS OF A.O.S TURN IN ON
ANA	12-6-2022	TRACE & ZONING REVIEW COUNCIL COMMENTS
DATE PREPARED:	12-8-2022	CITY COMMENT REVIEW ANALYSIS
8-20-2022		
PROJ NUMBER:		
270-2-07		

TRACT 7(BP-2)
CONCEPT PLAN

SHEET
CP-7

STONE RIDGE NORTH

Project #:	RZ22-05 & PT22-01P	Location:	North of W. 83 rd Street and Cedar Niles Road
Applicant:	Justin Milburn, Milburn C.E.	Project Type:	Rezoning & Preliminary Plat
Staff Planner:	Kimberly Portillo, AICP	Proposed Use:	Suburban Residential



PROJECT SUMMARY

The applicant proposes to develop approximately 168 acres of land north of W. 83rd Street near Cedar Niles Road as a mixed single-family and two-family subdivision to be known as Stone Ridge North. The proposal includes a rezoning from the AG Agricultural, RP-1 Planned Residential Low Density, NP-O Planned Neighborhood Office, and CP-1 Planned Neighborhood Commercial Zoning Districts to the RP-1 Planned Residential Low-Density and RP-2 Planned Residential Intermediate Zoning Districts. The proposal also includes a preliminary plat for 381 single-family lots, 53 two-family lots, multiple tracts for stormwater, landscaping, and amenities, easements, and public right-of-way for new public streets and related infrastructure improvements. The preliminary plat is intended to also serve as the preliminary plan. The applicant requests approval of the rezoning and preliminary plat for the development. The preliminary plan also includes requests for multiple deviations from the Unified Development Code (UDC) including: lot size for 182 lots, lot width for 302 lots, corner lot width for 7 lots, lot depth for 28 lots, and front yard setback for 381 lots, and minimum site area per dwelling unit for 32 lots. Staff supports the proposed deviations. This project requires a Public Hearing.

STAFF RECOMMENDATION: APPROVAL

SITE INFORMATION

This site was annexed into the City of Lenexa in November of 1986, at which time it was zoned and used as agricultural. It was subsequently rezoned to its current designation in 2009 as part of the larger Cedar Niles Estates rezoning and concept plan (RZ09-02 and PL09-01CP), which included a total of 278.63 acres north of W. 83rd Street and west of Mize Boulevard. The 2009 plan rezoned the property to RP-1, RP-3, NP-O and CP-1 with designated stream corridor buffer areas. The site has since remained vacant.

LAND AREA (AC)	BUILDING AREA (SF)	CURRENT ZONING	COMP. PLAN
168.35	N/A	AG, RP-1, NP-O, CP-1	Suburban Residential



Exhibit 1: Aerial Image of Subject Site

LAND USE REVIEW

The proposed use is a residential subdivision with single-family and two-family lots. The Future Land Use designation for this location is Suburban Residential, which is defined as: *Low density detached single-family housing not exceeding a gross density of 3.5 dwelling units per acre*. Most of the proposed development is single-family and fits within this land use. A smaller portion of the site is proposed for use as two-family lots; however, the overall density for the two-family portion is 3.04 dwelling units per acre. With the low density and high-quality architectural design, Staff is comfortable that the two-family portion of the development fits within the intent of the Suburban Residential land use category, while providing a variety of housing typologies onto the Lenexa market.

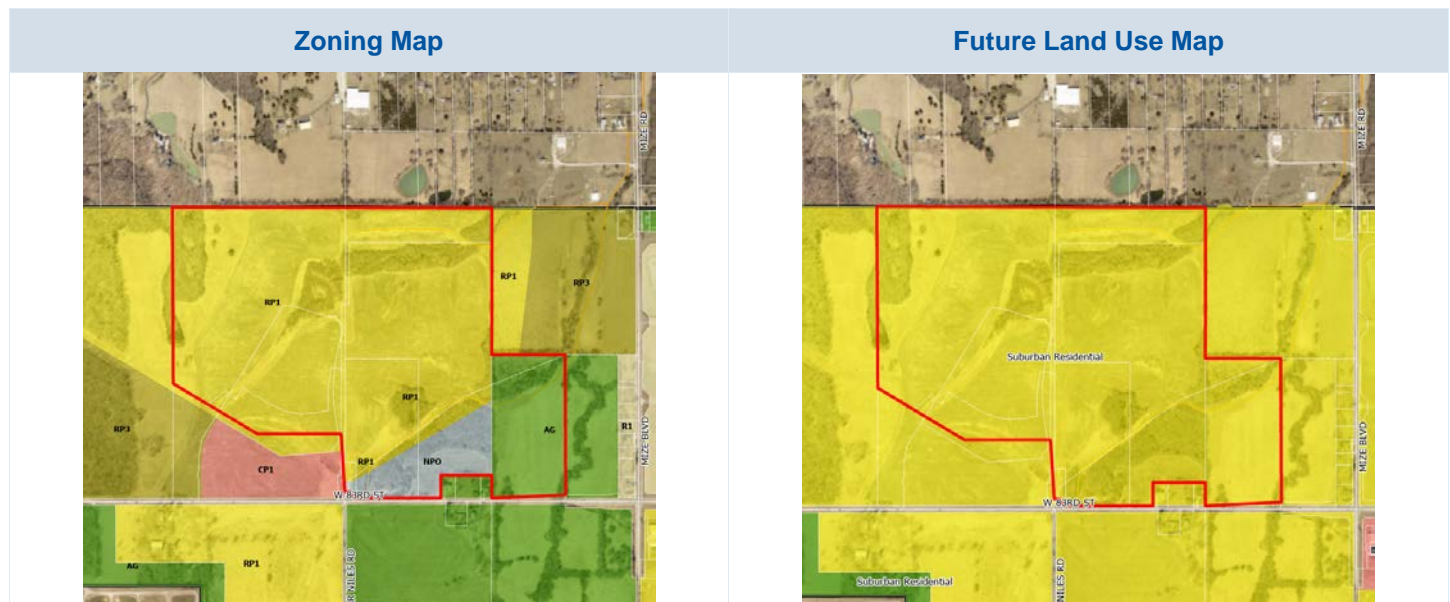


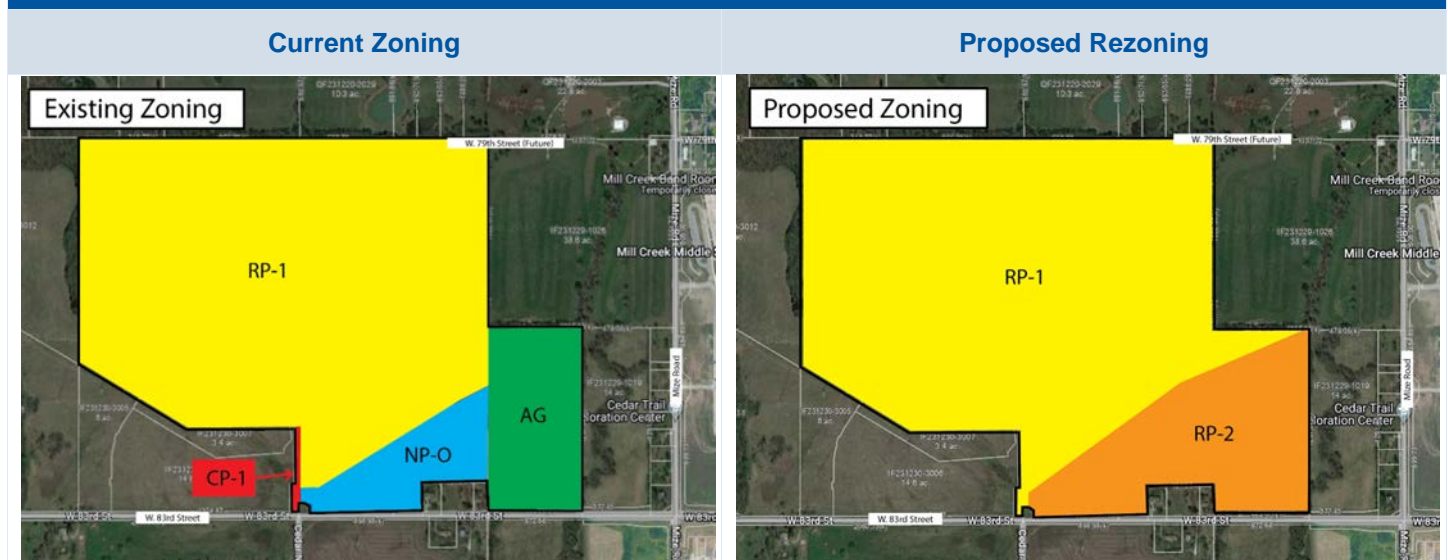
TABLE 1: COMPARISON OF SURROUNDING PROPERTIES

Vicinity	Land Use Designation	Zoning	Current Use
Subject Property	Suburban Residential	AG, RP-1, NP-O, CP-1	Undeveloped
North	Estate Residential Neighborhood (Shawnee)	RE Residential Estates & AG Agricultural (Shawnee)	Agricultural & Single-Family Residential (Shawnee)
South	Suburban Residential	RP-1 Planned Residential Low Density & AG Agricultural	Stone Ridge Single-Family Subdivision (future) & Agricultural
East	Suburban Residential	AG Agricultural, RP-1 Planned Residential Single-Family	Undeveloped
West	Suburban Residential	RP-1 Planned Residential Single-Family & RP-3 Planned Residential Medium High-Density	Undeveloped

The proposed use is compatible in context of the existing land uses and approved plans on adjacent sites. Suburban-style development has been encroaching west toward this area for the past two decades and is anticipated by the Comprehensive Plan and Future Land Use Map. Mill Creek Middle School, located near the proposed development at 83rd and Mize Boulevard, was constructed in 2005 to serve the growing population of Unified School District 232 (DeSoto). A 20-acre parcel is reserved for park land adjacent to the school. Recent residential development in the area includes Stone Ridge, a single-family subdivision with similar lots, just south of W. 83rd Street.

REZONING REVIEW

TABLE 2: REZONING ANALYSIS



Staff provides the following analysis for the review criteria within [Section 4-1-G-5](#) of the UDC.

1. The character of the neighborhood.

This is an agricultural neighborhood that is experiencing suburban residential growth. While most of the land is still agricultural, residential developments have been approved surrounding or in the vicinity of the proposed use. Stone Ridge, a rezoning from AG to RP-1 and plat for 105 single-family lots was approved in two phases at the southwest corner of 83rd and Cedar Niles Road in 2022. A rezoning to RE, Residential Estates and preliminary plat for Riverbend, consisting of 24 single-family lots, was approved in December of 2021 to the west of the proposed Stone Ridge North development. Lots within the Riverbend subdivision have lot sizes between two and two and a half acres. The subject property abuts the City of Shawnee along the northern property line, where uses are large-lot residential and agricultural and lots range in size from two and a half to ten acres.

2. The zoning and use of properties nearby.

The zoning and uses of nearby properties are provided in the Table 2. The proposed uses of single-family and two-family residential are compatible with existing and future planned uses of the land.

3. The suitability of the subject property for the uses to which it has been restricted.

The property is suited for the rezoning to RP-1, which fits within the designated future land use of Suburban Density residential. The portion of the property to be rezoned to RP-2 is proposed to have a density of 3.04 units per acre, which, despite being zoned for two-family, fits within the character of surrounding approved single-family developments and the designated future land use.

4. The extent to which the proposed use will detrimentally affect nearby property.

The proposed use will not detrimentally affect nearby property as the use fits within the designated future land use of the property. The proposed subdivision's density, landscaping, pedestrian connections, and street networks are appropriate for this area. The plans also account for access to the three excluded lots along W. 83rd Street should they be redeveloped in the future.

5. The length of time the subject property has remained vacant as zoned.

The subject property and surrounding lots were rezoned in 2009 as part of Cedar Niles Estates, which planned for a mix of single-family homes, villas, townhomes, and apartments with small community retail and office concentrated near 83rd and future Cedar Niles Road. The project was never developed as such and has remained vacant since.

6. The relative gain to public health, safety, and welfare due to the denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.

Staff is of the opinion that denial of this rezoning and preliminary plat would have no gain to public health, safety, or welfare.

7. Recommendation of City's permanent professional staff.

See Staff's recommendation and the end of this report.

8. Conformance of the requested change to the adopted or recognized Master Plan being utilized by the City.

The Master Plan designation for this area is Suburban Residential, which calls for primarily detached single-family housing at a density of less than 3.5 units per acre. This proposal does include some two-family units; however, about 88% of the proposed lots are single-family. Staff supports the variety of housing types and lot sizes provided and, with a proposed density no greater than 3.04 units per acre in the RP-2 Zoning District, feel this proposal fits within the intent of the Master Plan.

9. The availability and adequacy of required utilities and services to serve the proposed use. These utilities and services include, but are not limited to, sanitary and storm sewers, water and electrical service, police and fire protection, schools, parks and recreation facilities, etc.

Required utilities, including sewer, water, electricity, and communications, are available nearby and are adequate to service the development. The developer is responsible for extending various utilities into the site. Schools and designated future park land are located nearby and will serve the development.

10. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the street network influenced by the use, or present parking problems in the vicinity of the property.

The applicant provided a traffic impact study from July 2022 that analyzed the proposed development's impact to the 83rd Street & Cedar Niles Road intersection. Engineering Staff reviewed the study and found it to be generally acceptable with a few exceptions as noted here. First, the study implies that some improvements can be deferred until later phases; however, Staff requires all improvements at the time a connection is made to the street network.

The improvements the developer will be required to construct at its cost consist of right and left turn lanes at the access connections to 83rd Street. Staff notes that the study identified turn lane lengths only accounting for storage. However, Staff will require that the turn lanes be lengthened to include necessary length for deceleration. The 83rd Street & Cedar Niles Road intersection will not warrant a traffic signal based on the existing traffic along 83rd Street, the approved Stone Ridge South development, and this proposed Stone Ridge North development. Traffic volumes along 83rd Street are projected to increase from 6,200 vehicles per day to 11,370 vehicles per day with the proposed development. A two-lane roadway such as 83rd Street is considered at full capacity near 14,000 vehicles per day.

A necessary future improvement will be construction of Cedar Niles Road from 83rd Street approximately 700-ft north. Cedar Niles Road will require auxiliary turn lanes as development of the future Roman Catholic Archdiocese property is developed. Concurrently, appropriate turn lanes must be constructed on 83rd Street into the future Roman Catholic Archdiocese property at the time this property develops.

With these intersection/access improvements at the time of connection, it is Staff's belief that the proposed development's traffic can be adequately served by the street network system.

11. The environmental impacts the proposed use will generate including, but not limited to, excessive storm water runoff, water pollution, air pollution, noise pollution, excessive nighttime lighting or other environmental harm.

The development will utilize a variety of methods for stormwater maintenance and best management practices (BMPs), including maintenance of natural stream corridors and native vegetation, use of extended wet detention basins, and dry detention basins. This use is anticipated to be equal to similar subdivision developments will not generate excessive water pollution, air pollution, noise pollution, nighttime lighting, or other environmental harm.

12. The extent to which the proposed development would adversely affect the capacity or water quality of the stormwater system, including without limitation, natural stream assets in the vicinity of the subject property.

There are three streams located within the project boundaries. All three are located in tracts that encompass the stream and a required stream buffer zone. Natural vegetation will be preserved along the stream corridors. The layout has been designed around the existing topography of the site so as to not adversely affect the capacity or water quality of the stormwater system.

13. The ability of the applicant to satisfy any requirements (e.g. site plan, etc.) applicable to the specific use imposed pursuant to the zoning regulations in this Chapter and other applicable ordinances.

The applicant requested deviations from the Unified Development Code as discussed in subsequent sections of this report.

PRELIMINARY PLAT REVIEW

The layout proposes extension of future collector Cedar Niles Road north of 83rd Street, where it will arc to connect with future 79th Street when 79th Street is improved westward from Mize Boulevard. The proposed subdivision layout includes a total of 434 lots (381 single-family, 53 two-family), 20 tracts, and 13 streets and cul-de-sacs divided into four phases. The development is divided into two areas to separate the single-family lots from the two-family lots, which are divided by a stream and stream buffer zone on Tracts B and C. The portion north of the stream is proposed as 381 single-family lots on 133.52 acres with a density of 2.85 du/ac. Fifty-three two-family lots on 34.82 acres with a density of 3.04 du/ac are proposed south of the stream in the southeast corner of plat.

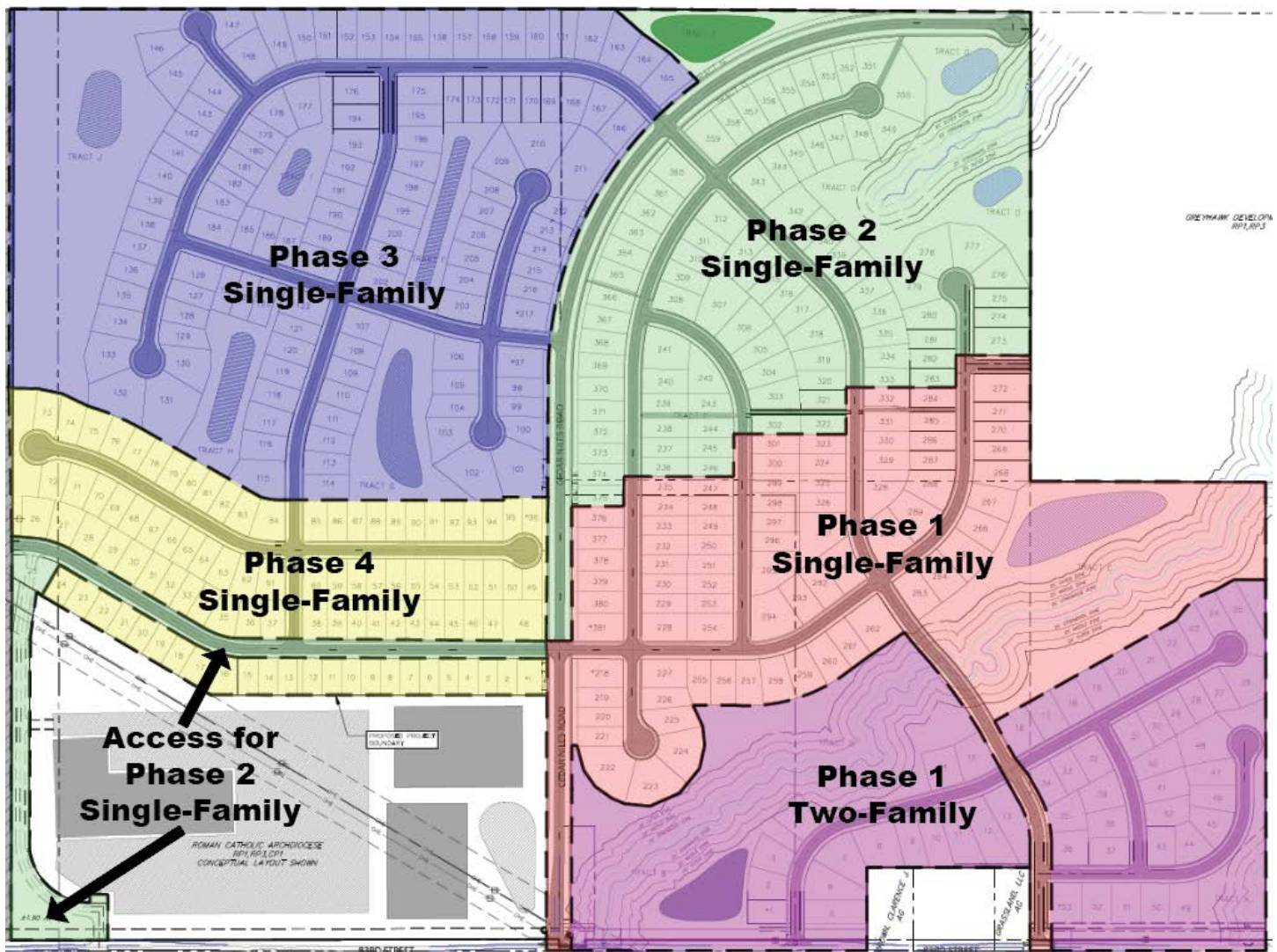


Exhibit 2: Phasing Plan

Phase 1 includes all 53 two-family lots and the 79 single-family lots immediately north of the stream buffer division. Tract A, which houses a separate stream, and Tracts B and C of stream buffer are both included in

phase 1. Tract R provides a mid-block pedestrian connection. Infrastructure to be constructed by the developer with this phase includes two access points off 83rd Street, including the initial section of Cedar Niles Road north of 83rd and a secondary access approximately a quarter mile east into the two-family area.

Phase 2 includes 86 single-family lots north of phase 1 and east of Cedar Niles, as well as expansion of Cedar Niles/ 79th Street and continuation of four streets from Phase 1. Tract D houses stream buffer while Tracts K and O provide landscape buffer areas. Due to fire code access restrictions, an access road along the western portion of the Roman Catholic Archdiocese property must be complete prior to permitting for phase 2.

Phase 3 includes 121 single-family lots at the northwestern corner of the subject site, abutting the City of Shawnee to the north and west of future Cedar Niles Road/79th Street. Tracts F, G, H, I, and J serve stormwater purposes for this phase. Two roadway connections going east from Cedar Niles Road will be constructed by the developer with this phase, with four cul-de-sacs and one through street into the phase 4 area.

Phase 4 is the final phase and includes an additional 95 single-family lots with two cul-de-sacs and a connection east-west to the adjacent future single-family residential subdivision known as Riverbend.

Multiple storm shelters are required because the homes are proposed to be slab-on-grade without standard internal storm shelters. Single-family lots 11, 62, 178, 220, 264, 366 and two-family lot 35 are designated as storm shelter locations and will be developed with a shelter structure for nearby residents instead of being developed with single-family homes. Lot 35 in the two-family area will house a storm shelter. The storm shelters are required because the homes are proposed to be slab-on-grade without internal storm shelters.

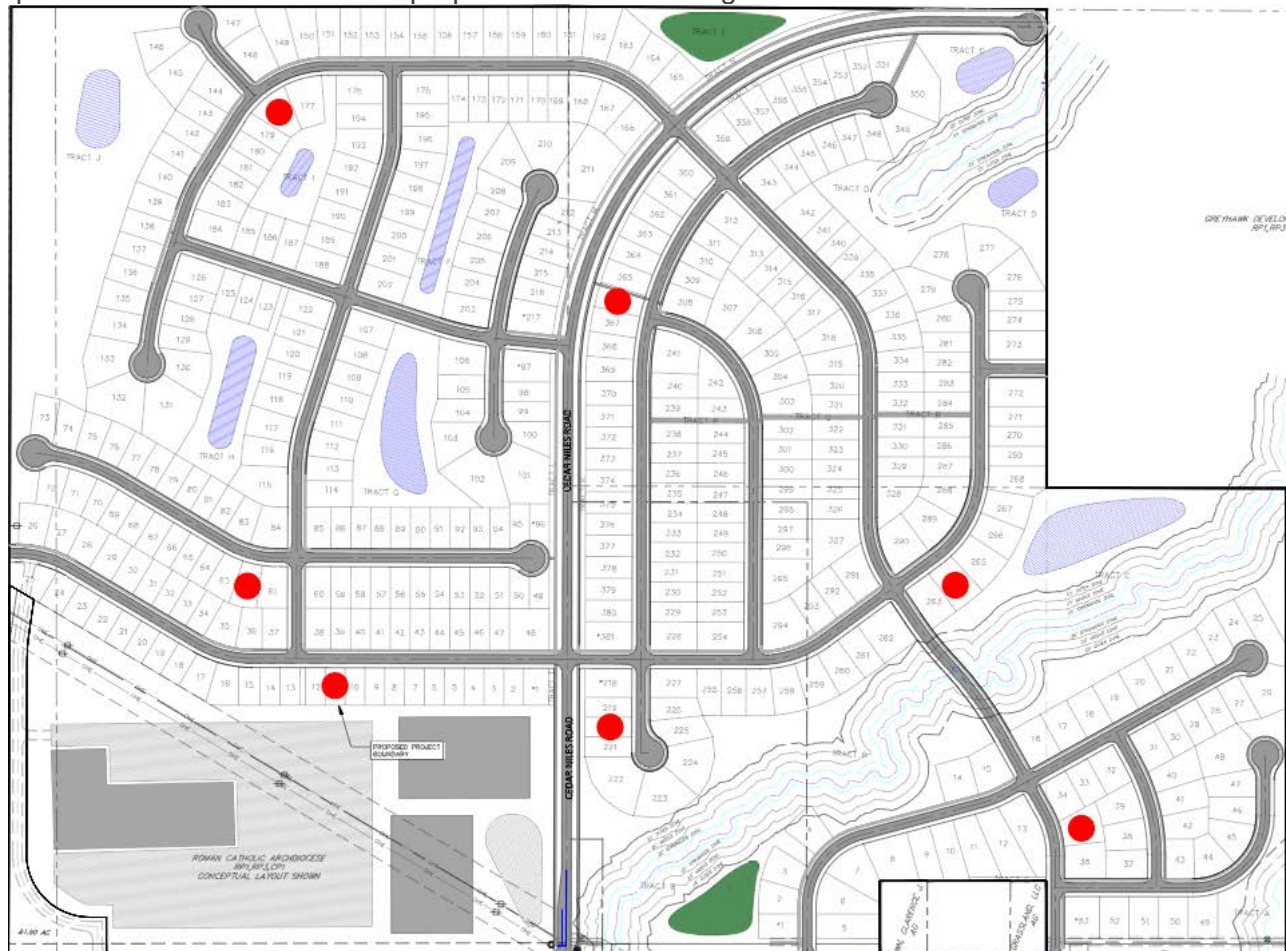


Exhibit 3: Locations of Designated Storm Shelters

Tracts A through J are provided for stormwater detention and BMPs and will be maintained by the Homeowners Association. Park areas are planned for Tracts E and B and additional amenities, such as walking trails, will be provided along the stream. Amenity areas will be owned and maintained by the Homeowners Association. Public rights-of-way and required easements will be dedicated to the City of Lenexa with the final plat.

TABLE 3: TRACTS

Tract	Phase	Purpose	Tract	Phase	Purpose
A	1	Stream buffer	K	1, 2	Landscape buffer
B	1	Stream buffer	L	3, 4	Landscape buffer
C	1	Stream buffer	M	3	Landscape buffer
D	2	Stream buffer and stormwater detention	N	2, 3	Landscape buffer
E	2	Stormwater detention	O	2	Landscape buffer
F	3	Stormwater detention	P	2	Pedestrian connection
G	3	Stormwater detention	Q	2	Pedestrian connection
H	3	Stormwater detention	R	1	Pedestrian connection
I	3	Stormwater detention	S	1	Future access reserve
J	3	Stormwater detention	T	4	Landscape buffer

Pedestrian connectivity is provided through a system of sidewalks and mid-block connections. Sidewalks are provided on both sides of Cedar Niles Road and along one side of minor roads. Pedestrian connections are provided mid-block on the eastern portion of the development where north-south blocks are longer to provide connectivity to the east in the direction of the Mill Creek Middle School. Additionally, a network of trails will be provided in designated stream buffer tracts.

DEVIATIONS

The applicant requested multiple deviations for the proposed development. The intent of the subdivision is to provide moderately sized, ranch style homes with high-quality exterior materials to appeal to a specific market that desires energy-efficient, low-maintenance homes. To achieve low-maintenance and energy-efficient homes, the applicant proposes elevating the exterior architecture beyond minimum design standards by using four-sided stone and brick exteriors and reducing lot sizes for some lots to minimize exterior maintenance needs. The lower infrastructure costs associated with smaller lots allows the applicant to use the higher-quality material while not driving up the overall cost of the home. The proposed reduced lot sizes call for reductions in the required lot size, width, depth, front yard setback, and site area per dwelling unit as listed in Tables 4 and 5.

TABLE 4: RP-1 DEVIATIONS

Deviation	# of Lots Needing Deviation	Requirement	Minimum Proposed	Maximum Difference
Front Yard Setback	381	30 ft	25 ft	-5 ft
Min. Lot Size	182	8,000 sf	5,431 sf *	- 2,569 sf
Min. Lot Width	279	70 ft	55 ft	-15 ft
Min. Corner Lot Width	7	80 ft	70 ft	-10 ft
Min. Lot Depth	28	115 ft	102 ft	-13 ft

TABLE 5: RP-2 DEVIATIONS

Deviation	# of Lots Needing Deviation	Requirement	Minimum Proposed	Maximum Difference
Min. Site Area per Dwelling Unit	32	5,000 sf	3,741 sf	- 1,259 sf
Min. Lot Width	23	80 ft	66 ft	-14 ft

**Note: The preliminary plat shows a requested deviation for 3,220 sf for a minimum lot size of 4,780 sf. Based on the lot area table also provided in the plan set, Staff found the smallest proposed lot to be 5,431 sf and thus suggests a modified request be approved.*

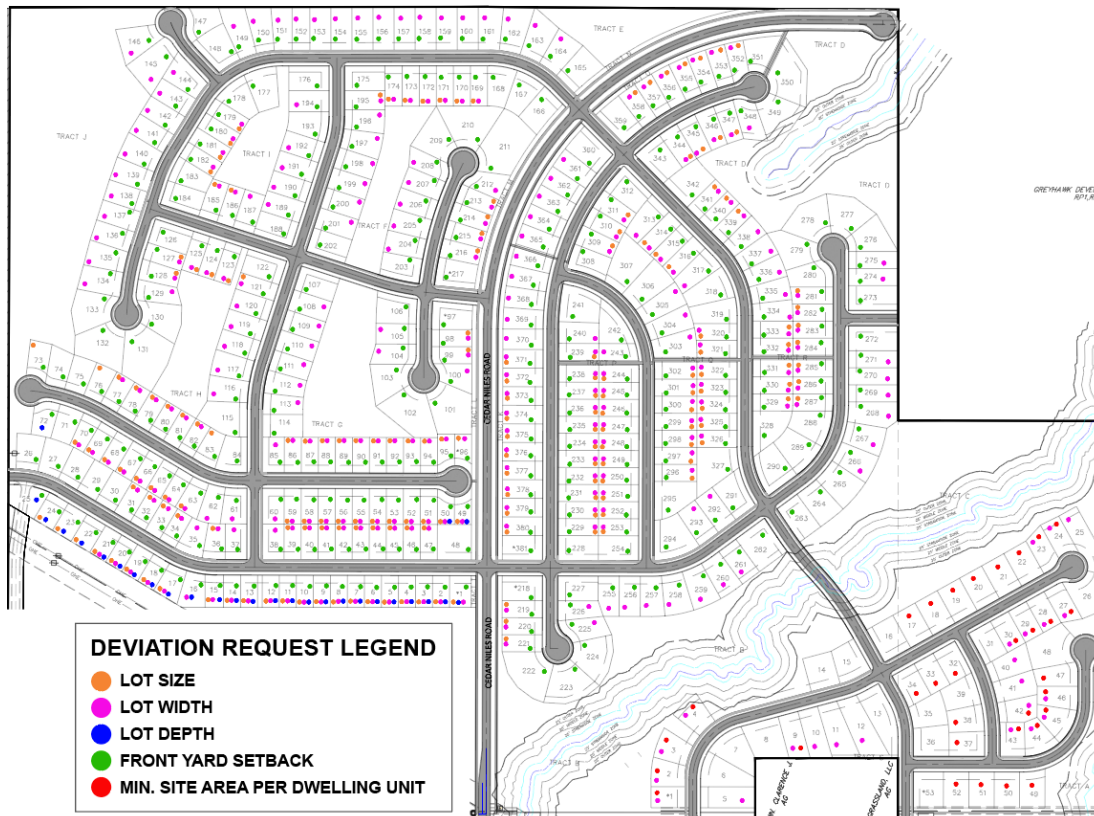


Exhibit 4: Deviation Requests by Lot

The proposal includes a variety of floorplan layouts with an average size of 2,000 square feet and Dwelling Classification E. Two primary styles are proposed for the single-family area: cottage lots and typical single-family lots. The cottage lot floorplans have a footprint of approximately 2,050 square feet and would be placed on the smaller single-family lots, which are Lots 1-96. Typical single-family ranch designs with a footprint between approximately 2,500 square feet and 3,000 square feet will be located on larger lots with greater depth. Staff reviewed the sample layouts to ensure side and rear yard setbacks are met with the proposed layouts.

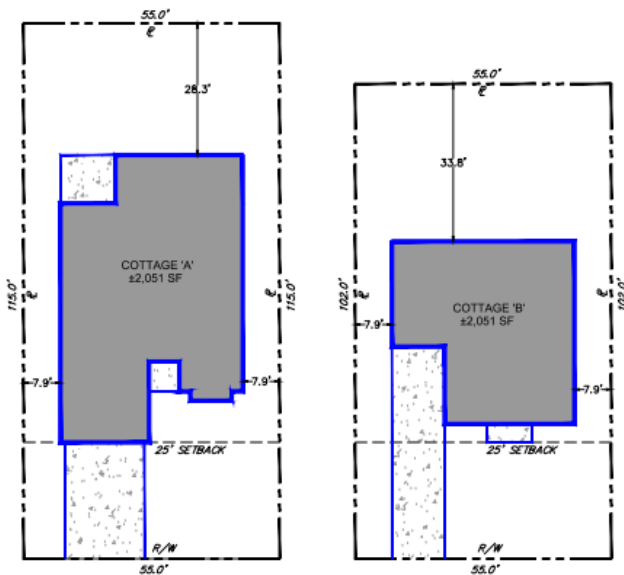


Exhibit 5: Smaller 'cottage' style floorplans of approximately 2,000 SF will be placed on the smaller single-family lots 1-96.

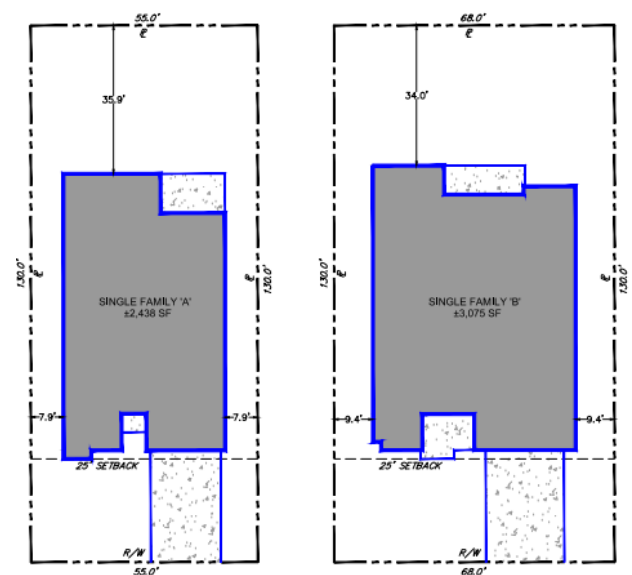


Exhibit 6: A standard ranch-style single family home floorplan of approximately 2,500 to 3,000 SF is proposed for the remaining single-family lots.

Staff worked with the applicant to modify the proposed deviations so that the development meets the applicant's intent of providing a unique product to the housing market while also remaining consistent with the quality development Lenexa is accustomed to with subdivision design. Design modification requested by Staff included greater width along primary corridors, including Cedar Niles Road/79th Street and the east-west through street that will connect to adjacent development. Staff also worked with the applicant to reduce the intensity of deviations requested, both in total quantity of deviations and degree of deviations proposed. This resulted in an overall reduction of the proposed number of single-family lots from 394 lots in the initial version of the plan to the currently proposed 381 lots.

A recently approved development south of the subject property, Stone Ridge, located at the southwest corner of 83rd and Cedar Niles Road received similar deviations for lot width, corner lot width, front yard setback, and lot area on many of the 105 approved lots.

★ Discussion Item: The Planning Commission must consider the requested deviations for front yard setback, lot size, lot width, corner lot width, lot depth, and minimum site area per dwelling unit.

ARCHITECTURE



Exhibit 7: Sample architectural style



Exhibit 8: Sample architectural style



Exhibit 9: Sample brick material



Exhibit 10: Sample brick material

Proposed elevations of the homes include use of brick and masonry materials. The proposed homes are slab-on-grade and do not offer a basement. Architectural details include front facing garages and covered entrances.

LANDSCAPING

Landscaping buffers are required for residential uses adjacent to arterial roads. This requirement applies to 83rd Street and Cedar Niles Road/79th Street. A tract of land for landscape buffering is provided and includes a black steel buffer fence. Street trees along all streets are required at a rate of 1 tree per 40 linear feet of street frontage. Two additional deciduous trees are to be provided per lot. The proposed development is capable of meeting landscaping and buffering requirements.

REVIEW PROCESS

- This project requires a recommendation from the Planning Commission and final approval by the City Council. Pending a recommendation from the Planning Commission, the project is tentatively scheduled for consideration from the City Council on February 7, 2023.
- The applicant should inquire about additional City requirements, such as permits and development fees.

RECOMMENDATION FROM PROFESSIONAL STAFF

★ Conduct a Public Hearing.

★ Staff recommends approval of the proposed rezoning and preliminary plat for Stone Ridge North.

- This application supports the following goal from Vision 2040:
 - Vibrant Neighborhoods

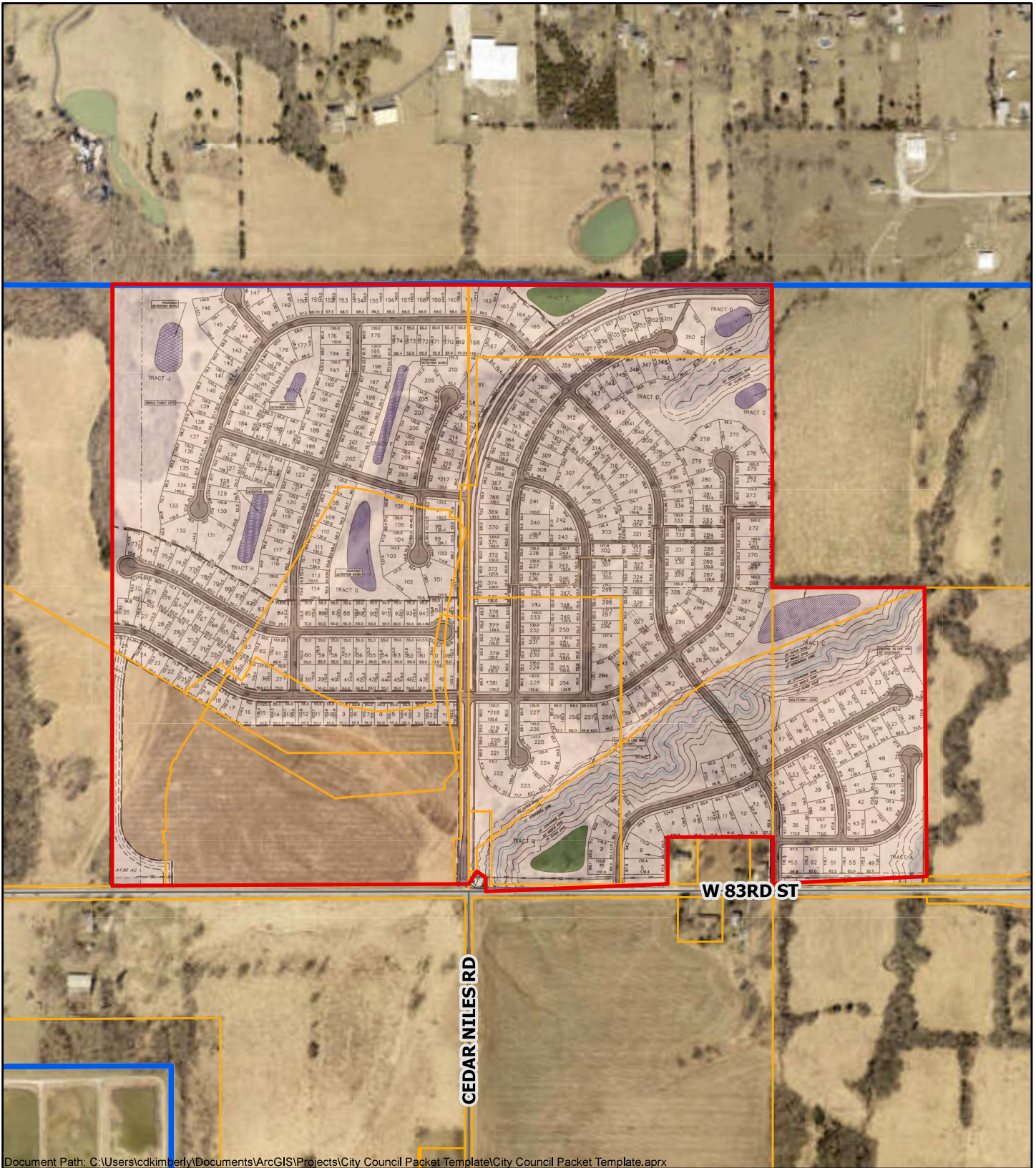
REZONING

Staff recommends **approval** for rezoning property from AG, RP-1, NP-O, and CP-1 to RP-1 and RP-2 for RZ22-05 – **Stone Ridge North** located north of 83rd Street near Cedar Niles Road, for a single-family and two-family residential subdivision.

PRELIMINARY PLAT

Staff recommends **approval** of the preliminary plat for PT22-01P – **Stone Ridge North** located north of 83rd Street near Cedar Niles Road, for a single-family and two-family residential subdivision, with the following deviations from the Unified Development Code:

1. Lot width, lot depth, lot area, corner lot width, and front yard setback in the RP-1 Zoning District as noted within the Staff Report and the proposed plans.
2. Lot width and lot area in the RP-2 Zoning District as noted within the Staff Report and the proposed plans.

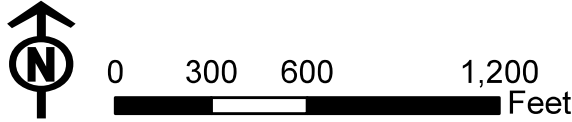


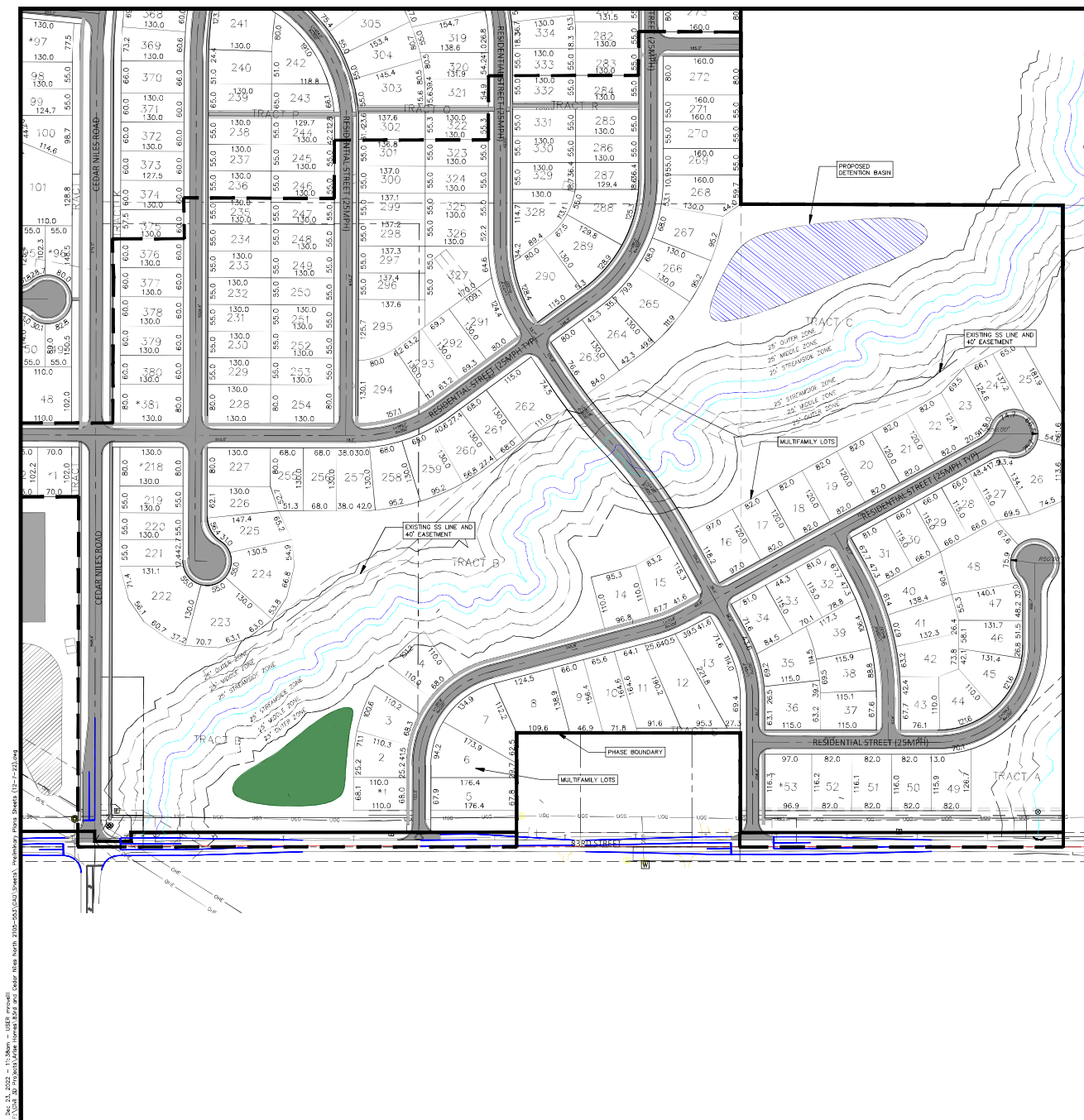
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Data Source: City of Lenexa and Johnson County Kansas
For further information, please call 913-477-7500

Stone Ridge North Rezoning & Preliminary Plat

RZ22-05 & PT22-01P





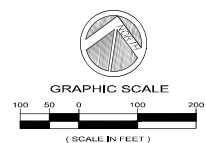
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 - PROPOSED WATER
 - EXISTING STORM SEWER
 - PROPOSED STORM SEWER
 - BUILDING LINE
 - PROPOSED R/W
 - EXISTING R/W
 - PROPERTY LINE



Milburn Civil Engineering, LLC
MILBURN CIVIL ENGINEERING, LLC
33135 W 83RD ST
PO BOX 4566018
LENEXA, KS 66227



STONE RIDGE NORTH
PRELIMINARY PLAT
83RD ST & CEDAR NILES RD
LENEXA, KS 66227



DATE 12/29/2022
DESIGNED BY ALB
CHECKED BY MB

REVISIONS

NO.	DESCRIPTION	DATE

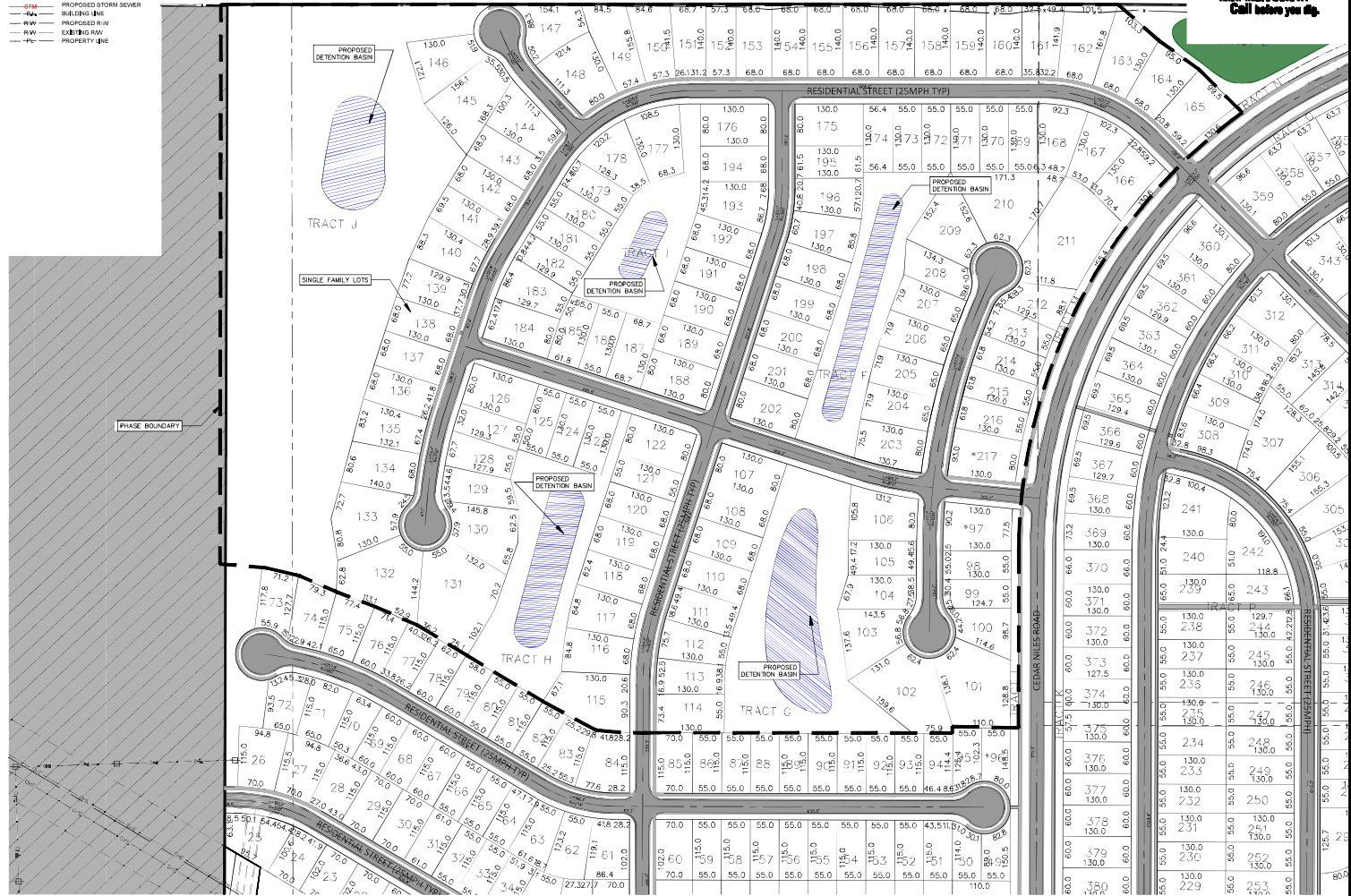
PRELIMINARY PLAT
PHASE 1

C105

PRELIMINARY-NOT FOR CONSTRUCTION

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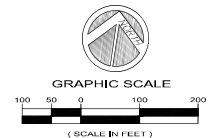
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 - PROPOSED CONTOUR
 - PROPOSED SANITARY
 - PROPOSED SANITARY
 - PROPOSED WATER
 - PROPOSED WATER
 - EXISTING WATER
 - PROPOSED STORM SEWER
 - BUILDING LINE
 - PROPOSED R/W
 - EXISTING R/W
 - PROPERTY LINE



Milburn Civil Engineering, LLC
MILBURN CIVIL ENGINEERING, LLC
23135 W 83RD ST
OVERLAND PARK, KS 66208
913.583.0367



STONE RIDGE NORTH
PRELIMINARY PLAT
83RD ST & CEDAR NILES RD
LENEXA, KS 66227



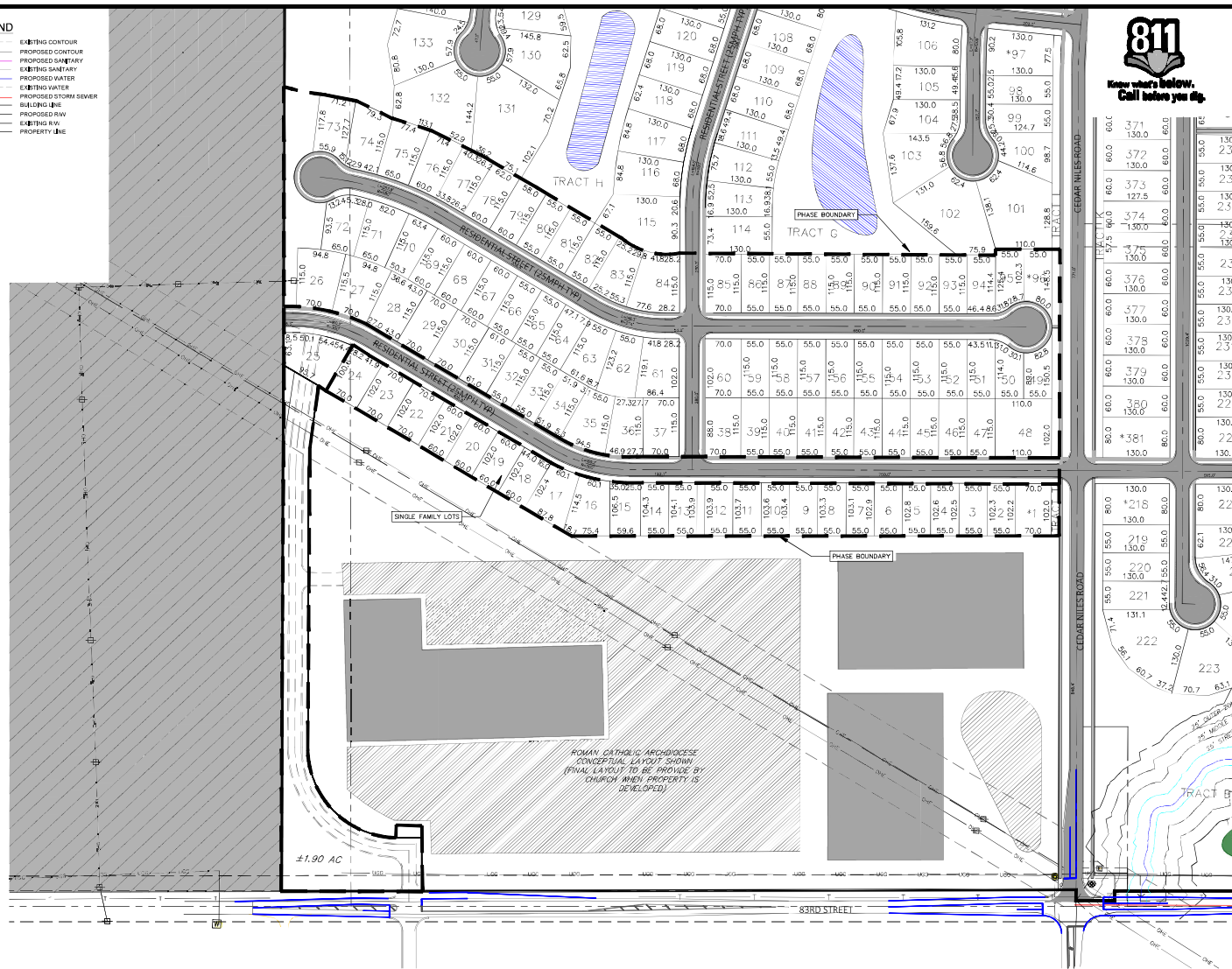
DATE: 12/29/2022
DESIGNED BY: ALB
CHECKED BY: MB

REVIEWS	REVISIONS

PRELIMINARY PLAT
PHASE 3

C107

PRELIMINARY-NOT FOR CONSTRUCTION



MILBJRN CIVL ENGINEERING, LLC
33135 W 83RD ST
DE SOTO, KS 66018
913 583 0367



PRELIMINARY PLAT

83RD ST & CEDAR NILES RD
LENEXA, KS 66227

DATE	12/23/2023
DESIGNED BY	AJS
CHECKED BY	M

REVISIONS REVISIONS

PRELIMINARY PLAT
PHASE 4

C108



GRAPHIC SCALE

(SCALE IN FEET)

PRELIMINARY-NOT FOR CONSTRUCTION

Dec 23, 2022 - 11:42am - USER mronell
\\CHS-3D\Projects\Arise\Homes\Bird and Cedar Niles North\2105-553\CAO\Sheets\ Preliminary Plans Sheets (12-7-22).dwg

BALL, DAVID F.
AG

SCANLON, MARK D.
AG

HILLHOUSE, DENNIS E.
AG

HILLHOUSE, DENNIS E.
AG

HILLHOUSE, DENNIS E.
AG

WURZ, MELISSA
AG

FREEMAN, BASIL E.
AG

OREN FARM, LLC
AG



Milburn Civil Engineering, LLC

MILBURN CIVIL ENGINEERING, LLC
33135 W 83RD ST
DE SOTO, KS 66208
913 583 0367



GREYHAWK DEVELOPMENT, LLC
RPI, RPS

MAIZE BLVD

STONE RIDGE NORTH

PRELIMINARY PLAT

83RD ST & CEDAR NILES RD
LENEXA, KS 66227

DATE 12/29/2022
DESIGNED BY ALB
CHECKED BY MB

REVISIONS

DEVIATION SUMMARY

C109

DEVIATION LEGEND

LOT SIZE -

LOT WIDTH -

LOT DEPTH -

FRONT SETBACK -

MIN SITE AREA PER DWELLING UNIT -

* DENOTES START/STOP FOR LOT NUMBERING

RP-1 Minimum Building Setbacks		
Description	Code	Deviation
Front/Street	30 ft	YES*
Side/Street	20 ft	NO
Side Yard	7 ft	NO
Rear Yard	20 ft	NO

* requesting 25 ft for lots 1-381 single family

RP-2 Minimum Lot Requirements		
Description	Code	Deviation
Width	80 ft	YES
Min Site Area	5000 sf	YES
Per Dwelling Unit		

RP-2 Minimum Building Setbacks		
Description	Code	Deviation
Front/Rear	25 ft	NO
Side/Street	20 ft	NO
Side Yard	7 ft	NO
Rear Yard	20 ft	NO

RP-3 Minimum Lot Requirements		
Description	Code	Deviation
Width	70 ft	YES
Depth	115 ft	YES
Area	8000 sf	YES

DEVIATION REQUEST SUMMARY - SINGLE FAMILY RESIDENTIAL (RP-3)				
DEVIATION TYPE	LOTS	DEVIATION REQUEST	CODE REQUIREMENT	
LOT WIDTH REDUCTION	3 39,131, 39,132, 39,133, 39,134, 39,135, 39,136, 39,137, 39,138, 39,139, 39,140, 39,141, 39,142, 39,143, 39,144, 39,145, 39,146, 39,147, 39,148, 39,149, 39,150, 39,151, 39,152, 39,153, 39,154, 39,155, 39,156, 39,157, 39,158, 39,159, 39,160, 39,161, 39,162, 39,163, 39,164, 39,165, 39,166, 39,167, 39,168, 39,169, 39,170, 39,171, 39,172, 39,173, 39,174, 39,175, 39,176, 39,177, 39,178, 39,179, 39,180, 39,181, 39,182, 39,183, 39,184, 39,185, 39,186, 39,187, 39,188, 39,189, 39,190, 39,191, 39,192, 39,193, 39,194, 39,195, 39,196, 39,197, 39,198, 39,199, 39,200, 39,201, 39,202, 39,203, 39,204, 39,205, 39,206, 39,207, 39,208, 39,209, 39,210, 39,211, 39,212, 39,213, 39,214, 39,215, 39,216, 39,217, 39,218, 39,219, 39,220, 39,221, 39,222, 39,223, 39,224, 39,225, 39,226, 39,227, 39,228, 39,229, 39,230, 39,231, 39,232, 39,233, 39,234, 39,235, 39,236, 39,237, 39,238, 39,239, 39,240, 39,241, 39,242, 39,243, 39,244, 39,245, 39,246, 39,247, 39,248, 39,249, 39,250, 39,251, 39,252, 39,253, 39,254, 39,255, 39,256, 39,257, 39,258, 39,259, 39,260, 39,261, 39,262, 39,263, 39,264, 39,265, 39,266, 39,267, 39,268, 39,269, 39,270, 39,271, 39,272, 39,273, 39,274, 39,275, 39,276, 39,277, 39,278, 39,279, 39,280, 39,281, 39,282, 39,283, 39,284, 39,285, 39,286, 39,287, 39,288, 39,289, 39,290, 39,291, 39,292, 39,293, 39,294, 39,295, 39,296, 39,297, 39,298, 39,299, 39,300, 39,301, 39,302, 39,303, 39,304, 39,305, 39,306, 39,307, 39,308, 39,309, 39,310, 39,311, 39,312, 39,313, 39,314, 39,315, 39,316, 39,317, 39,318, 39,319, 39,320, 39,321, 39,322, 39,323, 39,324, 39,325, 39,326, 39,327, 39,328, 39,329, 39,330, 39,331, 39,332, 39,333, 39,334, 39,335, 39,336, 39,337, 39,338, 39,339, 39,340, 39,341, 39,342, 39,343, 39,344, 39,345, 39,346, 39,347, 39,348, 39,349, 39,350, 39,351, 39,352, 39,353, 39,354, 39,355, 39,356, 39,357, 39,358, 39,359, 39,360, 39,361, 39,362, 39,363, 39,364, 39,365, 39,366, 39,367, 39,368, 39,369, 39,370, 39,371, 39,372, 39,373, 39,374, 39,375, 39,376, 39,377, 39,378, 39,379, 39,380, 39,381, 39,382, 39,383, 39,384, 39,385, 39,386, 39,387, 39,388, 39,389, 39,390, 39,391, 39,392, 39,393, 39,394, 39,395, 39,396, 39,397, 39,398, 39,399, 39,400, 39,401, 39,402, 39,403, 39,404, 39,405, 39,406, 39,407, 39,408, 39,409, 39,410, 39,411, 39,412, 39,413, 39,414, 39,415, 39,416, 39,417, 39,418, 39,419, 39,420, 39,421, 39,422, 39,423, 39,424, 39,425, 39,426, 39,427, 39,428, 39,429, 39,430, 39,431, 39,432, 39,433, 39,434, 39,435, 39,436, 39,437, 39,438, 39,439, 39,440, 39,441, 39,442, 39,443, 39,444, 39,445, 39,446, 39,447, 39,448, 39,449, 39,450, 39,451, 39,452, 39,453, 39,454, 39,455, 39,456, 39,457, 39,458, 39,459, 39,460, 39,461, 39,462, 39,463, 39,464, 39,465, 39,466, 39,467, 39,468, 39,469, 39,470, 39,471, 39,472, 39,473, 39,474, 39,475, 39,476, 39,477, 39,478, 39,479, 39,480, 39,481, 39,482, 39,483, 39,484, 39,485, 39,486, 39,487, 39,488, 39,489, 39,490, 39,491, 39,492, 39,493, 39,494, 39,495, 39,496, 39,497, 39,498, 39,499, 39,500, 39,501, 39,502, 39,503, 39,504, 39,505, 39,506, 39,507, 39,508, 39,509, 39,510, 39,511, 39,512, 39,513, 39,514, 39,515, 39,516, 39,517, 39,518, 39,519, 39,520, 39,521, 39,522, 39,523, 39,524, 39,525, 39,526, 39,527, 39,528, 39,529, 39,530, 39,531, 39,532, 39,533, 39,534, 39,535, 39,536, 39,537, 39,538, 39,539, 39,540, 39,541, 39,542, 39,543, 39,544, 39,545, 39,546, 39,547, 39,548, 39,549, 39,550, 39,551, 39,552, 39,553, 39,554, 39,555, 39,556, 39,557, 39,558, 39,559, 39,560, 39,561, 39,562, 39,563, 39,564, 39,565, 39,566, 39,567, 39,568, 39,569, 39,570, 39,571, 39,572, 39,573, 39,574, 39,575, 39,576, 39,577, 39,578, 39,579, 39,580, 39,581, 39,582, 39,583, 39,584, 39,585, 39,586, 39,587, 39,588, 39,589, 39,590, 39,591, 39,592, 39,593, 39,594, 39,595, 39,596, 39,597, 39,598, 39,599, 39,600, 39,601, 39,602, 39,603, 39,604, 39,605, 39,606, 39,607, 39,608, 39,609, 39,610, 39,611, 39,612, 39,613, 39,614, 39,615, 39,616, 39,617, 39,618, 39,619, 39,620, 39,621, 39,622, 39,623, 39,624, 39,625, 39,626, 39,627, 39,628, 39,629, 39,630, 39,631, 39,632, 39,633, 39,634, 39,635, 39,636, 39,637, 39,638, 39,639, 39,640, 39,641, 39,642, 39,643, 39,644, 39,645, 39,646, 39,647, 39,648, 39,649, 39,650, 39,651, 39,652, 39,653, 39,654, 39,655, 39,656, 39,657, 39,658, 39,659, 39,660, 39,661, 39,662, 39,663, 39,664, 39,665, 39,666, 39,667, 39,668, 39,669, 39,670, 39,671, 39,672, 39,673, 39,674, 39,675, 39,676, 39,677, 39,678, 39,679, 39,680, 39,681, 39,682, 39,683, 39,684, 39,685, 39,686, 39,687, 39,688, 39,689, 39,690, 39,691, 39,692, 39,693, 39,694, 39,695, 39,696, 39,697, 39,698, 39,699, 39,700, 39,701, 39,702, 39,703, 39,704, 39,705, 39,706, 39,707, 39,708, 39,709, 39,710, 39,711, 39,712, 39,713, 39,714, 39,715, 39,716, 39,717, 39,718, 39,719, 39,720, 39,721, 39,722, 39,723, 39,724, 39,725, 39,726, 39,727, 39,728, 39,729, 39,730, 39,731, 39,732, 39,733, 39,734, 39,735, 39,736, 39,737, 39,738, 39,739, 39,740, 39,741, 39,742, 39,743, 39,744, 39,745, 39,746, 39,747, 39,748, 39,749, 39,750, 39,751, 39,752, 39,753, 39,754, 39,755, 39,756, 39,757, 39,758, 39,759, 39,760, 39,761, 39,762, 39,763, 39,764, 39,765, 39,766, 39,767, 39,768, 39,769, 39,770, 39,771, 39,772, 39,773, 39,774, 39,775, 39,776, 39,777, 39,778, 39,779, 39,780, 39,781, 39,782, 39,783, 39,784, 39,785, 39,786, 39,787, 39,788, 39,789, 39,790, 39,791, 39,792, 39,793, 39,794, 39,795, 39,796, 39,797, 39,798, 39,799, 39,800, 39,801, 39,802, 39,803, 39,804, 39,805, 39,806, 39,807, 39,808, 39,809, 39,810, 39,811, 39,812, 39,813, 39,814, 39,815, 39,816, 39,817, 39,818, 39,819, 39,820, 39,821, 39,822, 39,823, 39,824, 39,825, 39,826, 39,827, 39,828, 39,829, 39,830, 39,831, 39,832, 39,833, 39,834, 39,835, 39,836, 39,837, 39,838, 39,839, 39,840, 39,841, 39,842, 39,843, 39,844, 39,845, 39,846, 39,847, 39,848, 39,849, 39,850, 39,851, 39,852, 39,853, 39,854, 39,855, 39,856, 39,857, 39,858, 39,859, 39,860, 39,861, 39,862, 39,863, 39,864, 39,865, 39,866, 39,867, 39,868, 39,869, 39,870, 39,871, 39,872, 39,873, 39,874, 39,875, 39,876, 39,877, 39,878, 39,879, 39,880, 39,881, 39,882, 39,883, 39,884, 39,885, 39,886, 39,887, 39,888, 39,889, 39,890, 39,891, 39,892, 39,893, 39,894, 39,895, 39,896, 39,897, 39,898, 39,899, 39,900, 39,901, 39,902, 39,903, 39,904, 39,905, 39,906, 39,907, 39,908, 39,909, 39,910, 39,911, 39,912, 39,913, 39,914, 39,915, 39,916, 39,917, 39,918, 39,919, 39,920, 39,921, 39,922, 39,923, 39,924, 39,925, 39,926, 39,927, 39,928, 39,929, 39,930, 39,931, 39,932, 39,933, 39,934, 39,935, 39,936, 39,937, 39,938, 39,939, 39,940, 39,941, 39,942, 39,943, 39,944, 39,945, 39,946, 39,947, 39,948, 39,949, 39,950, 39,951, 39,952, 39,953, 39,954, 39,955, 39,956, 39,957, 39,958, 39,959, 39,960, 39,961, 39,962, 39,963, 39,964, 39,965, 39,966, 39,967, 39,968, 39,969, 39,970, 39,971, 39,972, 39,973, 39,974, 39,975, 39,976, 39,977, 39,978, 39,979, 39,980, 39,981, 39,982, 39,983, 39,984, 39,985, 39,986, 39,987, 39,988, 39,989, 39,990, 39,991, 39,992, 39,993, 39,994, 39,995, 39,996, 39,997, 39,998, 39,999, 40,000, 40,001, 40,002, 40,003, 40,004, 40,005, 40,006, 40,007, 40,008, 40,009, 40,010, 40,011, 40,012, 40,013, 40,014, 40,015, 40,016, 40,017, 40,018, 40,019, 40,020, 40,021, 40,022, 40,023, 40,024, 40,025, 40,026, 40,027, 40,028, 40,029, 40,030, 40,031, 40,032, 40,033, 40,034, 40,035, 40,036, 40,037, 40,038, 40,039, 40,040, 40,041, 40,042, 40,043, 40,044, 40,045, 40,046, 40,047, 40,048, 40,049, 40,050, 40,051, 40,052, 40,053, 40,054, 40,055, 40,056, 40,057, 40,058, 40,059, 40,060, 40,061, 40,062, 40,063, 40,064, 40,065, 40,066, 40,067, 40,068, 40,069, 40,070, 40,071, 40,072, 40,073, 40,074, 40,075, 40,076, 40,077, 40,078, 40,079, 40,080, 40,081, 40,082, 40,083, 40,084, 40,085, 40,086, 40,087, 40,088, 40,089, 40,090, 40,091, 40,092, 40,093, 40,094, 40,095, 40,096, 40,097, 40,098, 40,099, 40,100, 40,101, 40,102, 40,103, 40,104, 40,105, 40,106, 40,107, 40,108, 40,109, 40,110, 40,111, 40,112, 40,113, 40,114, 40,115, 40,116, 40,117, 40,118, 40,119, 40,120, 40,121, 40,122, 40,123, 40,124, 40,125, 40,126, 40,127, 40,128, 40,129, 40,130, 40,131, 40,132, 40,133, 40,134, 40,135, 40,136, 40,137, 40,138, 40,139, 40,140, 40,141, 40,142, 40,143, 40,144, 40,145, 40,146, 40,147, 40,148, 40,149, 40,150, 40,151, 40,152, 40,153, 40,154, 40,155, 40,156, 40,157, 40,158, 40,159, 40,160, 40,161, 40,162, 40,163, 40,164, 40,165, 40,166, 40,167, 40,168, 40,169, 40,170, 40,171, 40,172, 40,173, 40,174, 40,175, 40,176, 40,177, 40,178, 40,179, 40,180, 40,181, 40,182, 40,183, 40,184, 40,185, 40,186, 40,187, 40,188, 40,189, 40,190, 40,191, 40,192, 40,193, 40,194, 40,195, 40,196, 40,197, 40,198, 40,199, 40,200, 40,201, 40,202, 40,203, 40,204, 40,205, 40,206, 40,207, 40,208, 40,209, 40,210, 40,211, 40,212, 40,213, 40,214, 40,215, 40,216, 40,217, 40,218, 40,219, 40,220, 40,221, 40,222, 40,223, 40,224, 40,225, 40,226, 40,227, 40,228, 40,229, 40,230, 40,231, 40,232, 40,233, 40,234, 40,235, 40,236, 40,237, 40,238, 40,239, 40,240, 40,241, 40,242, 40,243, 40,244, 40,245, 40,246, 40,247, 40,248, 40,249, 40,250, 40,251, 40,252, 40,253, 40,254, 40,255, 40,256, 40,257, 40,258, 40,259, 40,260, 40,261, 40,262, 40,263, 40,264, 40,265, 40,266, 40,267, 40,268, 40,269, 40,270, 40,271, 40,272, 40,273, 40,274, 40,275, 40,276, 40,277, 40,278, 40,279, 40,280, 40,281, 40,282, 40,283, 40,284, 40,285, 40,286, 40,287, 40,288, 40,289, 40,290, 40,291, 40,292, 40,293, 40,294, 40,295, 40,296, 40,297, 40,298, 40,299, 40,300, 40,301, 40,302, 40,303, 40,304, 40,305, 40,306, 40,307, 40,308, 40,309, 40,310, 40,311, 40,312, 40,313, 40,314, 40,315, 40,316, 40,317, 40,318, 40,319, 40,320, 40,321, 40,322, 40,323, 40,324, 40,325, 40,326, 40,327, 40,328, 40,329, 40,330, 40,331, 40,332, 40,333, 40,334, 40,335, 40,336, 40,337, 40,338, 40,339, 40,340, 40,341, 40,342, 40,343, 40,344, 40,345, 40,346, 40,347, 40,348, 40,349, 40,350, 40,351, 40,352, 40,353, 40,354, 40,355, 40,356, 40,357, 40,358, 40,359, 40,360, 40,361, 40,362, 40,363, 40,364, 40,365, 40,366, 40,367, 40,368, 40,369, 40,370, 40,371, 40,372, 40,373, 40,374, 40,375, 40,376, 40,377, 40,378, 40,379, 40,380, 40,381, 40,382, 40,383, 40,384, 40,385, 40,386, 40,387, 40,388, 40,389, 40,390, 40,391, 40,392, 40,393, 40,394, 40,395, 40,396, 40,397, 40,398, 40,399, 40,400, 40,401, 40,402, 40,403, 40,404, 40,405, 40,406, 40,407, 40,408, 40,409, 40,410, 40,411, 40,412, 40,413, 40,414, 40,415, 40,416, 40,417, 40,418, 40,419, 40,420, 40,421, 40,422, 40,423, 40,424, 40,425, 40,426, 40,427, 40,428, 40,429, 40,430, 40,431, 40,432, 40,433, 40,434, 40,435, 40,436, 40,437, 40,438, 40,439, 40,440, 40,441, 40,442, 40,443, 40,444, 40,445, 40,446, 40,447, 40,448, 40,449, 40,450, 40,451, 40,452, 40,453, 40,454, 40,455, 40,456, 40,457, 40,458, 40,459, 40,460, 40,461, 40,462, 40,463, 40,464, 40,465, 40,466, 40,467, 40,468, 40,469, 40,470, 40,471, 40,472, 40,473, 40,474, 40,475, 40,476, 40,477, 40,478, 40,479, 40,480, 40,481, 40,482, 40,483, 40,484, 40,485, 40,486, 40,487, 40,488, 40,489, 40,490, 40,491, 40,492, 40,493, 40,494, 40,495, 40,496, 40,497, 40,498, 40,499, 40,500, 40,501, 40,502, 40,503, 40,504, 40,505, 40,506, 40,507, 40,508, 40,509, 40,510, 40,511, 40,512, 40,513, 40,514, 40,515, 40,516, 40,517, 40,518, 40,519, 40,520, 40,521, 40,522, 40,523, 40,524, 40,525, 40,526, 40,527, 40,528, 40,529, 40,530, 40,531, 40,532, 40,533, 40,534, 40,535, 40,536, 40,537, 40,538, 40,539, 40,540, 40,541, 40,542, 40,543, 40,544, 40,545, 40,546, 40,547, 40,548, 40,549, 40,550, 40,551, 40,552, 40,553, 40,554, 40,555, 40,556, 40,557, 40,558, 40,559, 40,560, 40,561, 40,562, 40,563, 40,564, 40,565, 40,566, 40,567, 40,568, 40,569, 40,570, 40,571, 40,572, 40,573, 40,574, 40,575, 40,576, 40,577, 40,578, 40,579, 40,580, 40,581, 40,582, 40,583, 40,584, 40,585, 40,586, 40,587, 40,588, 40,589, 40,590, 40,591, 40,592, 40,593, 40,594, 40,595, 40,596, 40,597, 40,598, 40,599, 40,600, 40,601, 40,602, 40,603, 40,604, 40,605, 40,606, 40,607, 40,608, 40,609, 40,610, 40,611, 40,612, 40,613, 40,614, 40,615, 40,616, 40,617, 40,618, 40,619, 40,620, 40,621, 40,622, 40,623, 40,624, 40,625, 40,626, 40,627, 40,628, 40,629, 40,630, 40,631, 40,632, 40,633, 40,634, 40,635, 40,636, 40,637, 40,638, 40,639, 40,640, 40,641, 40,642, 40,643, 40,644, 40,645, 40,646, 40,647, 40,648, 40,649, 40,650, 40,651, 40,652, 40,653, 40,654, 40,655, 40,656, 40,657, 40,658, 40,659, 40,660, 40,661, 40,662, 40,663, 40,664, 40,665, 40,666, 40,667, 40,6			

From: [mary.guntert](#)
To: [Gloria Lambert](#)
Subject: Stone Ridge North (RZ22-05)
Date: Monday, November 21, 2022 2:51:14 PM

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Lenexa Planning Commission,

I am Mary Guntert, the trustee of the Myrtle E. Coker Trust. I am writing in behalf of the Trust, that was setup by my parent's Leon and Myrtle Coker years ago to preserve all the land they owned in the Lenexa area. They have always tried to work hand in hand with the City of Lenexa on numerous projects and did live long enough to see some to fruition.

The trust now owns one property in Lenexa at 83 rd. and Mize Rd. which is listed For Sale with Kevin Tubbesing's company and they have worked with your staff to fulfill your vision of how our property should be developed. I hope you continue to work with landowners and developers so this area will be another outstanding neighborhood in Lenexa.

Thank you
Mary Guntert

I do have one concerning comment about this area: it bothers a number of us older residents the vandalism (disrespect) of Governor John Anderson's abandoned home.

Granted it was not kept up that great, but the broken windows and torn off siding done at a party was pretty tacky. If they are going to tear it down get a permit and a demolition company and tear it down. John worked hard and did tremendous work for his neighbors, Johnson County and the state of Kansas.

Thank you and now I'll try not to fall off my soapbox!

Sent from my iPad

CALL TO ORDER

Chairman Poss called the regular meeting of the Lenexa Planning Commission to order at 7:13 p.m. on Monday, December 5, 2022. The meeting was held in the Community Forum at Lenexa City Hall at 17101 W. 87th Street Parkway, Lenexa, Kansas.

ROLL CALL

Commissioners Present

Commissioner John Handley
Commissioner Ben Harber
Commissioner Jerry Harper
Commissioner Don Horine
Commissioner David Woolf
Commissioner Brenda Macke
Vice-Chairman Mike Burson
Chairman Chris Poss

Commissioners Absent

Commissioner Jason Leib
Commissioner Curt Katterhenry

Staff Present

Scott McCullough, Director of Community Development
Stephanie Kisler, Planning and Development Administrator
Tim Collins, Engineering Construction Services Administrator
Andrew Diekemper, Fire Division Chief
Spencer Throssell, Assistant City Attorney II
Dave Dalecky, Planner II
Christa McGaha, Planner II
Gloria Lambert, Senior Administrative Assistant

APPROVAL OF MINUTES

The minutes of the November 7, 2022 meeting were presented for approval. Hearing no changes or corrections to the minutes of the regular meeting, Chairman Poss entertained a motion to **APPROVE** the minutes as written. Moved by Commissioner Burson, seconded by Commissioner Handley, and **APPROVED** by a majority voice vote.

Commissioner Horine and Commissioner Harber abstained due to their absence of the November 7, 2022 meeting.

CONSENT AGENDA

1. **Final Plat for Arbor Lake South, First Plat located at the northwest corner of Prairie Star Parkway and Shady Bend Road within the RP-1, Planned Residential (Low Density) District. PT21-14F**
Consideration of a final plat for the development of a 18.42-acre, 43 lot single-family subdivision.

2. **Final Plat for Mize Hill, Second Plat located at the southwest corner of Prairie Star Parkway and Clare Road within the RP-2, Planned Residential (Intermediate Density) District. PT22-24F**
Consideration of a final plat for the development of a 20.83-acre, 44 lot two-family subdivision.
3. **Final Plat for CubeSmart Lenexa located along Greenwood Street north of College Boulevard within the BP-2, Planned Manufacturing District. PT22-25F**
Consideration of a final plat for the development of a three-story, 91,600 sq. ft. climate controlled self-storage facility on 2.68 acres.
4. **Final Plat for Watercrest Landing, Sixth Plat located at the southwest corner of 89th Street and Freedom Street within the RP-1, Planned Residential (Low Density) District. PT22-26F**
Consideration of a final plat for the development of a 10.81-acre, 28 lot single-family subdivision.

Chairman Poss entertained a motion to **APPROVE** Consent Agenda Items one through 4. Moved by Commissioner Horine, seconded by Commissioner Woolf, and carried by a unanimous voice vote.

REGULAR AGENDA

PUBLIC HEARING

5. a. **Rezoning for Creekside Park located at the southeast corner of 91st Street and Clare Road from the AG, Agricultural District and the RP-1, Planned Residential Single-Family (Low-Density) District to the RP-1, Planned Residential Single-Family (Low-Density) District. RZ22-10 and RZ22-11**
- b. **Preliminary Plat for Creekside Park located at the southeast corner of 91st Street and Clare Road in the proposed RP-1, Planned Residential (Low Density) District. PT22-02P**
Consideration of the rezoning from RP-1 and AG Zoning Districts to the RP-1 Zoning District for the development of a 41-lot single-family residential subdivision on 44.45 acres.

APPLICANT PRESENTATION:

C.L. Maurer of Land Plan Engineering presented information about the proposed project. He displayed an image of the proposed plat. The project is south of future 91st Street. The property owner wishes to begin development of their property and it will match the single-family zoning that surrounds the area.

STAFF PRESENTATION:

Dave Dalecky presented the Staff Report. Mr. Dalecky stated there are 3 applications for the Planning Commission to act on for this item, 2 rezoning requests and the preliminary plat. The preliminary plat serves as the preliminary plan. He noted that City-owned property exists to the north of the subject property. The subject property is zoned both AG and RP-1. The northerly part is zoned AG and is owned by the City. The southerly part is zoned RP-1 and is owned by August Ruf. The 2 rezoning requests will rezone the entire property to RP-1. The Future Land Use designations are agricultural and suburban density residential. There is a lot of land mass around the property designated for suburban density residential. The development is consistent with the surrounding area. The site is just under 45 acres with 60 lots and 9 tracts. There are stream corridors that cross the property and converge near the apex of the curve of future 91st Street. There are three clusters of lots. All lots meet the minimum standard lot size for the RP-1 district. The middle cluster of lots are larger and one of these lots are retained for the existing home. The image on the screen displayed the street connections. There are two southern connections and three connections to future 91st Street. The first phase will be on the east side of the subdivision and connect to Gander Street. 91st Street will not be constructed in the first phase and will likely be constructed in phases. The quantity of lots in the first phase will meet Fire Department restrictions for the maximum number of lots from a single

access point. A slide showed the concept plan for the area north of future 91st Street in context with the proposed Creekside Park plan. Mr. Dalecky highlighted the existing trail and stream system on the City-owned property. The subdivision plan includes a trail connection going north-south just west of the stream corridor. The applicant requests a waiver of the fence buffer requirement near the area where the stream corridor meets the south side of future 91st Street. The 2017 plat, a final plat was approved but was never recorded. Mr. Ruf's property was rezoned from AG to RP-1. In the same time frame, a one-lot and one-tract final plat was approved but was never recorded.

Chairman Poss clarified that the rezoning request was rezoning the City property from AG to RP-1 and Mr. Ruf's property from RP-1 to RP-1.

PUBLIC HEARING:

Chairman Poss **OPENED** the Public Hearing and asked if anyone wished to speak on this item.

Nancy Heywood of 24292 W. 91st Terrace asked about the land directly behind the existing home and whether it was going to be developed. Chairman Poss said the applicant will answer that question after the public hearing.

Hearing no further comments from the public, Chairman Poss entertained a motion to **CLOSE** the Public Hearing. Moved by Commissioner Horine, seconded by Commissioner Burson, and carried by a unanimous voice vote.

PLANNING COMMISSION DISCUSSION:

Chairman Poss asked the applicant to clarify the development adjacent to the resident's property. Carl Maurer explained the owner's 3-acre parcel where he resides will remain the same.

Scott McCullough noted that Mr. Ruf is allowed to add accessory structures and make improvements to his property in the future as any property owner is allowed per the requirements of the UDC. He explained that Mr. Ruf's property is not proposed to be divided into smaller lots than what is being shown on the preliminary plat.

Commissioner Woolf asked for clarification for the waiver on the fence. Was it because of the stream corridor? The applicant responded, yes.

Commissioner Horine asked why we need to rezone from RP-1 to RP-1. Scott McCullough said this is to coordinate the action of rezoning the entirety of the subject property which connect the plan and zoning districts when doing the ordinance on the rezoned property.

Chairman Poss said that the golden criteria had been met on this application.

MOTION:

Chairman Poss entertained a motion to recommend **APPROVAL** of the request to rezone property from the AG, Agricultural to the RP-1, Planned Residential (Low-Density) Zoning District for the Creekside Park development located at approximately the southeast corner of Clare Road and 91st Street.

Moved by Commissioner Woolf, seconded by Commissioner Macke, and carried by a unanimous voice vote.

Chairman Poss entertained a motion to recommend **APPROVAL** of the request to rezone the 31 acres from the RP-1, Planned Residential (Low-Density) Zoning Districts to the RP-1, Planned Residential (Low-Density) Zoning District for the Creekside Park development located at approximately the southeast corner of Clare Road and 91st Street.

Moved by Commissioner Handley, seconded by Commissioner Burson, and carried by a unanimous voice vote.

Chairman Poss entertained a motion to recommend **APPROVAL** of the Preliminary Plat for Creekside Park single-family subdivision located at the southeast corner of 91st Street and Clare Road with the waiver of the fence requirement along the section of 91st Street that is along the stream corridor in Tract I, as shown in Exhibit 6 of this Staff's Report.

Moved by Commissioner Harber, seconded by Commissioner Macke, and carried by a unanimous voice vote.

PUBLIC HEARING

6. a. **Special Use Permit for Lenexa Smart Storage located at the northwest corner of 87th Street Parkway and Maurer Road within the CP-2, Planned Community Commercial District. SU22-11**

b. Preliminary Plan for Lenexa Smart Storage located at the northwest corner of 87th Street Parkway and Maurer Road within the CP-2, Planned Community Commercial District. PL22-21P

Consideration of a special use permit to allow a self-service storage use in the CP-2, Planned Community Commercial District for the development a three-story, 118,560 sq. ft. self-service storage facility on 2.18 acres.

APPLICANT PRESENTATION:

Stan Hernly, of Hernly Associates presented information about the proposed project. He noted the location is northeast of I-435 and 87th Street Parkway. He provided multiple images of the site to show context. There is an existing stormwater detention area in the northwest corner of the site that will be cleaned up and brought up to standards. The parking lot will be realigned to improve traffic flow near the southeast corner of the site. Effort will be made to preserve trees along the west side of the property. There is a loading/unloading area on the south side of the proposed self-storage building. There are no exterior access units on the south, east, and west sides of the building. There are exterior access units on the northwest and north sides. Parking is on the east side of the building. The office area is at the southeast corner of the building. The northwest corner of the building features a sawtooth construction for the garage door access on the first floor. Windows are present on all facades, especially on the south and east elevations. He explained the proposed building materials used on the three-story building. Materials include Nichiha brick, with stucco and EIFS on the upper portion. They will also use an architectural metal panel and trim around the windows. The building expanse is broken up with different materials and colors. The architecture is meant to blend with the elements found in other buildings within Lenexa Pointe commercial center, but more modern colors are proposed.

STAFF PRESENTATION:

Dave Dalecky presented the Staff Report. He noted that the plans include a self-storage use within a commercial area known as Lenexa Pointe. There are existing buildings and pad sites within the development. The commercial center is north of 87th Street Parkway and is bounded by I-435 to the west and Maurer Road to the east. He mentioned the improvement of a drive aisle near the southeast corner of the site. The applicant requested a deviation request near the southwest corner of the site where the pavement does not meet the required 100' setback from the highway. The development has a 50-foot setback to the curb of the drive and 78 feet to the building, which is consistent with the development surrounding it. He highlighted that some existing landscaping will be removed for construction and staff will coordinate how to supplement new landscape materials to meet City requirements for landscaping. He noted that the applicant agreed to remove access doors along the west and south facades. Staff will work with the applicant to work on enhancing the façade by adding more embellishments, such as material trim details and color changes. He provided a refresher regarding a 2017 application for this site. The 2017 plan was not approved by the City Council and it did include a self-storage use with in-line retail for

7,000 SF of the first floor. He displayed the 2017 site plan and elevations. A table comparing the 2017 plan and the current plan was shown on the screen. Staff supports the proposed plan and request for a special use permit.

Chairman Poss asked Dave Dalecky to clarify the deviation request for the highway setback. Mr. Dalecky noted it was a 50' setback deviation request.

PUBLIC HEARING:

Chairman Poss **OPENED** the Public Hearing and asked if anyone wished to speak on this item. Hearing no comments from the public, Chairman Poss entertained a motion to **CLOSE** the Public Hearing. Moved by Commissioner Horine, seconded by Commissioner Burson, and carried by a unanimous voice vote.

PLANNING COMMISSION DISCUSSION:

Commissioner Horine said he liked the 2017 proposal but was unsure of the retail aspect of the plan then. He likes the current plan and that there is going to be something going there. He asked if it will be manned 24 hours. Bucky Brooks of Copaken Brooks said it will not be manned 24 hours a day, but it will be manned during the day. Each customer will have an access code to enter through the gate at any time.

Commissioner Harber said there are a lot of recently constructed self-storage buildings and he thinks it is an attractive execution of a storage building use. He supports the project.

Commissioner Handley said that 2/3 of the building looks attractive but he would like to see the upper floors on the north side enhanced if possible.

Commissioner Woolf said the window placement looks odd on the north side. He agreed with Commissioner Handley.

Chairman Poss supported the project in 2017 and thought the golden criteria were met then. He asked if there is a shared parking agreement on the site. Dave Dalecky noted that the site has cross-access for the drive aisles. The site plan exceeds the parking requirements. Chairman Poss asked where construction materials will be staged. Mr. Dalecky deferred to the applicant on construction staging. Bucky Brooks of Copaken Brooks said it would all be worked out. Chairman Poss agreed that there are some walls of the façade that could be improved by breaking up the expanses.

MOTION:

Chairman Poss entertained a motion to recommend **APPROVAL** for a Special Use Permit for a Self-service storage use for Lenexa Smart Storage, located at the northwest corner of 87th Street Parkway and Maurer Road, for an indefinite period of time.

Moved by Commissioner Woolf, seconded by Commissioner Handley, and carried by a unanimous voice vote.

Chairman Poss entertained a motion to recommend **APPROVAL** for a preliminary plan for Lenexa Smart Storage for a Self-service storage use located at the northwest corner of 87th Street Parkway and Maurer Road with a 50' deviation from the highway setback requirement.

Moved by Commissioner Horine, seconded by Commissioner Harber, and carried by a unanimous voice vote.

7. Preliminary Plan for Prairie Star Medical Office Building located at 9471 Meadow View Drive within the CP-1, Planned Neighborhood Commercial District. PL22-07PR

Consideration of a revised preliminary plan for the development of an 11,641 sq. ft. medical office building on 1.35 acres.

APPLICANT PRESENTATION:

Any Gabbert with Renaissance Infrastructure Consulting presented information about the proposed project. He described the location as being on the northeast corner of Prairie Star Parkway and Meadow View Drive. The proposed plan is similar to the original preliminary plan. He is available for questions.

STAFF PRESENTATION:

Christa McGaha presented the Staff Report. The applicant requests revised preliminary plan approved for a medical office building. She noted the location on an aerial map. A stormwater detention pond is located to the east of the site. A church is adjacent to the north. A Casey's gas station and convenience store is to the west. Another medical building existing across Prairie Star Parkway to the south. She showed a comparison of the 2006 approved preliminary plan side-by-side with the current proposed revised preliminary plan. The property was platted as Lot 8 of Reserve Plaza. There are two access points on Meadow View Drive, one of which aligns with Casey's although it is not as wide. She noted different elements of the site, such as a patio, the bike parking area, a public space amenity, the trash enclosure, and a sidewalk connection to Meadow View Drive. She displayed the proposed landscape plan and pointed out the trees planned for removal and planned to be preserved. The architectural renderings were presented on the screen. There are three tenant spaces. The building features a wooden entrance feature on the northwest and southwest corners of the building. Staff supports the proposed plan.

PLANNING COMMISSION DISCUSSION:

Chairman Poss asked if anyone had questions. No one on the Commission had questions.

MOTION:

Chairman Poss entertained a motion to recommend **APPROVAL** for a revised preliminary plan for Prairie Star Medical Office Building located at 9471 Meadow View Drive within the CP-1, Planned Neighborhood Commercial Zoning District.

Moved by Commissioner Harber, seconded by Commissioner Horine, and carried by a unanimous voice vote.

STAFF REPORT

Stephanie Kisler noted that a CIP Presentation will be featured in January 2023. She wished everyone a happy holiday and happy new year.

ADJOURNMENT

Chairman Poss ended the regular meeting of the Lenexa Planning Commission at 8:07 p.m. on Monday, December 5, 2022.